



Bassendean Precinct Structure Plan

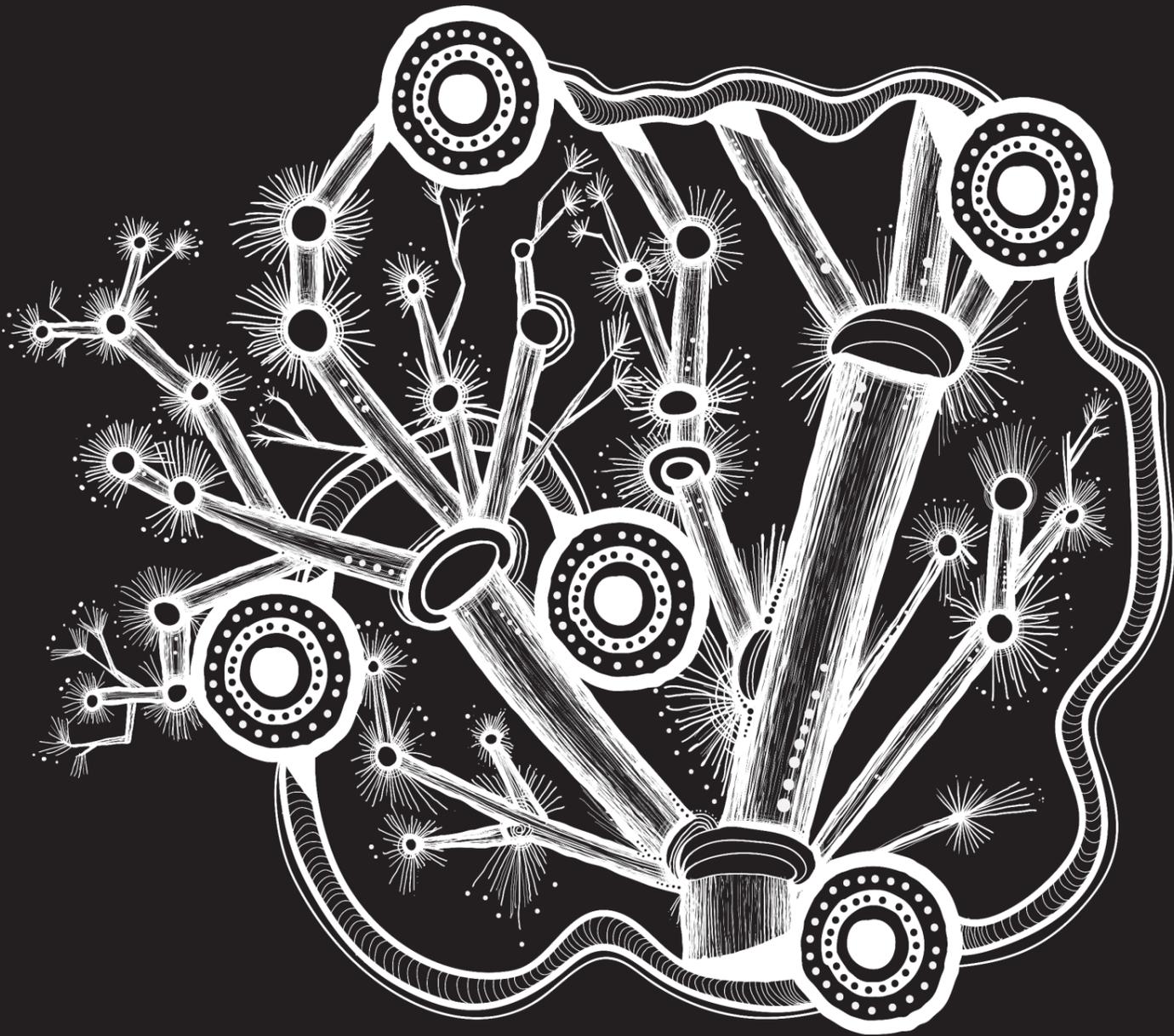
December 2025

Draft



Acknowledgement of Country

GHD acknowledges Aboriginal and Torres Strait Islander peoples as the Traditional Custodians of the land, water and sky throughout Australia on which we do business. We recognise their strength, diversity, resilience and deep connections to Country. We pay our respects to Elders of the past, present and future, as they hold the memories, knowledges and spirit of Australia. GHD is committed to learning from Aboriginal and Torres Strait Islander peoples in the work we do.



Prepared for the Town of Bassendean



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| Revision | Date | Approved |
|----------|----------|----------|
| A | 22.08.25 | AA |
| B | 25.10.25 | AA |
| C | 09.12.25 | AA |

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Endorsement

This Precinct Structure plan is prepared under the provisions of the Town of Bassendean Local Planning Scheme 11. It is certified that this Precinct Structure Plan was approved by resolution of the Western Australian Planning Commission on:

Signed for and on behalf of the Western Australian Planning Commission.

An officer of the Commission duly authorised by the Commission pursuant to section 16 of the *Planning and Development Act 2005* for that purpose, in the presence of:

Witness

Date

Date of Expiry

Table of amendments

| # | Summary | Amendment Type | Date |
|---|---------|----------------|------|
| | | | |

Summary table

The summary table provides an overview of the key attributes of the Precinct Structure Plan. The purpose of the summary table is to provide a reference to the key outcomes of the Precinct Structure Plan and for summary of key spatial attributes. The table may also be used to assess consistency with planning requirements, policy, and the Town's Local Planning Strategy.

| Item | Data | Plan Ref |
|--|--------------------------------|-----------------------|
| Total area covered by the structure plan | 56.55 ha | Part 1, section 1.1 |
| Area of each land use proposed: | | Structure Plan Map |
| Residential | 14.27ha | |
| Commercial | 10.85ha | |
| Industrial | Nil | |
| Total estimated lot yield | N/A | Part 2, section 2.3.1 |
| Estimated number of dwellings | 1,993 (including 557 existing) | Part 2, section 2.3.1 |
| Estimated residential site density | 79.3/ha*, 35.25/ha** | Part 2, section 2.3.1 |
| Estimated population | 4,185 | Part 2, section 2.3.1 |
| Number of high schools | Nil | Part 2, section 2.3 |
| Number of primary schools | 1 | Part 2, section 2.3 |
| Estimated commercial floor space | 44,600 sqm | Part 2, section 2.3.5 |
| Estimated area and percentage of POS: | | Part 2, section 2.3.7 |
| Regional open space | 5.95ha | |
| District open space | 3.74ha | |
| Neighbourhood parks | Nil | |
| Local parks | 1.19ha | |
| Estimated percentage of natural area | Nil | Part 2, section 2.2 |

*Based on net residential and commercial zoned land area

**Based on total area covered by structure plan

Executive summary

The Bassendean Precinct Structure Plan has been created to support the goals, planning principles, and community priorities outlined in the Town of Bassendean's Local Planning Strategy.

This Strategy, approved in 2020, sets out the community's key ideas for how land should be used and developed in the future. It shows a shared commitment from both the Town and the Western Australian Planning Commission to make sure future growth is well-organized and thoughtfully planned. The Structure Plan builds on this Strategy by providing a more detailed framework for planning in the Bassendean Precinct.

The Structure Plan includes three main parts:

- **Part 1 – Implementation**, which includes the statutory implementation mechanisms, statements of character and intent, development requirements and plan series.
- **Part 2 – Explanatory**, which is to inform and guide the operation of the implementation requirements. This part includes the relevant background, rationale and basis for the design of implementation outcomes.
- **Appendices** – The Bassendean Precinct Structure Plan is supported by a range of technical appendices that further give rationale to both parts of the document.

Part 1 of the Bassendean Precinct Structure Plan is to be read in conjunction with provisions of the Town of Bassendean Local Planning Scheme.

Clause 33 of the Local Planning Scheme 11 is to contain provisions that further implement the key attributes of the Bassendean Precinct Structure Plan. These provisions are to be made by a scheme amendment, proposed to be implemented with this Precinct Structure Plan.

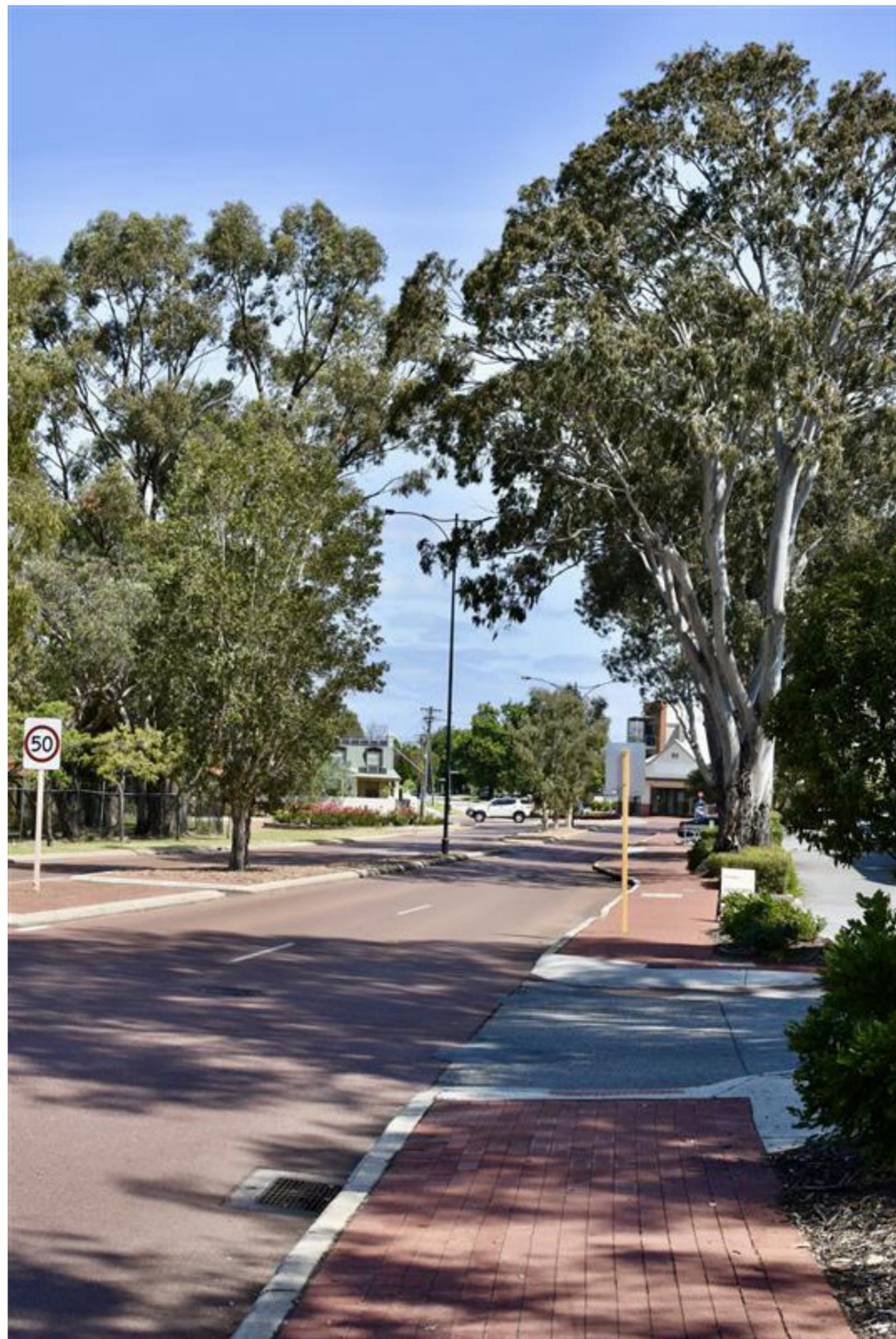
The Strategy recognises that the Bassendean Precinct will evolve over time to accommodate medium to high density mixed use development with increased functions in housing, employment and activity. The Bassendean Precinct Structure Plan has used the clear statements of outcomes in the Strategy, together with stakeholder feedback to further refine the planning outcomes for the Precinct.

The Bassendean Precinct Structure Plan ensures that development outcomes are consistent with the Strategy, the key priorities of the Town and its community, and the continuation of the enjoyment of amenity at the heart of Bassendean.



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Part 1 | Implementation

This Part sets out the vision for the area and provides guidance for the implementation of the Precinct Structure Plan.



1. Area and operation

1.1 Precinct area

This Precinct Structure Plan applies to all the land depicted as being within the precinct boundary on the Precinct Structure Plan Map.

1.2 Precinct Structure Plan Map

Map 1 – Precinct Structure Plan allocates zones and density codes within the structure plan area.

1.3 Operation

The plan is in effect from the date stated on the cover (once endorsed) and for a period of 10 years or for any other period approved by the WAPC.

Where any provision of the Precinct Structure Plan conflicts with the provisions of a planning scheme in force at the time, the provisions of the scheme prevail. Where this Precinct Structure Plan is inconsistent with the provisions of a specific policy or design guideline, or an adopted Local Development Plan, the adopted policy, design guidelines or Local Development Plan shall prevail to the extent of the inconsistency.

1.4 Application of the R-Codes

1.4.1 Land coded R80 or greater

State Planning Policy 7.3 – Residential Design Codes (Volume 2 – Apartments) (SPP7.3) provides that development provisions for land coded R80 or above may be set out in a Precinct Structure Plan.

SPP7.3 (Volume 2 – Apartments) applies to all residential dwellings coded R80 or above within the Precinct. The provisions of this Precinct Structure Plan and SPP7.3 (Volume 2 – Apartments) also apply to non-residential developments (or non-residential components of mixed-use developments) to the extent that:

- An Element Objective of the R-Codes provides guidance to the assessment of the development.
- An Acceptable Outcome included in Part 2.1 (Primary Controls) of SPP7.3 provides built form criteria for the development.
- Any other Acceptable Outcome or Design Guidance contained in SPP7.3 is relevant to the design of a non-residential development.

The relevant provisions of SPP7.3 that apply to non-residential development (or non-residential components of mixed-use developments) are as follows:

- Part 2 – Primary Controls.
- Part 3 – Siting the Development, including 3.3 Tree Canopy and Deep Soil Areas, 3.6 Public Domain Interface, 3.7 Pedestrian Access and Entries, 3.8 Vehicle Access, 3.9 Car and Bicycle Parking.
- Part 4 – Designing the Building, including 4.10 Facade Design, 4.12 Landscape Design, 4.15 Energy Efficiency, 4.16 Water Management, 4.17 Waste Management.

1.4.2 Land coded R60 or below

SPP7.3 (Volume 1) applies to all residential dwellings coded R60 or below within the Precinct.

1.5 Staging

This Precinct Structure Plan sets out a framework for gradual urban growth aligned with regional and local strategic objectives. It aims to deliver at least 1,368 new dwellings by 2050, primarily through mid-rise, mixed-use development. Non-residential growth will be achieved through redevelopment of civic assets and strategic landholdings.

Infrastructure upgrades will occur incrementally in response to development demand. Water and sewer networks require future upsizing, though these works are currently outside capital investment programs. Electricity supply will require feeder reconfiguration in the future. Drainage improvements are essential to address existing flooding and support future capacity but these improvements will occur in specific locations within the Precinct at appropriate times.

Public realm upgrades are a key component of the plan, intended to improve amenity and support community outcomes. These upgrades are partially funded through development incentives that offer additional building height in exchange for community benefit contributions. Delivery is not guaranteed and will depend on the uptake of these incentives by developers.

1.6 Exercise of discretion

Decision makers may vary to the criteria outlined in this Precinct Structure Plan, where it is satisfied that the variation meets the intent of the relevant provision and:

- Aligns with the Precinct vision, plan's objectives, and statements of desired outcome contained in this plan.
- Does not have significant impacts on heritage, streetscapes, public spaces, transport networks, or the amenity and development potential of adjacent properties.
- Demonstrates high-quality design outcomes and takes into account advice of a Design Review Panel.
- Does not contribute to under or over-development that would conflict with the intended built form or planning outcomes for the broader Precinct.

1.7 Precinct vision

Decisions relating to development, including determination of development applications, improvements to the public realm and amendments to Council policies are to be consistent the Precinct vision. The Precinct vision provides direction to these decisions and informs the provisions and guidance contained in this plan.

Bassendean will grow into a lively town centre, with diverse housing in the streets beyond. It will offer a mix of homes, shops, jobs, and public spaces, all designed to be safe, attractive, and easy to get around. As the area changes, it will keep its unique character and heritage, while creating new opportunities for the community to thrive.

1.8 Plan's objectives



Protect and celebrate the environment, and heritage and character values

- Tree canopy is retained and increased over time.
- New development to contribute to high quality landscaping and public spaces.
- Heritage places are conserved and celebrated.
- Buildings and places are sustainable, use less energy, retain and provide mature trees, minimise waste and promote sustainable travel.



Places are vibrant and interesting, and the design of spaces is tailored to community needs

- Development returns broad community benefits including new facilities, landscaped spaces, pedestrian connections, housing diversity and sustainability.
- Key streets are activated, interesting, and blend old and new.
- Activities are anchored at key nodes and away from quieter residential areas.
- Buildings and activity improve connection with key places like BIC Reserve and Bassendean Oval.



Land use is diverse and provides opportunities for new businesses, community connection, and local employment

- Bassendean is a destination for locals, visitors and workers.
- Land use controls encourage a range of new businesses and provide local employment.
- It is easy to establish a new business, or change from one land use to another.
- There is a diverse range of activity that brings in people day and night.
- Spaces are provided for creative industries and community facilities.
- Bassendean Oval is transformed into a key destination for recreation, community activities, entertainment and housing.



Buildings are mixed use, mid-rise and complement character and heritage

- New buildings strongly reflect the context and character that surrounds.
- Height and density is arranged to minimise impacts and encourage new development.
- Sunlight to key public spaces is retained.
- The form, materials and characteristics of new buildings complements existing character.



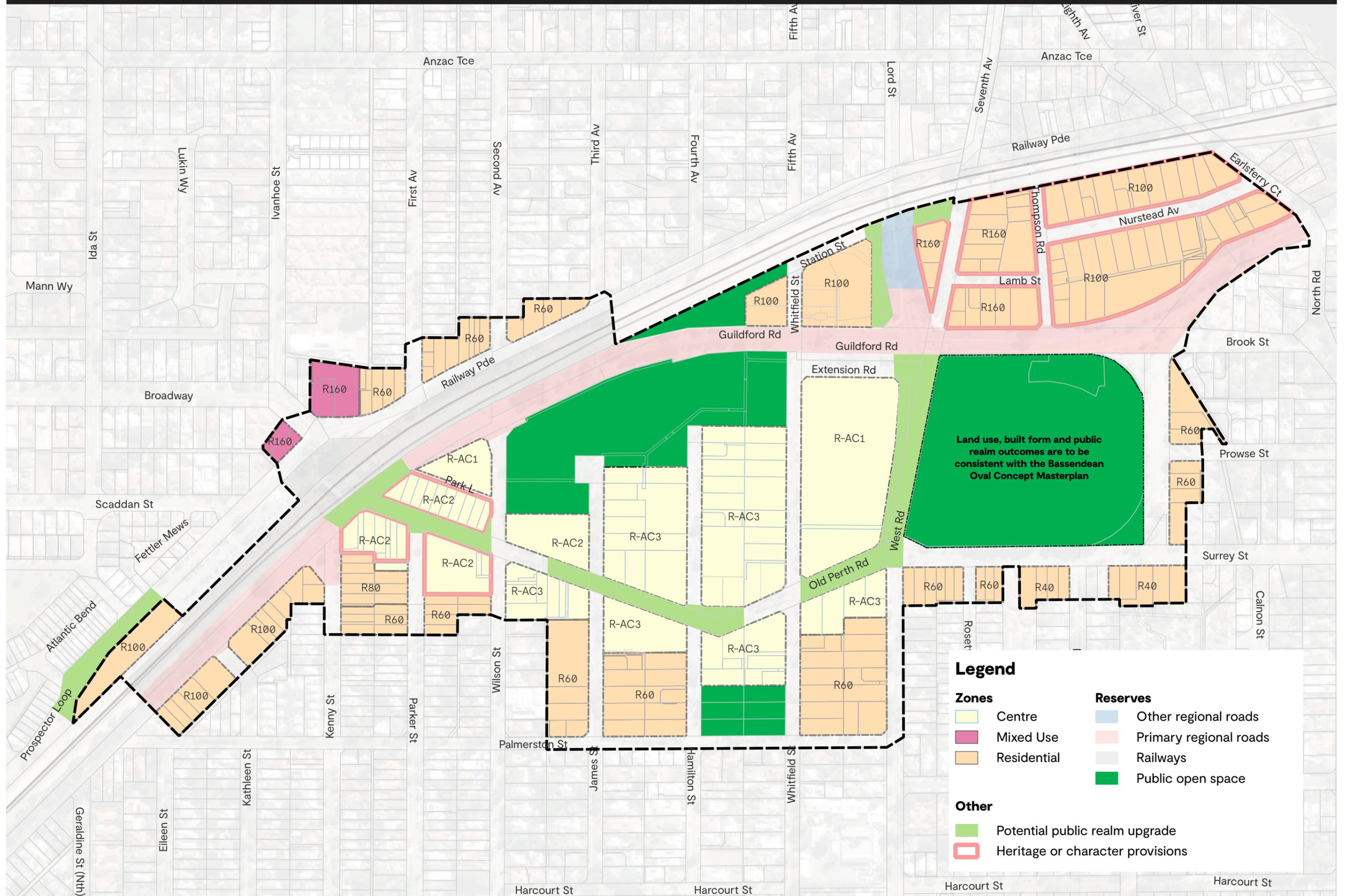
Movement networks are sustainable, safe and easy to use

- Improvements to the transport network encourage active transport, and make journeys safer and quicker.
- Pedestrians, cyclists and public transport users' safety and efficiency is prioritised.
- Mode-shift away from private vehicles is achieved.
- Access to public transport facilities is improved.
- A variety of land use around existing infrastructure is encouraged to facilitate use.



Public spaces are safe, inviting and evolve over time to provide for a growing population and workforce

- Public spaces are designed to improve safety and community connection.
- Improvements are made to key public space, including streets and parks to complement new developments.
- A series of public spaces connects activity along Old Perth Road with Bassendean Oval, BIC Reserve and Success Hill.
- Safety is improved over time through the design of buildings and spaces.



2. Subdivision and development requirements

2.1 Subdivision requirements

2.1.1 General requirements

When determining an application for subdivision, a decision maker shall have regard to the effect of the subdivision on the wider Precinct, considering:

- The purpose, vision and objectives set out in this Precinct Structure Plan.
- The design of public realm and movement arrangements specified in this Precinct Structure Plan.
- How future development will be able to address development criteria set out in the local planning framework.
- The preservation of character, pattern of subdivision, urban structure, and landscape.
- The requirements for lot servicing and vehicle access.

2.1.2 Contributions for public open space

All residential subdivisions shall contribute a minimum of 5 percent of the gross subdivisible area, or equivalent as cash-in-lieu. Contributions are to be made in accordance with WAPC's policies relating to public open space contributions and are to be used for:

- Upgrades to public open space within the precinct, consistent with the Town's Public Open Space Strategy.
- Improved access to public open space within the Precinct, recognising the lower rate of open space provision in areas adjoining the precinct.

2.1.3 Notifications on title

- Applications proposing 'noise sensitive premises' within proximity of the Perth-Midland railway, or Guildford Road shall be accompanied by an assessment that addresses the criteria of State Planning Policy 5.4 - Road and Rail Noise.
- Noise sensitive premises within the precinct that are exposed to transport noise that exceed the guidance contained in State Planning Policy 5.4 - Road and Rail Noise are to incorporate a notification on title pursuant to s 70A of the *Transfer of Land Act 1893* advising of exposure to excessive noise. Notice of this notification is to be included on the diagram or plan of survey.

2.2 General development requirements

2.2.1 Zones and coding

- Development and the use of land within the Precinct shall be in accordance with the zones and density codes depicted on **Map 1 - Precinct Structure Plan**.
- All development is to be consistent with objectives of the relevant zone depicted on **Map 1 - Precinct Structure Plan**, as specified in Table 1.

Table 1 Zone objectives

| Zone | Objective |
|-------------|---|
| Centre | <ul style="list-style-type: none"> — Provide a community focal point for people, services, employment and leisure that are highly accessible and do not adversely impact on adjoining residential areas. — Provide for non-residential uses that focus on weekly needs and services for a wider district catchment. — Provide a broad range of employment opportunities. — Ensure a mix of commercial and residential development, which provides for activity and accessibility at the street level and supports the provision of public transport and pedestrian links. — Provide for a wide range of different types of residential accommodation, including high density residential, to meet the diverse needs of the community |
| Mixed use | <ul style="list-style-type: none"> — To provide for a wide variety of active uses on street level which are compatible with residential and other non-active uses on upper levels. — To allow for the development of a mix of varied but compatible land uses such as housing, offices and eating establishments. |
| Residential | <ul style="list-style-type: none"> — To provide for a range of housing and a choice of residential densities to meet the needs of the community. — To facilitate and encourage high quality design, built form and streetscapes and provide for generous building separation that protects landscape character and residential amenity |

2.2.2 Land use

Zoned land

- In the Centre zone, decision maker shall have regard to the objectives of this Precinct Structure Plan, in conjunction with objectives and provisions of the Local Planning Scheme 11. **Table 2** describes uses that meet the objectives and uses that would not usually meet the objectives.

Table 2 Land use permissibility

| Activity type | Permitted uses that meet the plan's objectives | Uses that would usually not meet the plan's objectives |
|-----------------|---|---|
| Non-residential | Shop, Market, Restaurant/Cafe, Small bar, Tavern, Hotel, Art gallery, Child care premises, Consulting rooms, Educational establishment, Medical centre, Office, Recreation - Private, Community purpose, Cinema/Theatre | Bulky goods showroom, Fuel depot, Service station, Motor vehicle sales/repair, Restricted premises, Warehouse/storage |
| Residential | Dwellings (All types), Aged or dependant persons dwelling, Residential aged care facility. | Nil |

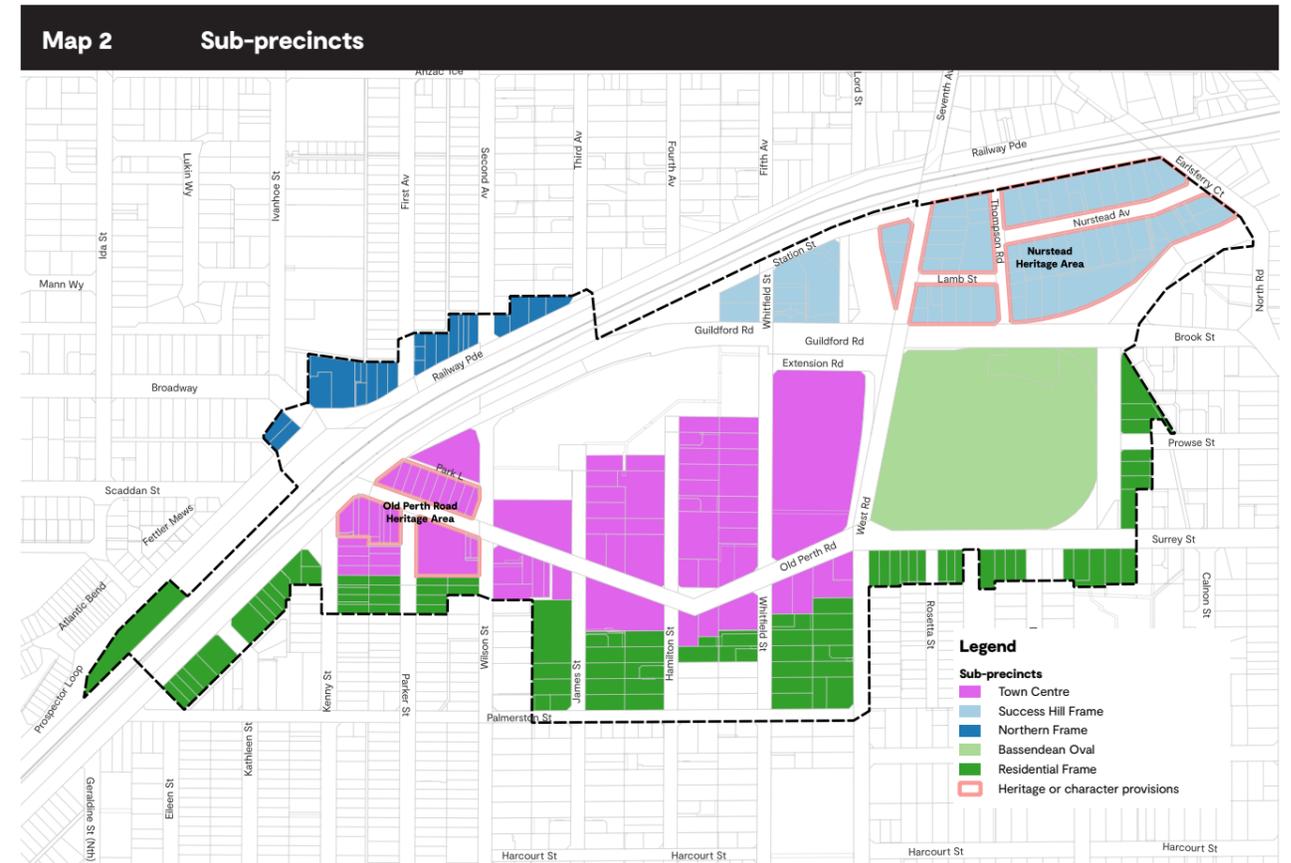
- Land use is to conform with guidance for ground floor land use contained in this plan.
- Land use in the Mixed Use and Residential zones, and any reserved land shall be in accordance with Local Planning Scheme 11.

Use of reserved land

- Use of reserves, including road ways and public open space areas shall be in accordance with the Town's local laws, Council policies and masterplans.
- Use of reserves adjacent to Active frontage (refer **Map 6 - Public domain interface**) is encouraged to be vibrant and diverse and include opportunities for alfresco dining and events. Activity is to be supported by landscaping improvements consistent with **Map 9 - Public Realm and Landscape Plan**.
- Use of land comprising the Bassendean Oval reserve is to be as per the Bassendean Oval Concept Masterplan.

2.2.3 Sub-precincts

The Precinct is divided into sub-precincts to describe the expected development, character and land use outcomes. Five sub-precincts are identified, including the Town Centre, which incorporates the Old Perth Road Heritage Area, the Success Hill Frame, which incorporates the Nurstead Heritage Area, Bassendean Oval, the Residential Frame and the Northern Frame. Development outcomes are to be consistent with the objectives described for each sub-precinct.



Town Centre

The Town Centre will:

- Be the place where people come together.
- Provide ample opportunities for business, amenities and public spaces for people to enjoy.
- Support land use that is diverse, fine-grain and scaled to reflect Bassendean's character.
- Be mid-rise with opportunities for larger buildings in specific locations that avoid impacts on neighbours.
- Have buildings that address the street and provide interest for passing pedestrians.
- Provide green streets, supported by on-structure planting.
- Provide safe and enjoyable connections with key parks and transport routes.
- Have buildings and spaces that are designed to complement existing character and heritage.
- Be safe for all types of people, day and night.

Success Hill Frame

The Success Hill Frame will:

- Be a mid-rise residential area with a mix of housing types.
- Provide legible, attractive and safe connection to Success Hill Station.
- Provide generous greenery in front and rear yards, be free from high fencing and parking.
- Ensure space is provided between buildings to soften transition between different scales of buildings.

Bassendean Oval

The Bassendean Oval sub-precinct will:

- Be developed in accordance with the Bassendean Oval Concept Masterplan.
- Contain a mix of recreational, mid-rise housing, civic and community uses, with supporting commercial uses that enhance vibrancy.
- Provide for improved recreational facilities, including playgrounds and landscaped areas.
- Be one of the key links between Success Hill and the Town Centre.

Northern Frame

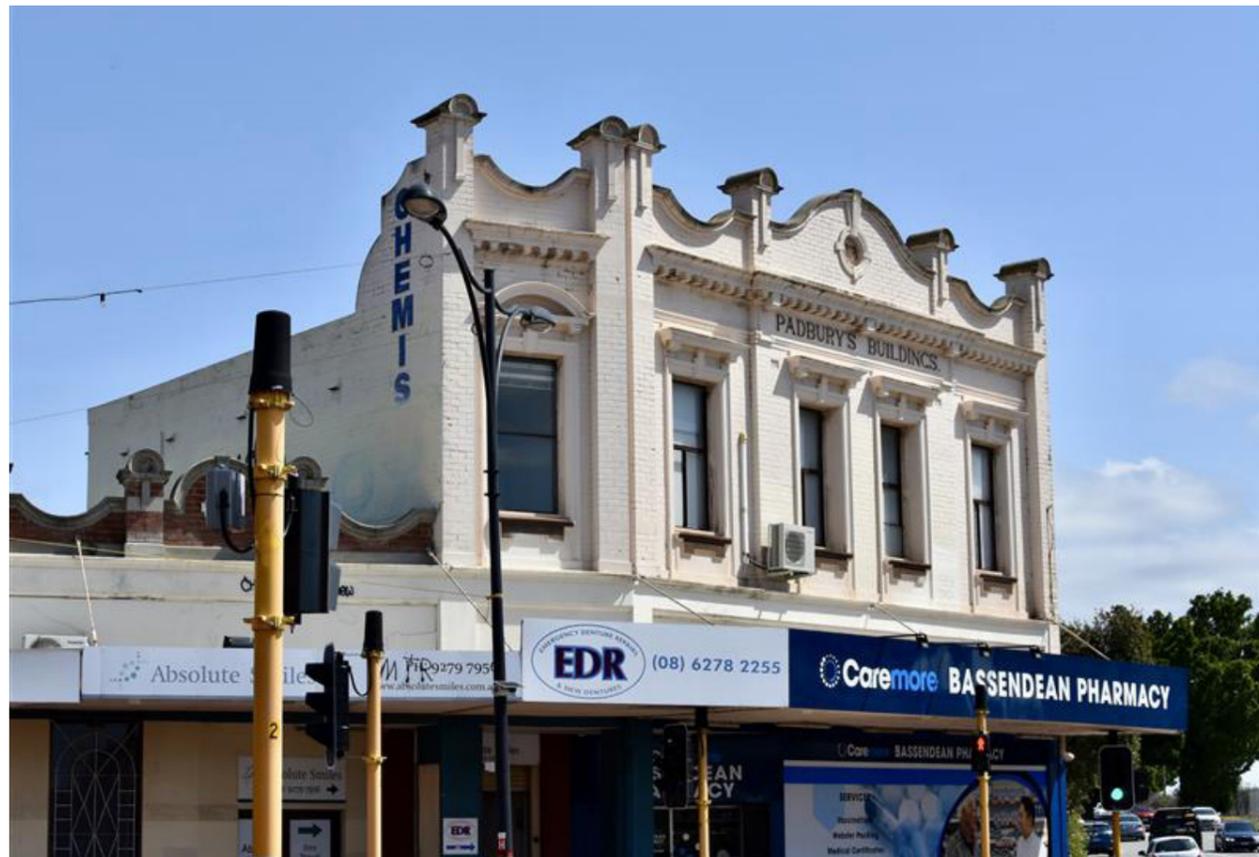
The Northern Frame will:

- Provide a node of activity and medium density housing that draws people towards the Town Centre.
- Support and encourage safe and easy movement across the Perth-Midland railway and Guildford Road.

Residential Frame

The Residential Frame will:

- Be quiet, green and leafy.
- Provide residential housing opportunities at medium densities nearby to the Town Centre.
- Provide a transition between suburban areas and the Town Centre.



2.2.4 Heritage and character

There are two areas within the Precinct where heritage and character considerations are key development considerations. These areas are the Old Perth Road Heritage Area and the Nurstead Heritage Area.

Development within these areas is to align with the design guidance provided in this section and any provisions contained in a local planning policy adopted for each area.

Old Perth Road Heritage Area

Development within the Old Perth Road Heritage Area addresses the design guidance contained in Table 3.

Table 3 Old Perth Road Heritage Area

| Design element | Design guidance |
|-------------------------------|---|
| Land use | <ul style="list-style-type: none"> — Ground floor uses are active and inviting. — Night time activity is encouraged. — Public spaces are used for events and activity that support business. |
| Scale | <ul style="list-style-type: none"> — Building proportions reflect existing scale (heights, parapets etc) with upper floors recessed from heritage fabric. — Scale is generous, while aligning with the form of existing heritage fabric. — Upper floors are recessed behind heritage fabric. |
| Landscape design | <ul style="list-style-type: none"> — Streets are greened through additional street tree planting and enhanced by on-structure planting. |
| Public realm interface | <ul style="list-style-type: none"> — Frontages to Old Perth Road promote continuous movement and activity, with services, utilities and driveways located on other frontages. Buildings address the street and provide comfort and interest for passing pedestrians. — Ground levels are heavily glazed and provide unobstructed interaction with the street. |
| Facade design and roof design | <ul style="list-style-type: none"> — Buildings are appointed with verandah and awning elements that reflect existing character. Awnings do not have columns, except where to support balconies. — New buildings avoid flat surfaces and include decorative features such as window surrounds, transoms, cornices and soffit eaves, parapets and pediment features. Features are complementary to existing fabric without mimicking existing details. — Facades are designed to reflect the fine grain form of traditional shopfronts and use lightweight materials. — Extensive ground floor glazing is encouraged, where consistent with the traditional form and use of the buildings. — Roofs are flat and edged by parapets. Skillion and pitched roofs are avoided. |
| Materials and colours | <ul style="list-style-type: none"> — Materials and colours are complementary to heritage fabric without replicating heritage places. — Shops fronts are simple in detail, utilising traditional materials like timber and lightweight aluminium in entryways, windows and glazing. — Colours reflect the existing use of neutral grey-sand hues, offset against bolder heritage colours. |

Nurstead Heritage Area

Development within the Nurstead Heritage Area addresses the design guidance contained in Table 4, in addition to requirements of the R-Codes.

Table 4 Nurstead Heritage Area

| Design element | Design guidance |
|-------------------------------|---|
| Primary controls | <ul style="list-style-type: none"> Street setback areas are landscaped and provide transition between semi-private and public areas. Street setback areas are free from vehicle parking, bin stores or private open space areas with the exception of balconies. |
| Landscape design | <ul style="list-style-type: none"> Development retains existing mature trees where appropriate. Provision is made for medium and large trees within the street setback area. New landscaping preferences native and water-wise species and integrates urban water management principles into designs. |
| Public domain interface | <ul style="list-style-type: none"> Building proportions mimic the lot pattern of the adjacent properties by utilising strong vertical elements to define former lot boundaries where amalgamation occurs. Side and rear setbacks provide separation, but are minimised (except to provide for solar access and privacy). |
| Facade design and roof design | <ul style="list-style-type: none"> Verandah and awning elements are used over exposed doorways and windows. Vertical columns and verandah/awning structures are lightweight. Buildings adopt pitched roof forms. Where lot amalgamation occurs, use of multiple hips and gables to reflect the scale of existing roof-forms. Buildings utilise traditional brick materials, in appropriate colours, and painted timber elements. Colours reflect the existing use of neutral grey-sand hues, offset against bolder heritage colours. |



2.2.5 Development requirements

All proposed development within the Precinct Structure Plan area is to comply with the requirements of this part. The requirements should be read in conjunction with the plan vision, objectives and the wider planning framework. Development requirements consist of:

| | |
|--------------------------------------|---|
| Desired outcomes | Statements used to inform the exercise of discretion in conjunction with the Element Objectives of the R-Codes |
| Development criteria | Criteria that describes development requirements, including amendments to the R-Codes. |
| Design guidance and rationale | Comments which provide additional context to guide decision makers in assessing compliance with the Desired Outcome and Development Criteria. |

2.2.5.1 Building height

Desired outcomes

- Height is greatest along Old Perth Road and graduates to surrounding neighbourhoods.
- Greater building height is incentivised on sites where visual and overshadowing impacts can be managed.
- New buildings enhance pedestrian comfort by preserving access to sunlight to key areas
- Character and heritage is reinforced through consistency in physical proportions.
- Building heights support opportunities for new housing and businesses.

Development criteria

Acceptable Outcome A2.2.1 of SPP7.3 is replaced to read as follows:

Maximum building height

- Development complies with the maximum building height limit depicted on **Map 3 - Building height**.
- Lift over-runs, rooftop plant and servicing are not extended more than 3.5 metres in height above the top of a building and shall be located, positioned or screened so as to not be significantly visible from the public realm.
- Where development exceeds the maximum building base height limit, the development shall be assessed in accordance with section 2.3 (Development Incentives for Community Benefit) of this plan.
- In the case of Lot 2 West Road, the maximum height of buildings on the land may be varied where all of the following criteria are met:
 - A Local Development Plan has been adopted.
 - The Local Development Plan describes how building height is re-distributed across the site to avoid amenity impacts, integrate with the public realm and provide community benefits in accordance with section 2.3 (Development Incentives for Community Benefit).

Floor-to-ceiling height

- Within the Old Perth Road Heritage Area, buildings provide floor-to-ceiling heights that complement the heights and parapets of adjacent heritage fabric.
- Where a dwelling is proposed on a ground floor facing a 'Semi-Active' frontage (refer **Map 6 - Public domain interface**), floor-to-floor heights shall be at least 4.0 metres to enable adaptation of the floorspace to non-residential uses.

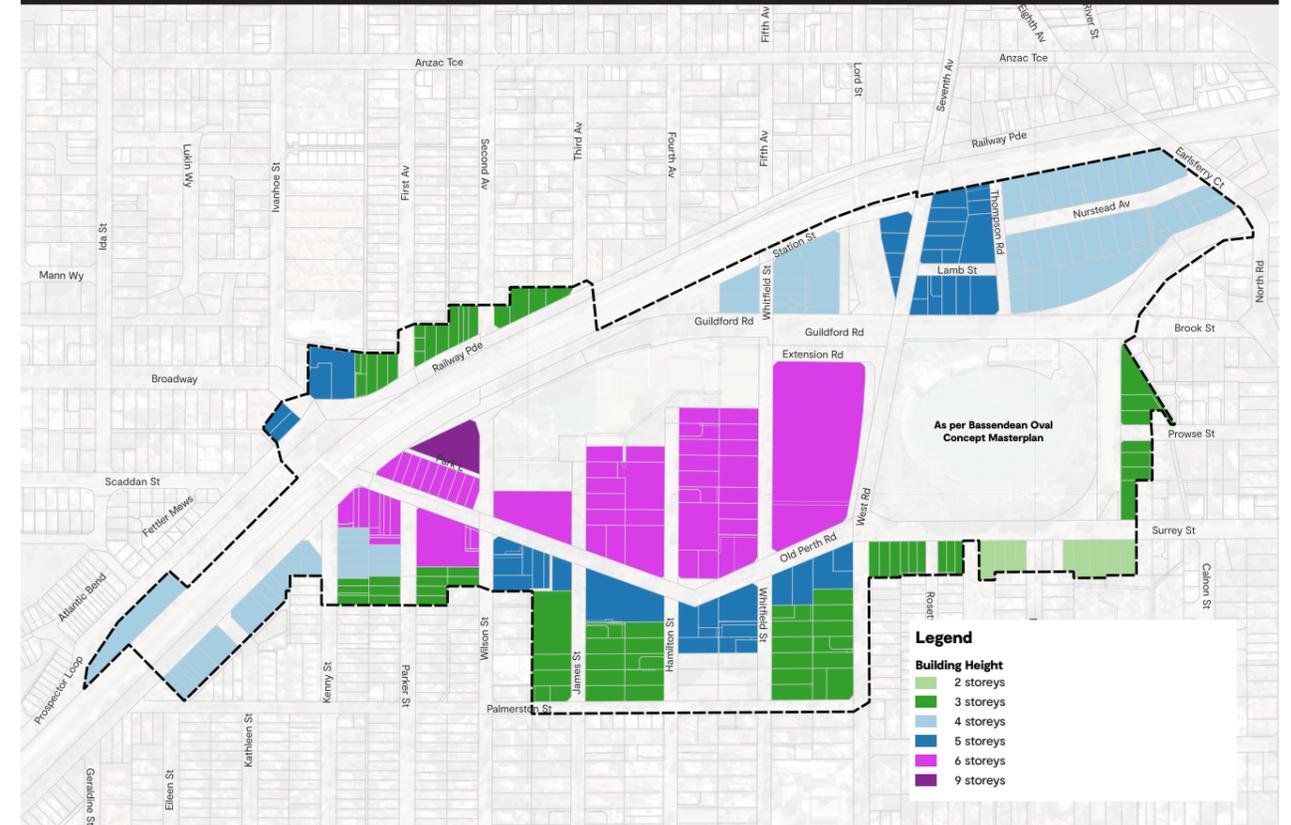
Design guidance and rationale

Building heights are planned to support growth targets set by Perth and Peel @3.5 million, the Town's Local Planning Strategy, and the Town Centre Masterplan. Taller buildings are focused around key locations such as Bassendean Station, and on large landholdings, where increased density supports public transport use and local activity.

Distribution of heights have been managed to protect the amenity of surrounding residential areas, ensuring new development fits comfortably within its context. Within the Old Perth Road Heritage Area, building scale and controls on floor-to-ceiling heights ensure new development is sympathetic to existing heritage fabric.

Ground floor ceiling heights have been designed to support long-term flexibility, allowing buildings to shift between residential and non-residential uses as demand changes over time.

Map 3 Building height



2.2.5.2 Street setbacks

Desired outcomes

- New buildings provide space for landscaping, seating and activity within setback areas.
- Taller buildings are setback to reduce visual bulk and preserve access to sunlight.
- Setbacks are designed to preserve the fine-grain lot structure along Old Perth Road and respond to heritage and character elements.
- Building designs are efficient, with multiple vertical stepped setbacks avoided.

Development criteria

Acceptable Outcome A2.3.1 of SPP7.3 is replaced to read as follows:

Street setbacks

- Development provides a minimum street setbacks as indicated on **Map 4 - Street setbacks**.

Setbacks at ground level

- Where development adjoins an 'Active Street' depicted on **Map 6 - Public domain interface**, a nil setback is provided except where to provide for:
 - The conservation of heritage fabric and consistency with the provisions of 2.2.4 - Heritage and character.
 - Retention, protection or enhancement of an existing building element worthy of retention.
 - Recesses for entries, doorways or essential services.

Upper level setbacks

- Where **Map 4 - Street setbacks** specifies a setback of 'Nil up to 2 storeys' or 'Nil or 2m* up to 4 storeys', upper levels are setback from the street to:
 - Provide separation to heritage protected place or contributory buildings within the Old Perth Road Heritage Area.
 - To ensure solar access to lot boundaries on the southern side of Old Perth Road at midday on the winter solstice (see Figure 1). Depending on the sites location and orientation, upper level setbacks will generally apply above the fourth storey, or 14.0 metres above natural ground level.
 - To provide tree retention or deep-soil areas in accordance with Design Element 3.3 of SPP7.3 - Residential Design Codes.

Design guidance and rationale

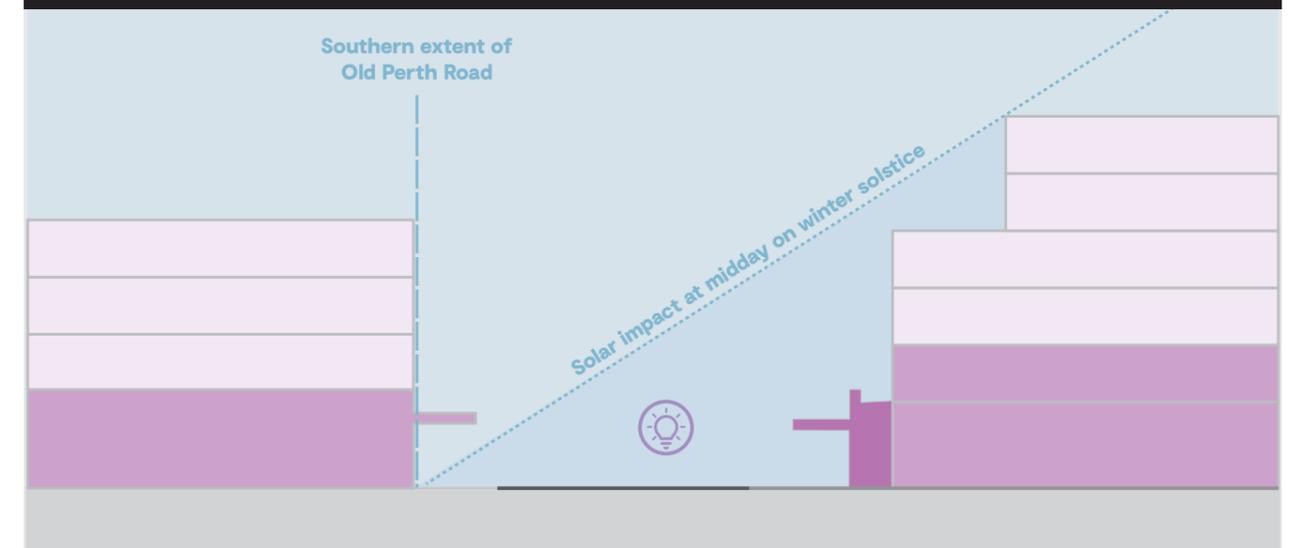
Street setbacks correspond to land use and activity within the building and the adjacent streetscape. Greater setbacks are applied to provide separation between public and private spaces where desirable (such as for ground floor residential dwellings). Mandated nil setbacks are applied on Old Perth Road to encourage interaction between businesses and public spaces, and to reflect the existing character of shopfronts within the Old Perth Road Heritage Area.

Upper floors are setback to protect the heritage values, preserve sunlight to footpaths, and support the growth of street trees.

Map 4 Street setbacks



Figure 1 Solar access to southern side of Old Perth Road



2.2.5.3 Side and rear setbacks

Desired outcomes

- Residential and commercial ground floor uses are designed to support vibrant streets.
- Parking, loading, and servicing are located to avoid impacts to pedestrian comfort and design quality.
- Development improves street level activation, pedestrian safety, and delivers quality design.
- Setbacks to neighbours minimises impacts of bulk, scale, privacy and access to light.

Development criteria

Acceptable outcome 2.4.1 of SPP7.3 is replaced to read as follows:

Side and rear setbacks

Development complies with the side and rear setbacks set out in Table 5 except where:

- A greater setback is required to address in Design Element 3.5 – Visual privacy of SPP7.3, and/or the boundary is a 'sensitive boundary' depicted on **Map 5 – Sensitive boundaries**, in which case the minimum boundary setback above the second storey shall be in accordance with Figure 2.
- The land is located within the Success Hill Frame, in which case the setbacks of Table 6 apply.

Table 5 Side and rear boundary setbacks

| R-Code | Boundary wall height (storeys) ¹ | Minimum side setbacks | Minimum rear setback |
|------------|---|-----------------------|----------------------|
| R60, R80 | As per the R-Codes | | |
| R100, R160 | 2 storeys ² | 3m | 6m |
| RAC-3 | 3 storeys ^{2,3} | Nil ³ | Nil |
| R-AC2 | No maximum | Nil | Nil |
| R-AC1 | As per Local Development Plan | | |
| Notes | ¹ Where the subject site and an adjoining site are subject to different codes, the length and height of any boundary wall on the boundary between them is determined by reference to the lower density code. ² Boundary wall only permitted on one boundary, and shall not exceed 2/3 length. ³ Where the subject site adjoins an area of public open space, buildings shall be setback a minimum of 2m. | | |

Table 6 Success Hill Frame side and rear boundary setbacks

| Boundary | | Minimum setback |
|----------------|-----------------|---|
| Side | Up to 2 storeys | 1.5 m |
| | Above 2 storeys | 3.0 m |
| Rear | | 6.0 m |
| Boundary walls | | Within the Success Hill Frame, boundary walls shall not be permitted except to provide for essential services or communal facilities. |

Design guidance and rationale

Setbacks help create vibrant, safe, and attractive streets by allowing space for active ground-floor uses and pedestrian movement. Locating parking and servicing away from the street frontage improves comfort and design quality. Setbacks also reduce the impact of building bulk on neighbours, protecting privacy, access to light, and overall amenity.

Setbacks in the Success Hill Frame area are designed to support high-quality urban outcomes as the area transitions and intensifies. Generous rear setbacks are applied to preserve mature trees and maintain the backyard character that defines the area's residential identity. Modified side setbacks help manage building bulk, protect privacy, and ensure access to natural light supporting a balanced approach to growth.

Map 5 Sensitive boundaries

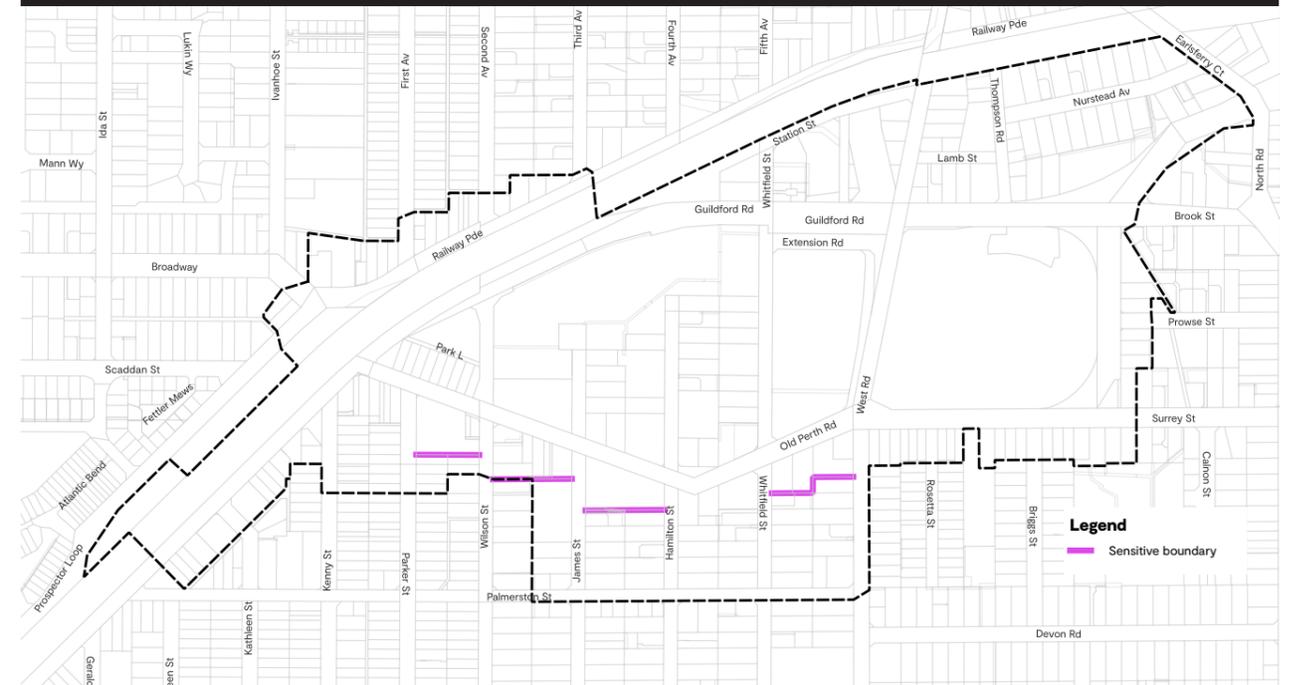
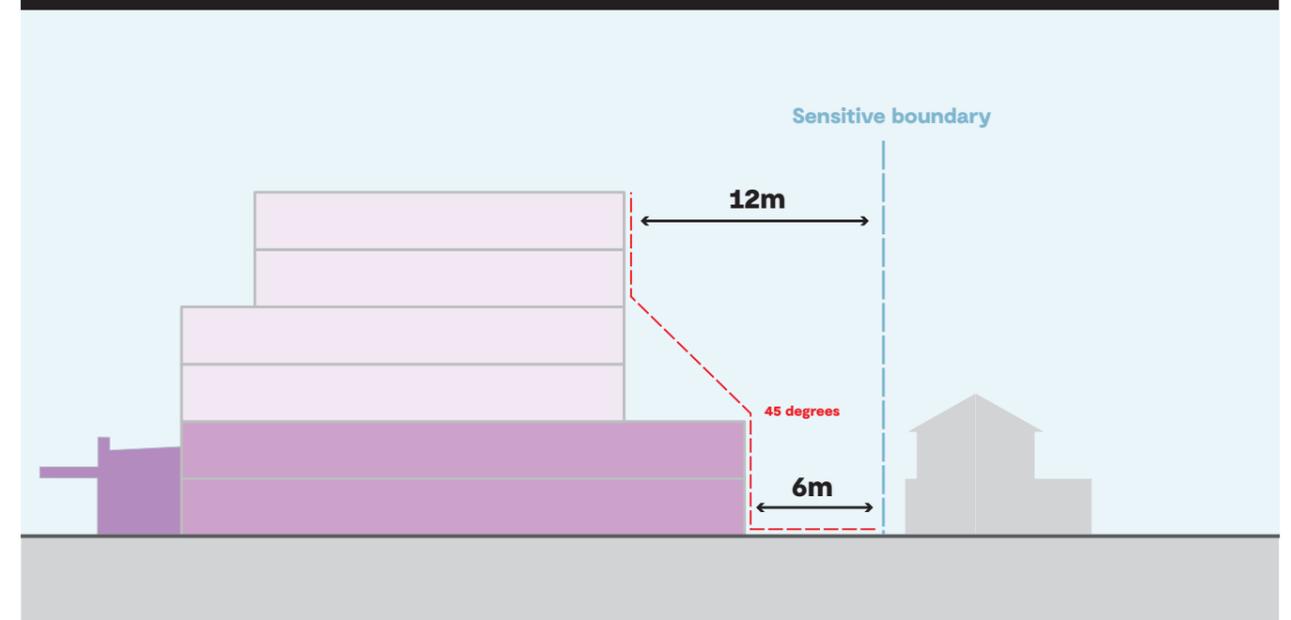


Figure 2 Sensitive boundary setback



2.2.5.4 Plot ratio

Desired outcomes

- a. In lower density and residential areas of the Precinct, buildings are appropriately scaled and separated.
- b. For key sites and mixed use areas, plot ratio is applied flexibly to support growth and a mix of building types.

Development criteria

Acceptable Outcome A2.5.1 of SPP7.3 is replaced to read as follows:

- a. Development complies with the plot ratio criteria set out in Table 7.

Table 7 Plot ratio

| R-Code | Plot ratio |
|--------------|------------|
| R80 | 1.0 |
| R100 | 1.3 |
| R160, R-AC3 | 2.0 |
| R-AC2, R-AC1 | No maximum |

- a. Additional plot ratio may be granted for a site coded R-AC3 or R160 where additional building height is sought under section 2.3 – Development incentives for community benefit.

Design guidance and rationale

Flexible plot ratio provisions across the Precinct support a balanced approach to growth, allowing for increased density and a mix of building types in key locations while maintaining appropriate scale in lower density areas.

It supports the delivery of diverse housing, commercial activity, and mixed-use outcomes in line with the Bassendean Town Centre Masterplan and Perth and Peel @ 3.5 million, which prioritises infill development near transport, services, and activity centres. It allows key redevelopment sites such as the Hawaiians Bassendean Shopping Centre to be developed in a way that is responsive to context, and that generates opportunities for community benefits.



2.2.5.5 Public domain interface

Desired outcomes

- Streets are active and safe because buildings face public areas, are well design and have clear entrances.
- Homes and businesses on the ground floor support a mix of uses and street-level activity.
- Shopfronts are design to align with existing character, be welcoming to customers, and promote safety.

Development criteria

Acceptable Outcome A3.6.2 of SPP7.3 (Volume 2) is amended to read as follows:

Design of frontages

- Where development extends over multiple lots, or is permitted to have a nil setback to a side boundary, vertical design elements/treatments are applied at the street interface to:
 - Reflect the original pattern of subdivision.
 - Reflect the original patterns of buildings, entries or tenancies at street level (refer Figure 3).
- Where a frontage is an 'Active' interface (refer **Map 6 - Public domain interface**):
 - Awnings, balconies, or the like shall provide continuous weather protection for pedestrians.
 - Retail and commercial units have a depth between 6 and 12 metres, and providing direct, universal access to the public footpath.
 - Blank walls or sections of walls do not exceed 3 metres in length, except where to provide for essential services or address the provisions of 2.2.4 - Heritage and character.
 - Shop front are articulated with appropriate use of stall risers, window design, awnings, and other architectural features, that compliment the design of neighbouring retail frontages.
 - At least 50% of the street interface at the ground floor should be clear, untinted glazing.
 - Alfresco areas are encouraged where there is adequate verge space.
- Where a frontage is a 'Semi-Active' interface (refer **Map 6 - Public domain interface**):
 - Ground floor tenancies should be capable of conversion between residential and commercial uses, by demonstrating:
 - Compliance with accessibility requirements as per the National Construction Code.
 - Provide an internal floor-to-floor height of at least 4.0 metres.
 - Blank walls or sections of walls should not exceed 3 metres in length, except where to provide for essential services.
 - Weather protection is provided to pedestrian entries and where interaction occurs between public and private areas, such as servery windows or seating areas.
 - Setback areas may be used for commercial activities like alfresco dining, provided adjacent ground floor uses are non-residential and does not impact the amenity of adjacent residential use.
- Car parking is not visible from the street. Car parking is sleeved on the ground floor to conceal car parking on frontages where a frontage is designated as 'Active Street' or 'Semi-Active' is indicated on **Map 6 - Public domain interface**.

Development requirements

- Buildings makes a contribution to unique character of the built environment within the Precinct and the wider neighbourhood. New buildings and spaces shall be designed to address the design quality outcomes specified in Table 9 and where applicable, by reviewed by a Design Review Panel in accordance with the Town's LPP9 - Design Review Panel.

Design guidance and rationale

Mixed-use development and active frontages are essential to achieving a vibrant, safe and inclusive place. Public domain interface provisions ensure interaction between spaces and people are aligned to other elements like building scale and land use by:

- Describing frontage types and design quality expectations to improve street-level engagement, safety, and contribute to a vibrant town centre.
- Appropriately locating residential and commercial ground floor uses that support mixed-use outcomes and reduces conflicts between uses.
- integrating parking, loading, and servicing opportunities while protecting pedestrian comfort and public realm quality.

Map 6 Public domain interface

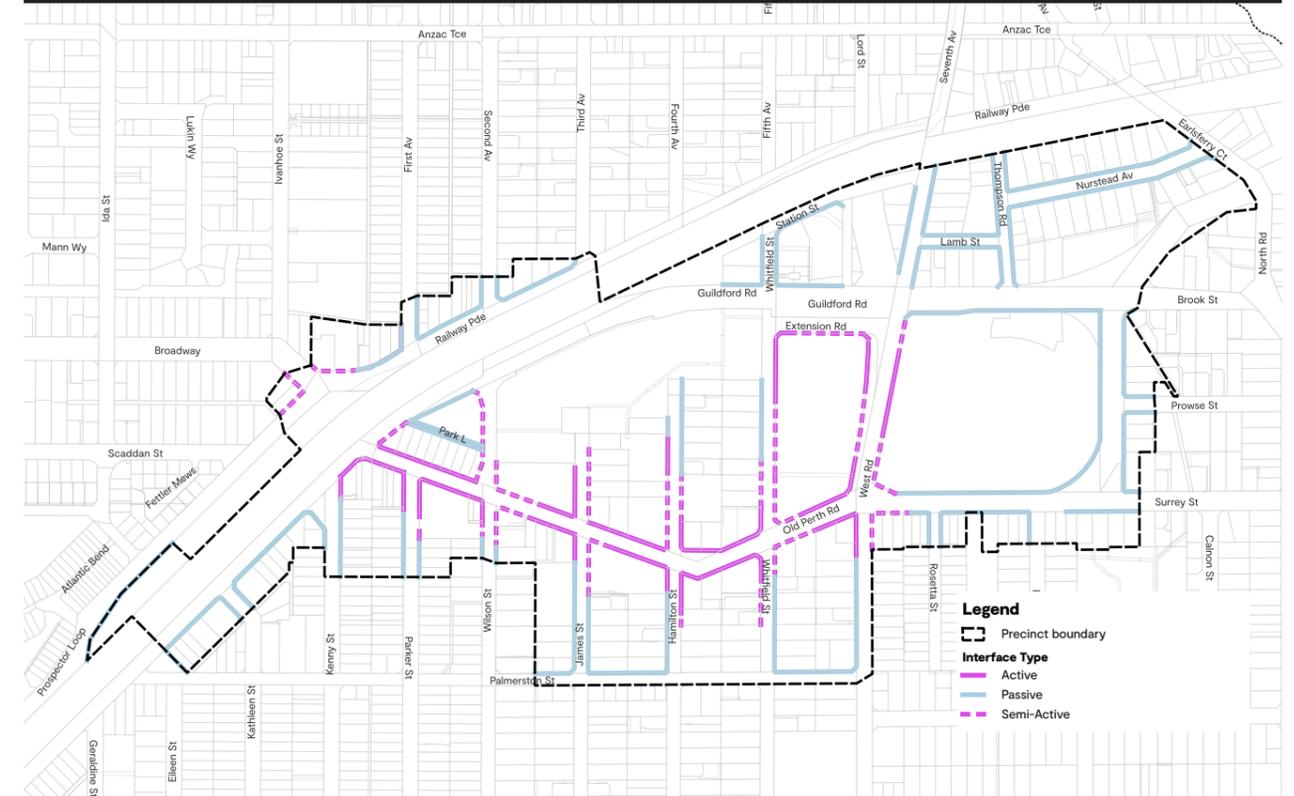
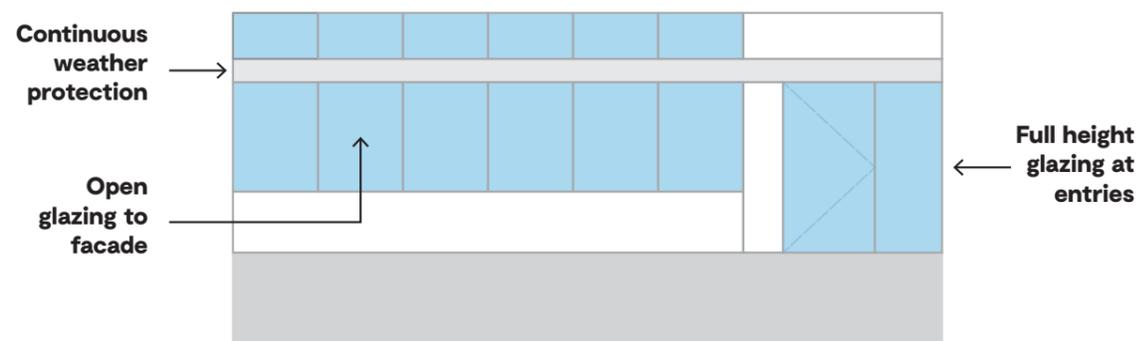


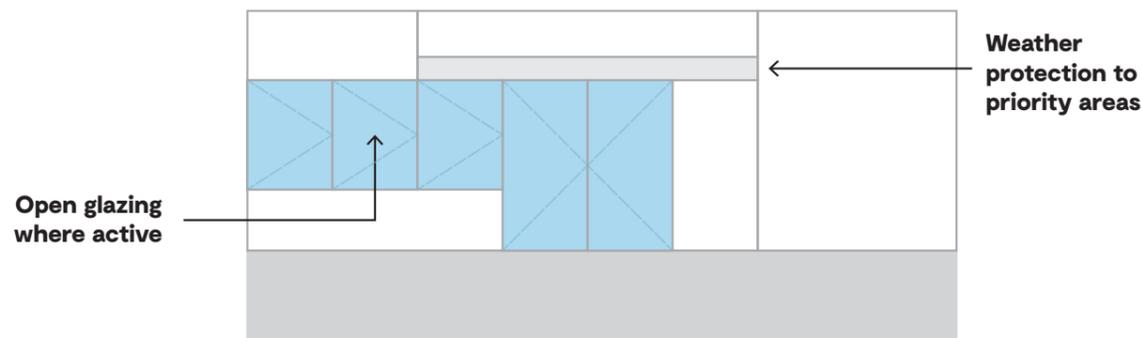


Figure 4 Ground floor interface

Active frontages



Semi-active frontages



2.2.5.6 Ground floor uses

Element objectives

- a. Ensure ground floor uses match the level of activity expected in the street.
- b. Where public areas are intended to be vibrant, ground floors include shops, cafés, or other uses that encourage people to interact with the street.
- c. On quieter streets, uses like residential entries, and offices are placed at ground level to suit the streets character and preserve amenity.

Development requirements

- a. Ground floor land uses are as per Table 8, according to the frontage type depicted on **Map 6 - Public domain interface**.
- b. Where a land use is not identified in Table 8, the use shall align with the Precinct vision, plan's objectives and sub-precinct objectives of this plan.

Table 8 Preferred ground floor land use

| Public domain interface | Preferred ground floor land use |
|-------------------------|---|
| Active | Shop, Restaurant/Cafe, Small bar, Tavern, Amusement parlour |
| Semi-Active | Art gallery, Child care premises, Consulting rooms, Educational establishment, Medical centre, Office, Recreation - Private |
| Passive | Residential dwellings, Aged or dependant persons dwellings, Residential buildings |

Design guidance and rationale

Ground floor uses play a key role in shaping vibrancy, character, and economic performance. In areas identified for high activity the inclusion of shops, cafés, and other interactive uses at ground level supports street activation and encourages foot traffic.

On passive streets or transitional areas, placing residential entries or offices at ground level helps maintain amenity and respects the local context.

This approach is supported by the Economic Development Strategy, which promotes flexible, place-based planning to support a mix of uses and employment opportunities across the precinct.

2.2.5.7 Vehicle access and movement

Desired outcomes

- a. Ensure vehicle access points work well with planned improvements to footpaths, streetscapes, and public spaces.
- b. Support a street layout that prioritises people over cars, helping create a more active and welcoming public realm.
- c. Maintain enough public parking to support local shops and businesses, without encouraging unnecessary private vehicle use.
- d. Make walking and cycling easier and safer by improving paths and facilities, helping people choose active travel.
- e. Work towards more sustainable travel patterns overall through provision of end-of-trip facilities, use of shared parking scheme and electric vehicle charging.

Development criteria

Vehicle access points

Acceptable Outcome A3.8.1 of SPP7.3 (Volume 2) is replaced to read as follows:

- a. Vehicle access points are:
 - i. Designed to integrate with potential public realm and movement upgrades identified on the **Map 9 - Public Realm and Landscape Plan**.
 - ii. Designed to minimise the appearance of vehicle entries to the public domain.
 - iii. Not located on a frontage designated as an 'Active' interface or 'Semi-Active' interface on Old Perth Road (refer **Map 6 - Public domain interface**), except where this is the sites only frontage to a public road.
 - iv. Positioned to provide separation from intersections and pedestrian/cycling crossing points.

Network improvements

- b. Improvements to the movement network are to be provided as depicted on **Map 7 - Movement Plan** and in accordance with the considerations identified in the Transport Impact Assessment and Parking Management Strategy.
- c. Where a movement network improvement is identified beyond the boundary of a lot, the development is to be designed to integrate with the planned improvement to the public realm.

Mid-block links

- d. Where identified on **Map 7 - Movement Plan**, development is to provide mid-block links generally in the locations shown. Links must maintain clear and legible access and be design to be universally accessible. Where a link is provided through a non-residential premise, it may be closed to the public outside of trading hours, where necessary to provide for safety and crime prevention.

Vehicle parking, charging and sharing

- e. Vehicle parking, including end-of-trip facilities, are to be provided in accordance with the R-Codes and any adopted local planning policy.
- f. Development shall be designed to allow for capacity for electric vehicle charging points, equivalent to a minimum of 20% of the total bays provided.
- g. Any development proposing greater than 50 dwellings is to incorporate parking bays for use as part of a residential car share scheme. The car share scheme shall operate in accordance with a management plan, applied as condition of approval.

- h. Despite any requirements of the local planning policy, development may under supply vehicle parking for non-residential uses, where:
 - i. Motorcycle parking, bicycle parking and end-of-trip facilities are over-supplied, and
 - ii. The development prioritises the use of non-private vehicle arrival (public transport, taxis, ride share, car share); and,
 - iii. The proponent can demonstrate, through Transport Impact Statement accompanying the development application, that long-term trends in movement will lead to reduced parking demand in the long-term.
 - iv. There is demonstrated capacity available in a nearby public parking facility.

Design guidance and rationale

The Bassendean Precinct Structure Plan supports a movement network that prioritises people over cars, aligning with the Town's Local Integrated Transport Strategy and the Town Centre Masterplan.

Vehicle access points must be carefully designed to integrate with planned upgrades to footpaths, streetscapes, and public spaces such as the proposed one-way conversion of Old Perth Road and pedestrian green links to BIC Reserve and Bassendean Oval. These changes aim to improve pedestrian safety, reduce vehicle dominance, and support a more active and welcoming public realm.

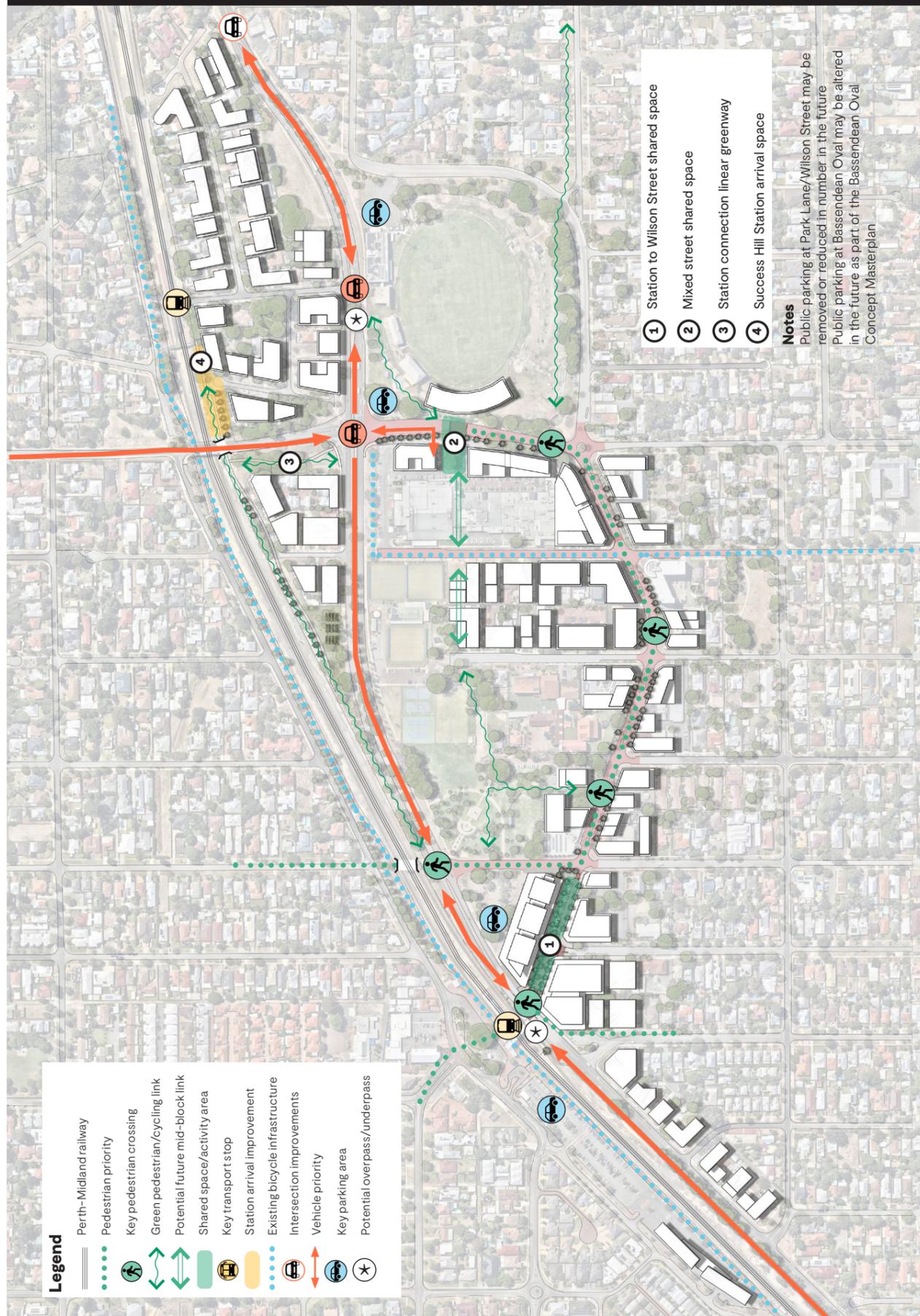
To support local businesses, the plan maintains sufficient public parking (over 500 on-street and public parking station bays available) while discouraging excessive private vehicle use through shared parking schemes, time restrictions, and end-of-trip facilities.

These strategies align with the Town's Local Planning Policy 8 - Car Parking and End-of-Trip Facilities, and promote mode shift toward walking, cycling, and public transport.

This Precinct Structure Plan targets a reduction in private vehicle trips from 78% to 65%, with increases in public transport (15%), walking/cycling (5%), and working from home (15%) by 2050. This supports the Perth and Peel @ 3.5 million urban consolidation strategy and ensures transport infrastructure evolves to meet future demand sustainably.



Map 7 Movement Plan



2.2.5.8 Design quality

Element objectives

- Buildings are designed as a whole, with all parts working together to create a strong and clear design.
- Materials, colours, and finishes reflect Bassendean's local character, history, and cultural identity.
- Buildings look good from the street and contribute to welcoming and attractive public spaces.

Table 9 Design quality

| Design principles | Desired outcome |
|------------------------------------|--|
| Context and character | Design is responsive to the features and qualities of the areas undulating topography and natural vegetation |
| Landscape quality | Demonstrates improvement of local environmental systems, flora and fauna is a priority. Delivers highly integrated, memorable public and private places that reflect a predominantly native landscape character, with exotic landscaping as appropriate. Landscaping plans should demonstrate full compliance with the Water Corporations Waterwise Development criteria |
| Built form and scale | Delivers a considered built form outcome that carefully and successfully negotiates between existing character and an intended future character |
| Functionality and building quality | Demonstrates functional benefits over the full life-cycle of the development by enhancing operational efficiency and minimising maintenance. Buildings and spaces are universally accessible and designed to reflect inclusionary principles. |
| Sustainability | Demonstrates that the sustainability of the built environment is a priority. Delivers environmental, social and economic outcomes that will assist to promote the identity of the local area |
| Amenity | Delivers spaces that are generous, welcoming and universally accessible |
| Legibility | Establishes a high degree of implicit legibility with direct connections between spaces and local focal points through built form and landscape design, without reliance upon active mechanisms such as signage systems |
| Safety | Establishes a high degree of implicit safety through built form and landscape design. Consolidates night-time activation areas to avoid land use conflict and provide for safe use. Provides for safety through avoidance of enclosure or entrapment, passive overlooking and digital surveillance. |
| Community | Offers an inclusive and equitable response to local community needs and broader social context |
| Aesthetics | Results in a sophisticated and coherent design solution at all scales Aligns with the existing character of materiality, form, tones and colours of the built environment within Bassendean. |

2.2.5.9 Public art

Element objectives

- a. Public art helps tell Bassendean's story and celebrates its cultural identity.
- b. Art and cultural activities make places feel more welcoming, interesting, and connected to the community.
- c. Public art is part of the overall design of buildings and spaces, not just added on later.

Development requirements

- a. Public art contributions are to be made in accordance with an adopted policy of the Town of Bassendean.
- b. Contributions apply in addition to any contribution required for public open space, vehicle parking, or public realm upgrade.
- c. All contributions are to be designed and implemented consistent with any policy, guideline or strategy of the Town relating to public art and appropriately address principles relating to sustainability, cultural heritage, access and inclusion.

Design guidance and rationale

New development in Bassendean provides a valuable opportunity to deliver integrated public art that aligns with the Town's Public Art Policy and Arts, Culture and Events Strategy (2023–2026), by enriching local identity and supporting creative industries. Projects should endeavour to involve collaboration with local artists, reflect cultural heritage and celebrate community stories.



2.2.5.10 Heritage places

Element objectives

- a. New buildings don't overwhelm heritage places.
- b. The size, form, and materials of new buildings are respectful of nearby heritage qualities.
- c. Re-use and adaptation of heritage places is encouraged.
- d. Works recognise the potential to disturb Aboriginal cultural values that may be present in the precinct.

Development requirements

- a. For development on or adjoining a heritage place, the local government may require greater setbacks to protect the visual significance and integrity of the heritage place.
- b. The siting and design of buildings adjoining a heritage place should respect its visual significance and integrity, ensuring the new building does not overwhelm or adversely affect the heritage place, considering design, size, scale, materials, setbacks, and proportion, especially from the street view.
- c. New development involving additions or alterations to a heritage place, or on a site containing or adjoining a heritage place, must include a Heritage Impact Statement justifying the appropriateness of the new development.
- d. Where a development is proposed within any registered Aboriginal site, advice should be sought from the Department of Planning, Lands and Heritage to understand obligations under the *Aboriginal Heritage Act 1972*.
- e. Where development is proposed on land within the actual boundary of an Aboriginal cultural heritage place that proposes disturbance works, an Aboriginal cultural heritage management plan is to be submitted with the development application.

Design guidance and rationale

New development in Bassendean presents an opportunity to conserve and celebrate local heritage by adapting sensitively to its historic context, in line with the Town's Heritage and Character Local Planning Policy and Local Heritage List. The siting and design of adjoining developments should respect the scale, materials, and proportions of heritage buildings, maintaining their prominence and integrity. These measures help preserve Bassendean's unique character and ensure that growth is sensitive to its cultural context.

2.2.5.11 Tree canopy and deep-soil zones

Element objectives

- a. Keep mature trees where possible to protect shade, habitat, and local character.
- b. Buildings and infrastructure work around existing trees, especially in residential areas.
- c. Replace removed trees with suitable species that will grow large and provide canopy.
- d. Encourage mixed-use developments to include trees through early planning and flexible design.

Development requirements

- a. Development applications impacting regulated trees within the Precinct shall be assessed in accordance with the Town's Local Planning Policy 13 – Tree retention provision, and where impacting street trees the Town's Street and Reserve Trees Council Policy.
- b. In the Centre zone, the provision of deep-soil areas and tree retention may be reduced where:
 - i. The place is a heritage protected place; and
 - ii. The retention of trees, or provision of deep-soil areas would be detrimental to the protection of cultural heritage.
- c. In determining a development application, decision makers may vary any criteria of this Precinct Structure Plan and the provisions of the R-Codes where:
 - i. A regulated tree is proposed to be retained in perpetuity; and,
 - ii. The extent of variation to the criteria of this Precinct Structure Plan is commensurate to restriction on development otherwise permitted that is a direct result of the retention of a regulated tree; and,
 - iii. The extent of variation to criteria remains consistent with objectives of this Precinct Structure Plan, element objectives or performance criteria of the R-Codes, and the guidance set out in Town's Local Planning Strategy.

Design guidance and rationale

Growing tree canopy is a key objective of the Town of Bassendean, reflecting strong community support for protecting mature trees, enhancing canopy cover, and providing habitat. Council policies recognise that trees provide critical environmental, health, and amenity benefits including cooling urban areas, improving air and water quality, supporting biodiversity, and contributing to neighbourhood character. This plan is supported by an Environmental Assessment which outlines that the Precinct lacks sufficient tree canopy. The Town's Street and Reserve Trees Policy sets a target of 30% canopy cover by 2040 and promotes tree planting in public spaces to achieve a continuous canopy wherever possible. Continued planting, protection of existing mature trees and provision of more street trees will green the Precinct over time.

2.2.5.12 Sustainable development

Element objectives

- a. Design buildings to use less energy and water.
- b. Include features like solar panels, rainwater tanks, and electric vehicle chargers.
- c. Protect natural areas and increase tree canopy to cool the town.
- d. Support development that reduces carbon emissions and helps fight climate change.

Development requirements

- a. Buildings are designed to include water fittings and appliances within one level of the highest level available under the Water Efficiency Labelling and Standards (WELS) system and include infrastructure to harvest, retain and utilise rainwater and greywater.
- b. New development with a gross floor area greater than 3,000 sqm is to achieve at least a 5 Star Green Star rating under the Green Building Council of Australia rating system, or an equivalent rating system. Applications are to be supported by a Green Star Design Review certificate, or a report prepared by a suitably qualified person providing evidence of an equivalent rating.

Design guidance and rationale

The Town's LPP2 – Sustainable Development encourages buildings that reduce energy and water use, improve indoor comfort, and support climate resilience. These goals align with Green Star standards from the Green Building Council of Australia, which promote efficient design, renewable energy use, and low-emission materials. By integrating features like solar panels, rainwater tanks, and EV chargers, developments contribute to Bassendean's sustainability targets and help reduce carbon emissions. Importantly, tools like Green Star consider a wider range of sustainability factors than energy and water, extending to construction waste, sustainable transport and overall community well-being.



2.2.5.13 Environmental resource management

Element objectives

- a. Storm water discharge and quality is managed to reduce pressure on infrastructure networks
- b. Storm water is managed on-site through retention and infiltration
- c. Impacts to groundwater are minimised
- d. New development considers impacts to other environmental resources.

Storm water and groundwater

- a. Development and subdivision shall be designed considering the relevant requirements of the Better Urban Water Management Guidelines, or any successor document.
- b. Applications for major new developments should be supported by reporting demonstrating that stormwater flows will be reduced compared to pre-development levels.
- c. Development proposing basements are supported by investigations that demonstrate works intersecting with groundwater areas will not adversely impact water quality or distribution.

Acid sulfate soils

- d. Development proposing works at depths with the potential to intersect with acid-sulfate soils are supported by an acid sulfate soils management plan.

Design guidance and rationale

The Town's Local Planning Policy No. 14 – Stormwater and water sensitive urban design principles promote keeping stormwater on-site through retention, infiltration, and reuse. This helps prevent pollutants from entering the Swan River and other waterways, supports groundwater recharge, and reduces pressure on public drainage systems.

Relatively shallow groundwater conditions in some parts of the Precinct have the potential to add complexity to basement construction. Applications should be supported by investigations that demonstrate works intersecting with groundwater areas will not adverse impact water quality or distribution.

2.2.5.14 Passive surveillance and safety

Element objectives

- a. The design of publicly accessible spaces provides opportunities for passive surveillance
- b. Lighting design supports good surveillance, raises perceptions of safety and deters offending.

Development requirements

Buildings are designed to minimise opportunities for crime and concealment through the following measures:

- a. Entries incorporate crime prevention through environmental design principles including being direct and clearly visible and well-lit. Lighting and design elements that ensure entries are legible and safe to access at night.
- b. Frontage areas are design to minimise opportunities for places of concealment.
- c. Passive surveillance is provided to street, open space and communal areas by upper floor balconies, major openings and other outdoor living areas.
- d. Major new developments are supported by a Crime Prevention through Environment Design Assessment in accordance with guidelines published by the Western Australian Planning Commission.

Design guidance and rationale

Designing public spaces with passive surveillance, such as windows, active frontages, and clear sightlines, helps deter crime and makes people feel safer. Good lighting further supports visibility, reduces hiding spots, and improves perceptions of safety, especially at night. These measures align with Bassendean's planning goals for a safe, inclusive, and well-used public realm.



2.3 Development incentives for community benefit

Where development exceeds the maximum height limit of this Precinct Structure Plan, community benefits are to be provided consistent with this section.

Benefits required

Development that exceeds the maximum building depicted on **Map 3 - Building height** may be approved where all the following criteria are met:

- The site is depicted as having additional building height on **Map 8 - Additional building height**.
- Benefits are provided as set out in Table 10. Development shall provide benefits that achieve a combined benefit weighting value of at least 1.0, except in the case of Lot 2 West Road, which shall exceed 1.50. Delivery of benefits may be staged to reflect the incremental redevelopment of Lot 2. Staging of benefits should be included in a Local Development Plan for the lot.
- The benefits are offered voluntarily by the proponent.
- The overall building height is not more than the additional building height limit.

Design and provision of benefits

- Benefits are provided in accordance with this Precinct Structure Plan or any relevant policy, masterplan (or equivalent), strategy or guidelines endorsed by the Town of Bassendean. This includes considerations of disability, access and inclusion, conservation of cultural heritage values and public art.

Justification for benefits

- For the 'Public parking' and 'Cultural facilities' benefits, the development application is to be supported by a 'Needs Assessment' which demonstrate how the benefit assists in satisfying the social, cultural, recreational or environmental needs of the community. In considering whether adequate need is established, consideration is given to whether the benefit may be provided as part of other developments.

Provision of monetary contributions, in lieu of benefits

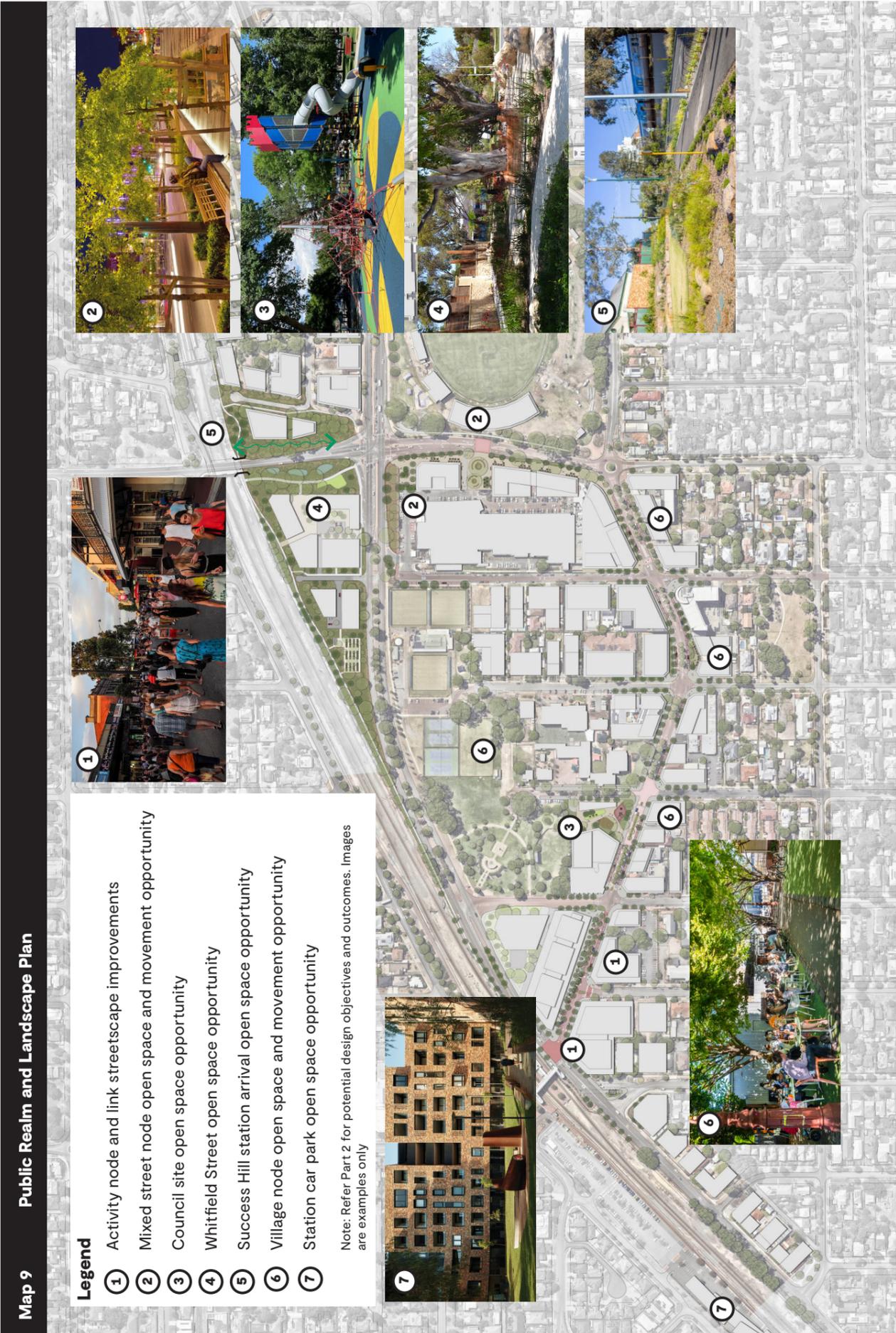
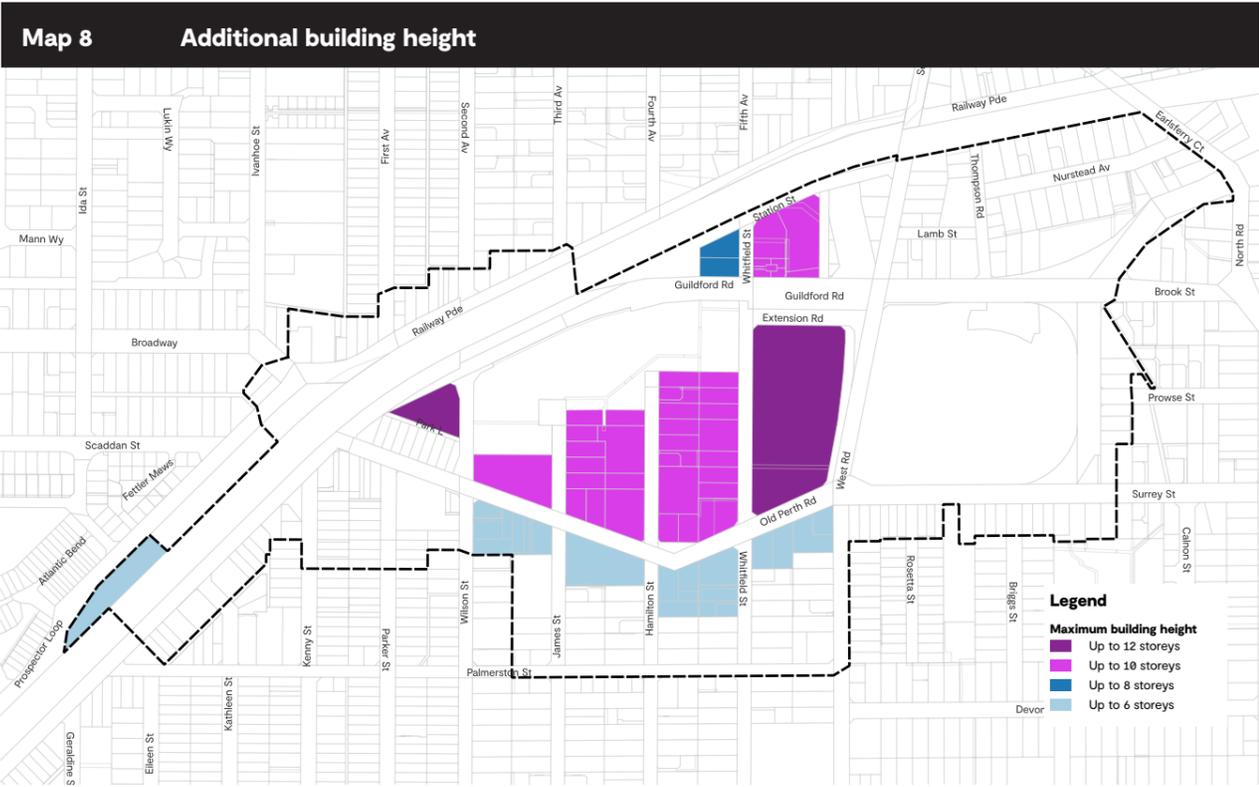
- A monetary contribution may be made in lieu of providing benefits.
- The method for calculating monetary contributions, in lieu of providing benefits is to be set out in a local planning policy, consistent with the provisions of State Planning Policy 3.6 - Developer Contributions, adopted by the Town.
- Where a monetary contribution is proposed to be made, conditions shall be applied to an approval ensuring payment is made accordance with the adopted local planning policy.
- Monetary contributions are to be paid to the local government, held in trust, before expended for the purpose of the benefit.
- Proponents may seek to enter into a deed of agreement with the Town where delivery of benefit items relates to land owned and/or managed by the Town.

Design guidance and rationale

The benefits system supports strategic growth by ensuring developments that exceed height limits contribute meaningfully to the community. It aligns with the Town of Bassendean's vision for a vibrant town centre and complies with State Planning Policy 3.6 by promoting a fair and transparent framework that enables developers to voluntarily contribute to improved public spaces and facilities. Benefits may be provided either in-kind or through monetary equivalents. The height limits (Map 3) enables this plan to meet the minimum targets of the Local Planning Strategy. Additional height allowances offer greater growth potential, appropriately linked to voluntary and commensurate community benefits.

Table 10 Community benefits for development incentives

| Benefit | Criteria | Benefit weighting value |
|---------------------------------|--|-------------------------|
| Public realm improvement | An in-kind contribution is made to a public realm upgrade directly adjacent to the site, in accordance with Map 9 - Public Realm and Landscape Plan . | 1.0 |
| Public plaza | Development provides for hard and soft landscaping of a public plaza. The plaza shall be designed to provide adequate shade, seating, landscape and areas of play/activity that complement the adjacent activity. | 1.0 |
| Mid-block link | Permanent, legible, safe and universally accessible pedestrian access is provided through the site in the location(s) indicated on Map 7 - Movement Plan . | 0.75 |
| Conservation of heritage values | A place with identified cultural heritage value, excluding heritage-protected places, that is worthy of conservation is retained and integrated into the development. | 0.75 |
| Cultural facilities | Provision of cultural facilities (cultural, art spaces or galleries) guided by demonstrated demand for such facilities, or provided in accordance with a social infrastructure plan of the Town. Cultural facilities include live performance spaces, light industrial creative production spaces, artist studios, photography and film production facilities, and other creative industry workspaces. | 0.75 |
| Affordable housing | Provision of affordable dwellings (minimum 10% of total number of dwellings) in partnership with an approved housing provider or not-for-profit organisation. | 0.75 |
| Sustainable building | Development meets or exceeds a 5 Star Green Star design rating under the Green Building Council of Australia, or an equivalent rating system as agreed by the Local Government | 0.50 |
| Public parking facility | A public parking facility that provides for parking for land use on other sites in the surrounding area. | 0.50 |
| Waste management | The development provides a communal waste storage/collection facility that provides for all waste management of the site and at least one other site within the Precinct Structure Plan area | 0.25 |
| End-of-trip facilities | Provision of public bicycle parking and end of trip facilities in addition to the requirements of the relevant local planning policy for the users of the building and members of the public who are not tenants/users. This includes provision of secure bicycle parking and drying rooms. | 0.25 |
| Public facilities | Provision of public facilities (play space, public toilets, change rooms) guided by demonstrated demand for such facilities. | 0.25 |



2.4 Additional details

2.4.1 Information required for development application

The following studies and plans may be required by the local government to provide certainty in considering and managing key issues associated with a proposed development, and to ensure high quality development within the Precinct:

- Design report, setting out how the development addresses State Planning Policy 7.0 - Design of the Built Environment, together with any design criteria described in this plan.
- Where existing mature tree(s) are proposed to be removed, a report from an arborist with at least an Australian Qualifications Framework Level 5 qualification.
- A Transport Impact Statement, or Transport Impact Assessment consistent with the WAPC's Transport Impact Assessment Guidelines, including an assessment of capacity available in surrounding public transport network.
- A Landscape Plan prepared by a landscape designer that includes design rationale, materials for hard and soft landscaping, species list, irrigation plan and achievement of water-wise design principles.
- Where required by this plan, a Green Star Design Review certificate or equivalent report, prepared by a suitably qualified person.
- Details of proposed public art contribution(s) in accordance with the Town's Local Planning Policy.
- Where a development has the potential to impact groundwater or surface water management, a preliminary Water Management Plan.
- Waste Management Plan, where applicable.

2.4.2 Terms used

Affordable means workspace or housing provided in perpetuity at rents maintained at least 20% below the current market rate.

Car share scheme means a system where privately managed vehicles are available for short-term use by building tenants.

Cultural facilities means a space, building or structure used for cultural activities and the creative arts, and includes but is not limited to live performance spaces, light industrial creative production spaces, artist studios, photography and film production facilities, and other creative industry workspaces.

Public realm upgrade means improvements to the public domain in the form of:

- Repaving, kerbing and landscaping within the streetscape
- Implementation of raised pedestrian thresholds, intersections, line markings and crossing areas
- Creation of open space areas, by way of kerb extensions, footpaths, areas of landscaping and hardscaping. This includes upgrades to existing un-constructed spaces forming parts of road reserves.
- Installation of seating, lighting, play equipment and other forms of hardscaping.
- Necessary civil infrastructure upgrades, including upgrades to drainage systems.



Part 2 | Explanatory

This Part explains the factors informing the Precinct Structure Plan, detailing how the provisions of the plan will deliver the plans objectives.



1. Introduction

This Precinct Structure Plan has been prepared to guide development of the Bassendean Precinct. It has been prepared by the Town of Bassendean in accordance with the WAPC's Guidance for Structure Plans issued in August 2023 and having regard to the design guidance outlined in SPP7.2 – Precinct Design and SPP7.0 – Design of the Built Environment.

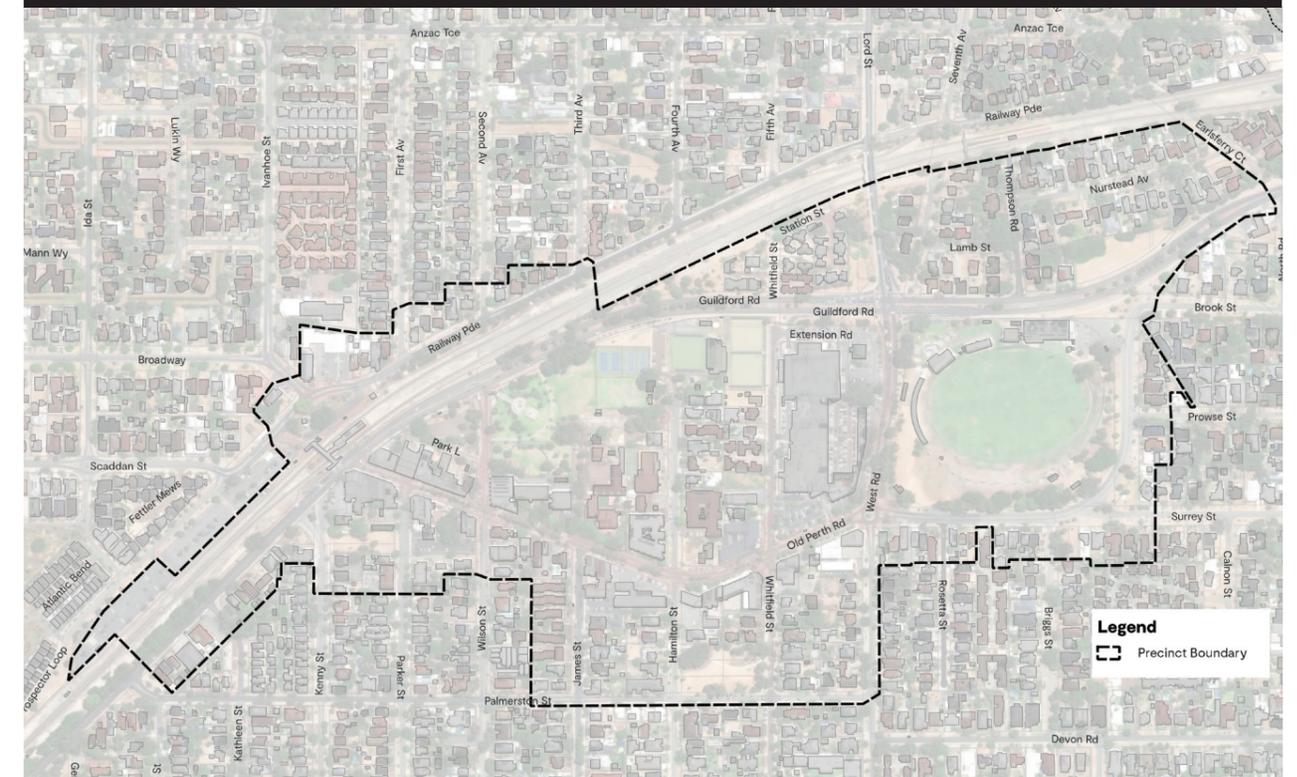
The purpose of the Precinct Structure Plan is to facilitate and maximise the redevelopment and future growth of the Bassendean Precinct as a key destination where people chose to live, work, and recreate.

The Precinct Structure Plan will ultimately facilitate the vision created by both public and private stakeholders within the local community for the Bassendean Precinct.

1.1 Precinct Structure Plan area

The Bassendean Precinct Structure Plan area incorporates the land surrounding the Bassendean station and Success Hill station, focusing on land within a modified walkable catchment set out in the Town's Local Planning Strategy and Town Centre Masterplan.

Figure 1 Precinct Structure Plan area



2. Site and physical analysis

2.1 Physical context

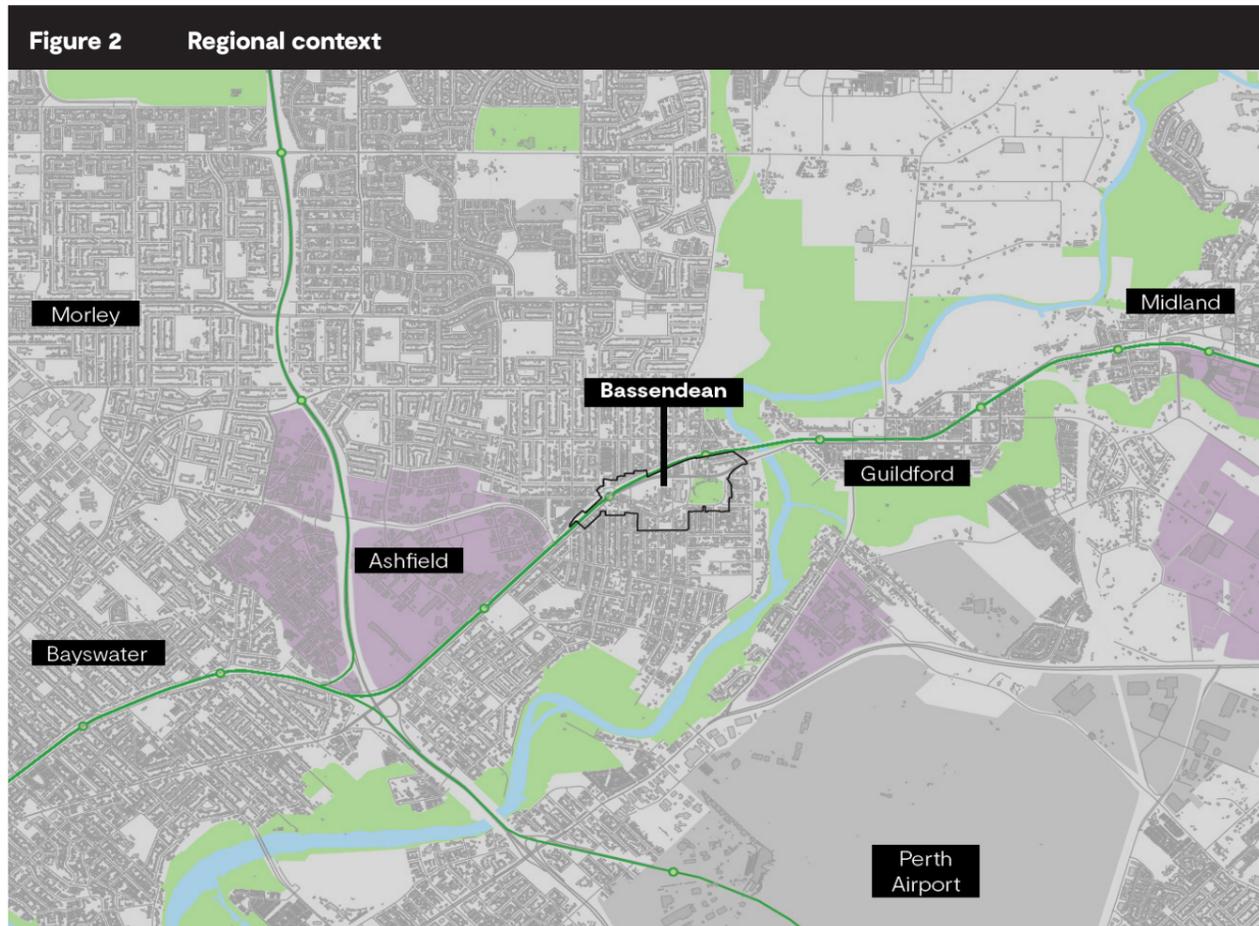
The Precinct's built condition and underlying ownership is a key influence in the scale and future development. These factors have been considered in the development of this Precinct Structure Plan. Detailed exploration of these issues is considered in the context of the precinct's urban structure, urban ecological condition, transport connections, and built-form outcomes, described in Part 3 - Opportunities and constraints.

2.2 Regional location

The Bassendean Precinct is positioned in Perth's eastern suburbs, forming part of a series of connected centres along the Perth-Midland railway line. Connected to major centres like Maylands, Bayswater and Midland, the Bassendean Precinct plays an important role in connecting residents of Bassendean, Ashfield and Success Hill to wider transport networks, areas of employment and activity and natural amenity.

The Bassendean Precinct is well located to enable current and future residents to leverage employment opportunities at major employment hubs like Ashfield, Bayswater, Midland and Morley. The Swan River significantly curtails the centres catchment to the south and to a lesser extent to the east, dividing the Bassendean Precinct from otherwise nearby locations such as Redcliffe/Perth Airport, Hazelmere, Guildford and the lower Swan Valley.

The precincts' location approximately 10km from Perth CBD positions Bassendean well to leverage the opportunities from a growing population, growing employment and activity.



| | |
|---------------|---|
| Midland | <p>Midland is the major sub-regional activity centre in eastern Perth. It serves a vast area of several hundred thousand people, providing a broad mix of retail and commercial activity, industrial employment, major education and health institutions and mid-rise, higher density residential living.</p> <p>Land use change in the Midland activity centre has considerable potential to influence land use outcomes for Bassendean. Recent and planned growth at St John of God Public Hospital Midland, expansion to Midland Gate shopping centre, relocation of Midland railway station are major projects that will influence land use and movement patterns in Bassendean. The co-location of these major facilities has the potential to attract a greater share of strategic office and mixed commercial employment and draw activity away from Bassendean.</p> |
| Morley | <p>Morley Galleria shopping centre and the surrounding mixed use precinct provide considerable retail and commercial floorspace to the wider north-east corridor. The centre is considerably larger than Bassendean, offering 98,000sqm of shop land use and over 115,000 of mixed commercial floorspace. Planned long-term expansion of the centre could add upward of 50,000sqm of additional retail and commercial floorspace. The recently opened Morley station – part of the METRONET Morley/Ellenbrook line – provides improved connection between the activity centre and the wider region.</p> |
| Bayswater | <p>The Bayswater Activity Centre has emerged as one of Perth's most well connected, outside of the central city. The convergence of the Forrestfield/Airport line and Morley/Ellenbrook line, together with the existing Midland line provide for considerable centre access.</p> <p>Like Bassendean, Bayswater offers a main street style commercial and retail precinct, supported by a mix of entertainment, community/social and light industrial uses. These diverse uses and excellent transport connectivity are supporting a growing local residential population.</p> |
| Perth Airport | <p>Perth Airport is identified as a Specialised Centre (Aviation and logistics) in Perth and Peel @3.5million. The Perth Airport Masterplan anticipates employment growth within the airport estate to grown from 9,900 jobs in 2018 to 18,700 jobs by 2040; with the vast majority of these jobs being in non-aviation sectors. Significant freight and passenger movements, together with increased logistics and mixed business activity will capture considerable consumer expenditure, employment opportunities, and dramatically change movement dynamics. Bassendean's position within Perth Airports employment region presents opportunities – albeit minor – for increased employment in warehousing, non-strategic office and accommodation services.</p> |

2.1.2 Local context

The Bassendean Precinct is positioned at significant transport junctions, including the Perth–Midland railway and Guildford Road and West Road/Lord Street. These connections provide regional connectivity, but also provide direction to local movements around the precinct. Movement characteristics are directional focal points of activity, such as existing railway stations, Bassendean shopping centre and Bassendean Oval.

Residential streets, containing lower density single residential dwellings frame the southern interface of the precinct. Bassendean Primary School on Palmerston Street provides a community focal point for the residential neighbourhood. These streets provide strong directional local movement towards the Precinct. To the east, a short journey along Surrey Street provides access to the Swan River at Point Reserve. Over the Swan River is Guildford, though access is limited to a single bridge at Guildford Road.

To the north of the Perth–Midland railway, suburban streets in Eden Hill consist of mixed single and grouped dwelling developments of the later twentieth century. Adjacent is the significant industrial area at Ashfield, which provides a range of employment opportunities across service industries, manufacturing, storage and other industrial processing activities.

2.1.3 Tenure, ownership

Land ownership in the precinct is fragmented, with many small lots owned independently or co-owned by multiple entities, making land acquisition and development more complex. This fragmentation, while challenging for development, contributes to diversity in building form, quality and land use – contributing to the precinct’s unique character. Conversely, several large sites under single ownership exist. The Town of Bassendean and the State of Western Australia are also prominent landowners in the precinct, with key assets such as BIC Reserve and Bassendean Oval.

Table 1 Large landholdings within the precinct

| Location | Description | Owner |
|-------------------|--------------------------------------|------------------------------|
| 2 West Road | Bassendean shopping centre | Hawaiian Investments Pty Ltd |
| 46 Old Perth Road | Town Administration and Library | State of Western Australia |
| 1 Park Lane | Crown lot used for vehicle parking | State of Western Australia |
| 47 Old Perth Road | R&I Rossi Bassendean shopping centre | Private |

2.1.4 Built form and character

Bassendean’s character is shaped by various building types, including a number of heritage-listed structures. Most buildings are low-rise ranging from 1–2 storeys, with the tallest reaching 6 storeys. Current planning controls allow development in accordance with the Town Centre Masterplan which contemplated varying levels of development density and maximum building heights generally up to 6 storeys, with specific sites permitted up to 10 storeys.



Finer-grain shopfronts which enhance the precinct’s character with their architectural diversity and active presence, featuring consistent street walls, awnings, and detailed frontages and glazing.



Large-format commercial buildings which support economic activity by accommodating large retailers but can negatively impact the streetscape with prominent car parking and inactive shopfronts.



Mixed-use apartment buildings that blend commercial and residential uses, which match the area’s scale and complement local character in terms of materials, scale and form.



Away from Old Perth Road and in the Success Hill Frame, single and two storey dwellings are a mix of building styles and age, with varying landscape character, design forms and bulk.



The built form, land use functions and movement patterns of the precinct have been heavily influenced by the Perth–Midland railway.



Just beyond Old Perth Road sits a variety of generous public spaces serving a variety of recreational and passive functions.

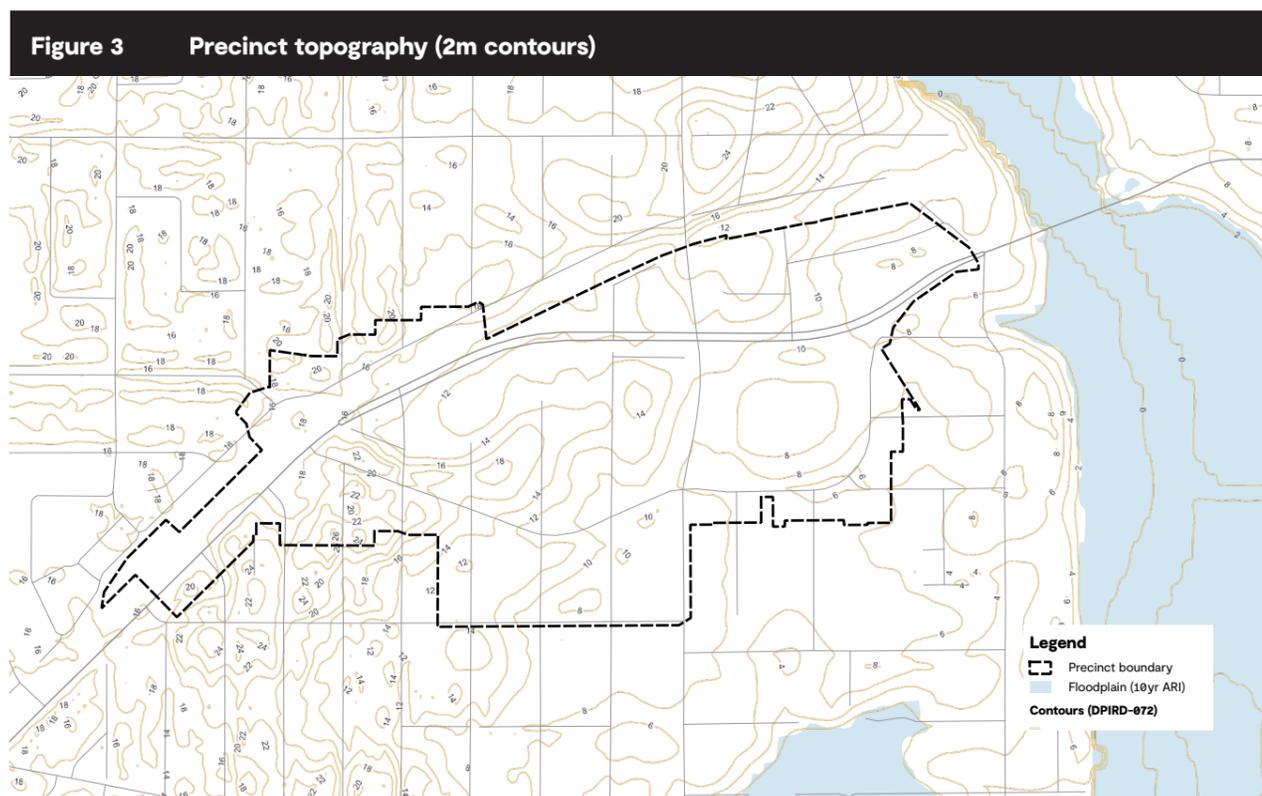
2.2 Environmental context

The environmental context of Bassendean, encompassing the public realm, climate, land form, biodiversity, and local water cycle, significantly influences the potential for future development. Analysis and consideration of these factors have informed the development of this Precinct Structure Plan. This discussion is informed by the findings of the Environmental Assessment, Biodiversity Strategy and Flora Management Plan and the Local Water Management Strategy.

2.2.1 Topography

Topography within the Precinct generally ranges from 10 to 25 m Australian Height Datum (AHD), generally sloping east towards the Swan River.

A significant fall in topography occurs along Wilson Street, between Old Perth Road and Guildford Road. Other low-lying areas occur at BIC Reserve, Bassendean Oval and Whitfield Street, north of Guildford Road. The lower topographic levels at Wilson Street and Whitfield Street have influenced provisions of this plan that enable greater building height.



2.2.2 Soils

Table 2 Soil classifications

| Soil type | Description | Land use considerations |
|---------------------------------|---|--|
| Pale deep sand (Soil Group 444) | Sand >80 cm deep with white, grey or pale yellow topsoil. | <ul style="list-style-type: none"> — Poor fertility and water holding — Prone to wind erosion — Groundwater recharge and nutrient leaching issues — Neutral to acid pH |
| Semi-wet soil (Soil Group 103) | Dark grey to black organically stained sands, waterlogged to 30-80 cm for a major part of the year. | <ul style="list-style-type: none"> — Water logging and seasonal inundation — Artificial draining may be an option — Often good summer moisture — Acidic to neutral pH |

There are two key soil types within the Precinct, described in Table 2. These soil types extend over large areas of the Swan Coastal Plain, particularly in proximity to the Swan River, including nearby areas of Bayswater and Belmont, indicating that the identified soil types can support a range of development forms.

The generally sandy conditions present opportunities for basement construction. However the relatively shallow water table may impact feasibility in eastern portions of the Precinct.

2.2.3 Bushland and native vegetation

There are no Bush Forever sites within or directly adjacent to the Precinct. Bush Forever Sites No. 305 and No. 491 exist less than 1km from the Precinct boundary but are unlikely to be impacted by development within the Precinct area. The pre-European vegetation complexes within the precinct are likely to be as follows:

- Swan Complex: comprises fringing woodlands of Flooded Gum and Freshwater Paperbark with localised occurrences of Swamp Sheoak and Salt-water Paperbark areas of higher salinity. This vegetation occurred along the Swan River Foreshore and was originally approximately 24% of the Town's pre-European vegetation.
- Bassendean Complex-Central and South: occurred on the upland areas and where it still occurs is typified by Banksia Low Open Woodland dominated by Candlestick Banksia and Firewood Banksia, with scattered Christmas tree, Pricklybark and open shrubby under story, with Dryandra and Xanthorrhoea. This was originally the most extensive vegetation type in Bassendean, covering approximately 67% of the Town.

Most native vegetation within the Town has been cleared, due to its long history of development. Native vegetation remaining within the Precinct is limited to scattered trees and small pockets within public open space areas.

2.2.4 Significant trees and urban forest

The Town has a relatively low canopy cover of 15.7%. To 2020, an increase was observed over all land types with street blocks increasing from 11 to 13%, roads from 11 to 15% and parks from 19 to 25%. The majority of canopy cover is located within parks and recreation reserves (36%) and residential zones (36%). Very little tree canopy is located on land zoned for commercial/retail purposes.

Like the rest of the Town, the majority of mature canopy is located on public open space areas (BIC Reserve, Bassendean Oval) and in lower density residential streets. Limited canopy is provided on key redevelopment sites, such as Bassendean Hawaiians shopping centre.

2.2.6 Significant flora and fauna

A search of the DBCA's Threatened and Priority Flora database (DBCA-036), identified that there are no threatened or priority flora recorded within the Precinct.

A search of the DBCA's Threatened and Priority Fauna database (DBCA-037) identified two recordings of threatened fauna (bird) within the precinct and one recording of specially protected fauna (migratory bird). The database recordings are not always locationally accurate, and to confirm the presence or absence of fauna, on-ground surveys are required.

The edge of a known roost site for the Carnaby's Black Cockatoo intersects with the eastern edge of the Precinct. Given their value to roosting habitat, trees within and in proximity to this site should be retained where possible.

2.2.7 Climate

The climate within the Precinct is typically classified by hot, dry summers and mild, wet winters. The winds are strongest during summer with 51% of winds in December exceeding 20km/hr at 1500 hours compared with only 20% in May and 25% in June, July, and August. The typical summer pattern is strong easterly winds in the morning swinging to a strong south-westerly wind or sea breeze in the afternoon.

2.2.8 Climate change

The Town recognises that the climate is changing as seen in a decline in annual rainfall, increase in extreme weather events such as prolonged heat waves and severity of storms. It is important that community and Council assets and infrastructure are resilient to changes in climate. Climate resilience should be considered where possible in the provisions. Actions may include sustainable building design, water/biodiversity sensitive urban design and increased urban canopy cover.

2.2.9 Ground water

The Town of Bassendean includes part of the south-eastern edge of the Gnangara Groundwater Mound. The Gnangara Mound is a significant water resource for maintaining natural systems, such as wetlands and groundwater dependent vegetation, and for human use, such as public water supply.

The DWER Perth Groundwater Map indicates that the depth to groundwater is typically between 5 m to 15 m below ground level (bgl), depending on the location within the Precinct and the local surface elevation.

2.2.10 Surface water

The Precinct is not located within the DWER Flood Extent and Floodplain Area. However, areas of to the east of the Precinct are affected by the 1 in 10 year (10% AEP) flood extent and other designated flooding events.

Effective management of storm water is essential to prevent flooding and the pollution of nearby water bodies. Within the Town, a majority of storm water directly infiltrates into the soil. Storm water resulting from more developed, impervious areas is directed to a network of piped and/ or open drains to outlets that discharge into the Swan River. Section 2.3.10 describes the current issues with the storm water drainage network and potential longer-term implications as the Precinct develops.

2.2.11 Bushfire risk

There are no areas of the Precinct designated bush fire prone in accordance with State Planning Policy 3.7 – Planning in Bushfire Prone Areas.

2.2.13 Contamination

A search of the DWER Contaminated Sites Database identifies five (5) contaminated sites within the Precinct. It is understood that the database does not include reported sites, including those awaiting classification or classified as Potentially contaminated – Investigation required. Where contaminated sites are known to exist, or are identified during construction activities, these should be managed in accordance with the Contaminated Sites Act 2003 (WA). An Opportunity site at 1 Park Lane is identified as 'Contaminated – restricted use'.

2.2.12 Water conservation

Better management of storm water has the potential to reduce the use of potable water within the precinct. Further opportunities for non-potable use identified in the Local Water Management Strategy include:

- Installation of infrastructure to harvest and store storm water runoff from roof and impervious surfaces.
- Installation of grey water recycling systems.
- Use of alternative fit for purpose water sources for both internal non-potable uses (toilet flushing, laundry, irrigation, and potentially cooling towers) and external irrigation.
- Minimised use of ongoing pumping of groundwater ingress and de-water disposal over the life of the building.



Figure 4 Wetlands and floodplains

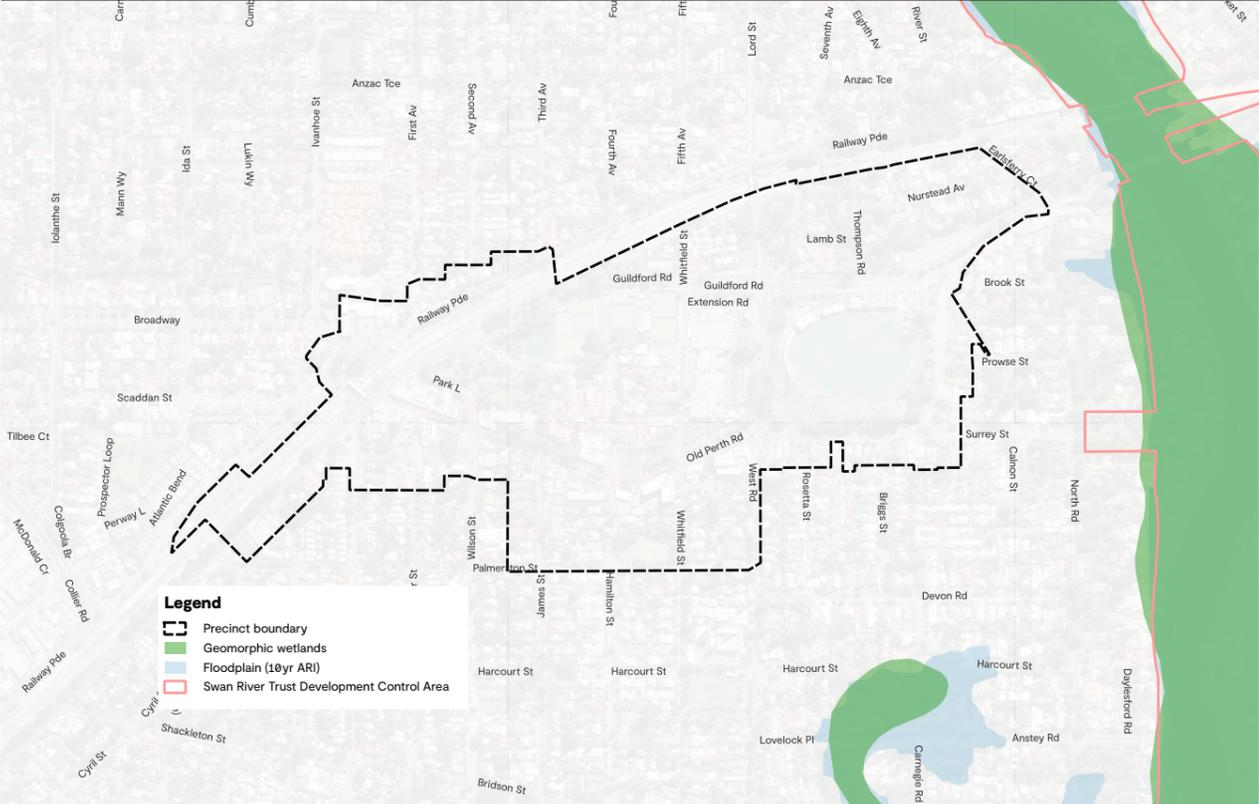
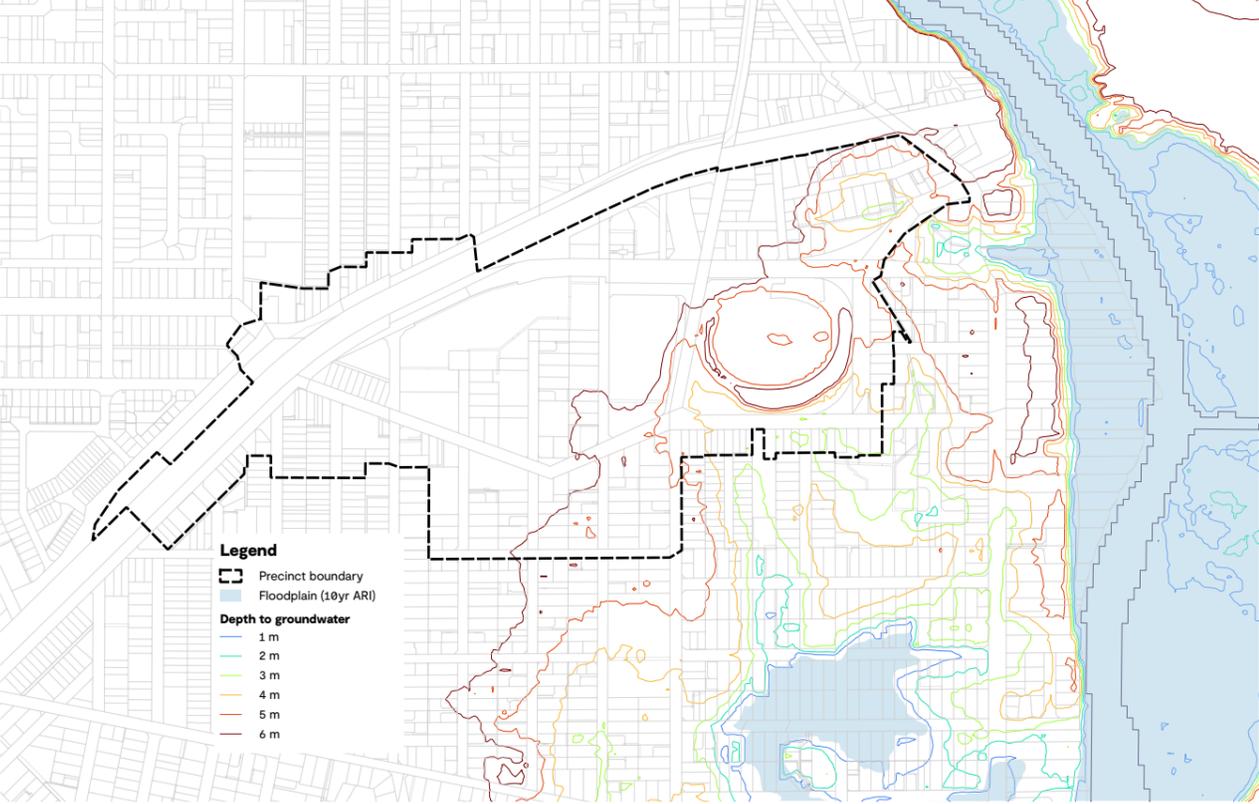


Figure 5 Depth to groundwater



2.3 Community context

The community context of the Precinct identifies several key points of guidance that have been used to define floorspace composition, provision of diverse land use and movement outcomes.

2.3.1 People and growth

The Town exhibits a population that is ageing faster than that of Western Australia as a whole. The community context of the Precinct identifies several key points of guidance that have been used to define floorspace composition, provision of diverse land use and movement outcomes. Western Australia Tomorrow – Population Report No. 11 (2016 to 2031) contains the latest population forecasts by age and sex, for Western Australia and its regions. It forecasts that the population of the Town in 2031 will be between 12,600 and 15,800, with the median growth scenario being a population of 14,170. This represents an overall decline in the Town's population.

This contrasts with the Central Sub-Regional Planning Framework, which estimates a total population of 24,300 within the Town by 2050. This divergence suggests that land use change will be the most significant driver of population change in the Town.

Table 3 Forecast population growth (2016–2031)

| Area | 2016 | 2021 | 2026 | 2031 |
|--------------------|--------|--------|--------|--------|
| Town of Bassendean | 15,555 | 15,030 | 14,660 | 14,170 |

There are an estimated 557 dwellings within the precinct at present. The provisions of this Precinct Structure Plan are estimated to generate opportunities for 1,174 additional dwellings by 2050. Of these, approximately 90% are expected to be multiple dwellings, and 10% medium density town houses or single house development. This plan also assumes development occurring on the Bassendean Oval Opportunity Site, resulting in a further 197 dwellings. The total dwelling yield of this plan is therefore 1,927 dwellings.

On the basis of an average dwelling occupancy of 2.1 persons per household, the total expected population within the precinct is 4,048 people.

2.3.2 Age and family composition

The Town has an older population compared to Greater Perth. 28% of residents are aged 10–34 and 24% are aged 60+, compared to Greater Perth's residents of 33% and 20% respectively. Demand for aged services may be higher in the precinct due to these distributions.

Table 4 Age and family composition

| Category | Bassendean | Greater Perth |
|--------------------------------|------------|---------------|
| Median age | 40 | 37 |
| Couple family without children | 39.7% | 37.6% |
| Couple family with children | 42.7% | 45.7% |
| One parent family | 15.7% | 15.1% |

2.3.3 Incomes and housing cost

Income and housing costs within the Town align with those in Greater Perth. Rents and mortgage repayments are marginally lower than those in Greater Perth, aligned with lower incomes. This alignment suggests that pressures on accommodation costs and incomes within the Town are broadly typical of those in the wider metropolitan region.

Table 5 Incomes and housing costs (2021)

| Category | Bassendean | Greater Perth |
|----------------------------------|------------|---------------|
| Median household income (weekly) | \$1,772 | \$1,865 |
| Median weekly rent | \$330 | \$350 |
| Median mortgage repayments | \$1,850 | \$1,907 |

2.3.4 Housing

Housing in the Town consists of a higher proportion of town houses compared to Greater Perth. Infill development, including apartment buildings in the Precinct have contributed to growing levels of residential density in the Town over the preceding decades.

Table 6 Housing types (2021)

| Category | Bassendean | Greater Perth |
|--------------|------------|---------------|
| Single House | 74.9% | 77.8% |
| Town house | 19.0% | 14.2% |
| Apartment | 6.0% | 7.6% |

2.3.5 Non-residential floorspace

Trade area

The assessed trade area (approximately 10km radius from the Precinct) of the Precinct contains approximately 105,000 dwellings (2024). Forecast growth in the catchment is expected to increase the number of dwellings in the trade area to 143,000 by 2050, representing an increase of 36%.

Compared to Greater Perth, the trade areas income composition (and spending propensity) is broadly similar. Incomes in the trade area are slightly less in higher quintiles and significantly higher into lower quintiles. While incomes across the trade area are broadly consistent, they are generally lower than Greater Perth. Part of this outcome may be attributable to the small household size in the trade area.

Existing non-residential floor space

The Precinct is estimated to contain approximately 11,980sqm of Shop floorspace and 10,656sqm of non-Shop commercial floorspace's. A significant volume of this floorspace is contained within the Bassendean shopping centre.

Commercial vacancies

The Town has a low-level of vacant commercial floorspace relative to benchmark areas. When compared to similar locations such as Cambridge, Belmont and Cottesloe, the Town's commercial floorspace performs well. The low ratio suggests that the Precinct likely has sufficient demand to expand current commercial floorspace supply.

Floorspace gaps

The Needs Assessment and Economic, Retail and Employment Strategy (refer Appendix B) assess the likely floorspace gaps within the precinct. Commercial floorspace gaps have been aligned to broad floorspace categories to provide an understandable potential mix of uses that could eventuate at the Bassendean Precinct.

Table 7 Floorspace gaps (2025)

| Category | Gap (sqm) | Summarised use category |
|---------------|-----------|---|
| Entertainment | 1,239 | Gymnasium and sport community facility, libraries, arts and cultural centre |
| Health | 796 | Church and community organisations, welfare and charitable services |
| Office | 4,113 | Profession services, real estate |
| Retail | 718 | Furniture retail |
| Service | 235 | Light fitting retail |
| Utilities | 2,465 | Parking |
| Storage | 1,053 | Machinery and equipment supplies, warehouse and storage |

Floorspace need

Detailed analysis of the Precinct's retail catchment, available expenditure and regional floorspace supply included in the Needs Assessment and Economic, Retail and Employment Strategy (refer Appendix B). Projections indicate that the Precinct could support approximately 18,000sqm of Shop/Retail floorspace by 2050.

This represents an increase of approximately 6,000 sqm over the coming decades. A further 15,000sqm of non-shop commercial floorspace is forecast, representing a considerable increase in demand in professional service industry. Project floorspace need across Shop/Retail and Non-Shop categories are summarised in Table 8.

Table 8 Estimated floorspace need (2024-2050)

| Category | 2024 | 2029 | 2034 | 2039 | 2044 | 2050 |
|-------------|--------|--------|--------|--------|--------|--------|
| Shop/retail | 11,980 | 13,134 | 14,068 | 15,189 | 16,398 | 17,984 |
| Non-shop | 10,656 | 16,992 | 23,133 | 24,130 | 25,205 | 26,616 |

Employment projection

Increasing the availability of floorspace at the Bassendean Precinct is key to maintaining (or increasing) the level of employment self-containment within the Town. Total employment within the Precinct is project to increase from approximately 809 full-time equivalent positions in 2024 to 1,270 by 2034.

Table 9 Employment projection (2024-2050)

| Category | 2024 | 2029 | 2034 | 2039 | 2044 | 2050 |
|----------|------|------|-------|-------|-------|-------|
| Total | 809 | 943 | 1,006 | 1,082 | 1,163 | 1,270 |

Employment self-sufficiency (ESS) refers to the ratio between number of jobs available in a region and the number of people employed. A higher ESS (closer to 100%) would suggest a strong levels of economic activity. Employment self-containment (ESC) refers to the proportion of jobs in a region occupied by residents of the same area. A high ESC would suggest alignment between workforce characteristics and local job types.

The Town's ESS is 77%, while the Town's ESC is only 12%. This analysis shows that:

- The Town's low ESC suggests opportunities to encourage employment opportunities that match the skills of the local workforce.
- The Town's low levels of commercial vacancies indicates that there is likely demand to expand floorspace supply.
- The large size of the Town's manufacturing workforce may present opportunity for the precinct to attract additional customer expenditure.
- The Precinct could look to attract knowledge-intensive service industries to improve the Town's employment self-containment. This includes office, health and education floorspace types. The Precinct could also accommodate greater cultural industries to improve the Town's employment self-containment. Greater entertainment and retail floorspace are relevant types of businesses.

2.3.6 Cultural heritage

Aboriginal heritage

There are four registered, lodged or historic Aboriginal cultural heritage places within the precinct. There have been 17 previous surveys and survey reports that cover the precinct area.

Table 10 Aboriginal cultural heritage places

| ID | Place name | Place type | Listing status |
|--------------|-----------------------------|---|----------------|
| ACH-00003487 | Bennett Brook: Eden Hill R. | Camp; Meeting Place; Water source | Registered |
| ACH-00003757 | Success Hill | Artefacts/Scatter; Birthplace; Camp; Ritual/Ceremonial; Creation/Dreaming Narrative; Fish Trap; Traditional Structure; Meeting Place; Other; Quarry; Repository/Storage Place; Water Source | Registered |
| ACH-00003758 | Helena River | Ritual/Ceremonial; Creation/Dreaming Narrative; Repository/Storage Place | Registered |
| ACH-00003840 | Bennett Brook: Camp Area | Burial; Artefacts/Scatter; Camp; Ritual/Ceremonial; Creation/Dreaming Narrative; Fish Trap; Historical; Hunting Place; Traditional Structure; Plant Resource; Water Source | Registered |

This Precinct Structure Plan is supported by a Aboriginal Cultural Heritage Assessment (Appendix H) prepared to provide further context to the significance of Aboriginal cultural heritage within the precinct.

European settlement history

Land grants along the Swan River were allocated from 1829, with more than 3,000 acres of rich alluvial land set aside which was to be used as a government farm. The land was then divided up into two large blocks and five smaller ones, these blocks were located on the north-western banks of the Swan River and was named West Guildford in 1829.

The original name of the Town of Bassendean was West Guildford with the town being laid out early in 1831. The name Bassendean is derived from a parcel of land granted to James Henty, who originally named it 'Stoke Farm'. The farm was then acquired by Peter Broun in 1832 who renamed it Bassendean after his family's estate of the same name in Berwickshire.

Bassendean continued to grow slowly but steadily until the 1890s with the discovery of gold. The discovery saw a rapid influx of people. The river environment played an important role in the growth of local industry. River side industries included brick making and by 1901 there were several brickyard's including those north of the railway line near Success Hill.

The river flats also supported industries such as dairy farming, poultry yards, market gardening, and seasonal cropping of wheat, oats, hay and corn. Bassendean became a thriving industrial area in the early 1900s with a number of factories and mills established in the area. There was a high demand for labour at the Midland Railway workshops, which in turn gave the suburb its distinct working class character and further accelerated its development.

Historic heritage

Within the precinct, there are several cultural heritage places formally protected under planning frameworks. This includes:

- Two (2) heritage places identified on the State Register of Heritage Places, including several related/child places (refer Table 11).
- Three (3) places included on the Town of Bassendean Heritage List a management category of 'Exceptional Category 1', being Bassendean Fire Station, Padbury Buildings, Town Pillar Box.
- A further sixteen (16) places included on the Town of Bassendean Heritage List with a management category of 'Considerable - Category 2'.

These places represent considerable cultural heritage value within the eastern region of Perth, and provide strong definition to the precincts aesthetic, social identity and sense of place.

Table 11 State register of heritage places

| ID | Place name | Address | Listing status |
|-------|-------------------------------|-------------------------------|-------------------------------|
| 129 | Bassendean Fire Station (fmr) | 10 - 14 Parker St, Bassendean | State Register Place |
| 7403 | Bassendean Oval | 140 Old Perth Rd, Bassendean | State Register Place |
| 18088 | Bassendean Oval Entrance Gate | 1 West Rd, Bassendean | Child of State Register Place |
| 18089 | Bassendean Oval Grandstand | 1 West Rd, Bassendean | Child of State Register Place |
| 18090 | MacDonald Grandstand | 1 West Rd, Bassendean | Child of State Register Place |

Figure 6 Heritage places



Old Perth Road Heritage Area

The Old Perth Road Heritage Area applies to land along Old Perth Road between Guildford Road and Wilson Street. It includes a number of places already listed on the Town's Heritage List. To the south of the precinct is the Kenny Street Heritage Area. Assessment of development within heritage areas is to be undertaken in accordance with Local Planning Policy 4 - Heritage and Character.

Success Hill Frame and Nurstead Heritage Area

The Success Hill Frame contains a range of buildings with varying cultural heritage significance. In preparing this Precinct Structure Plan, consideration was given to whether the Success Hill Frame area warranted adoption as a heritage area. The area contains six places previously designated of heritage value and are included in the Town of Bassendean Local Heritage Survey. Two places are included on the Heritage List which provides greater statutory controls over the future development of these places. A heritage assessment was prepared (refer Appendix G - Success Hill Heritage Assessment). The assessment determined that there were several places in the Success Hill Frame of considerable or some cultural heritage value. This Precinct Structure Plan includes provisions to strengthen the protection of heritage value within the portion of the Success Hill Frame east of West Road where heritage and character values are strongest. Formal recognition of the Nurstead Heritage Area requires separate resolution of Council and modification to the Town's Local Planning Policy 4 - Heritage and Character.

2.3.7 Public open space

The Precinct contains a range of public open space areas, including the regionally significant Bassendean Oval. In total, approximately 10.88ha of public open space is available within the precinct. This represents approximately 19.2% of the precincts total area. This exceeds the 10% of gross subdivision area criteria set out in the WAPC's Development Control Policy 2.3 - Public Open Space, however this open space network comprises almost entirely regional open space areas.

However, as per the Town's Local Planning Strategy, the suburb of Bassendean provides only 8.04% of land area as public open space. The concentration of public open space within the precinct leads to design

responses contained in this plan that seek to improve the functionality and accessibility of public open space areas for the benefit of the wider Bassendean neighbourhood.

Table 12 Public open space areas

| Name | Hierarchy classification | Area (approx) | Function |
|---------------------------------|--------------------------|---------------|------------|
| Bassendean Oval | Regional | 5.95 ha | Sport |
| BIC Reserve | District | 3.74 ha | Recreation |
| BIC Reserve North | Local | 0.44 ha | Nature |
| Palmerston Square | Local | 0.6 ha | Recreation |
| Christie Park | Small | 0.1 ha | Recreation |
| Surrey Street Public Open Space | Small | 0.05 ha | Recreation |

The Department of Local Government, Sport and Cultural Industries (DLGSC) suggests that between 16-19.5sqm of public open space, for active recreation and/or sports use should be provided per resident for metropolitan local government areas.

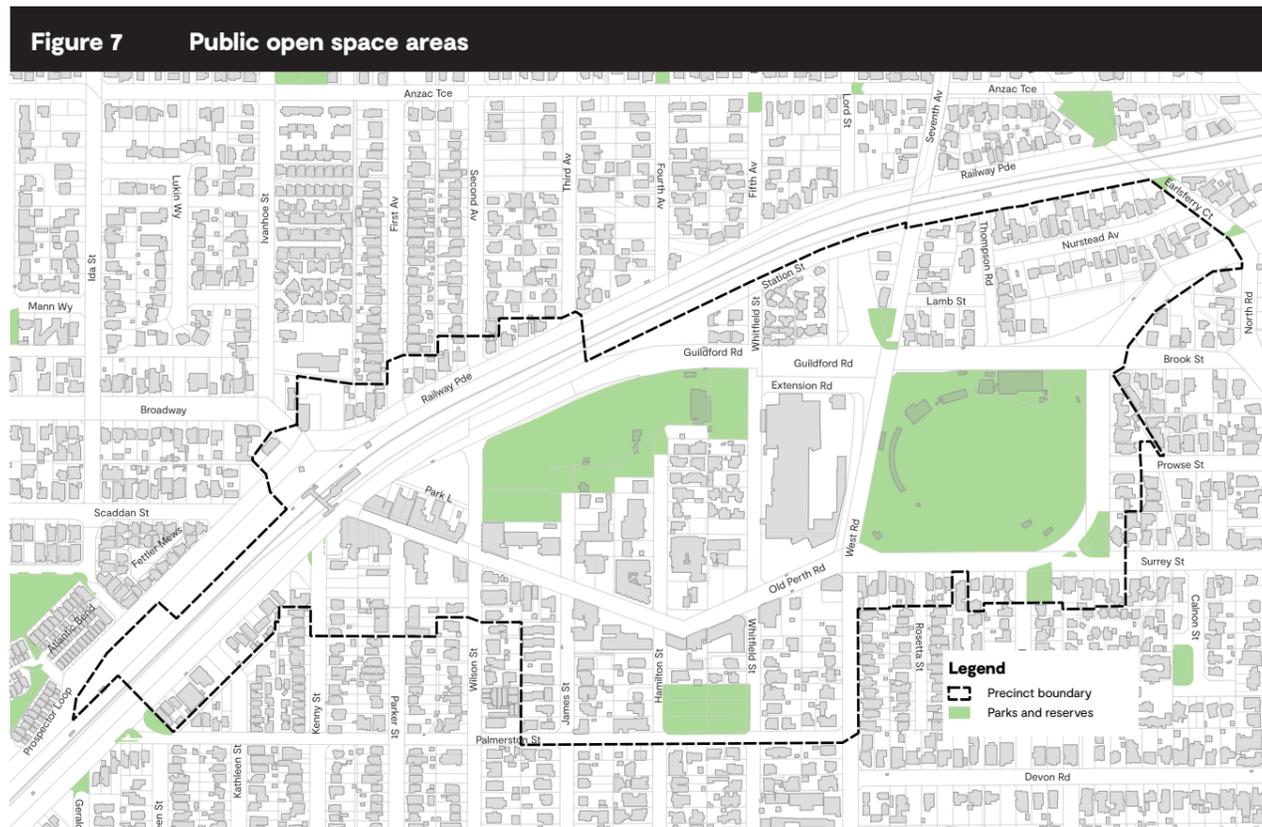
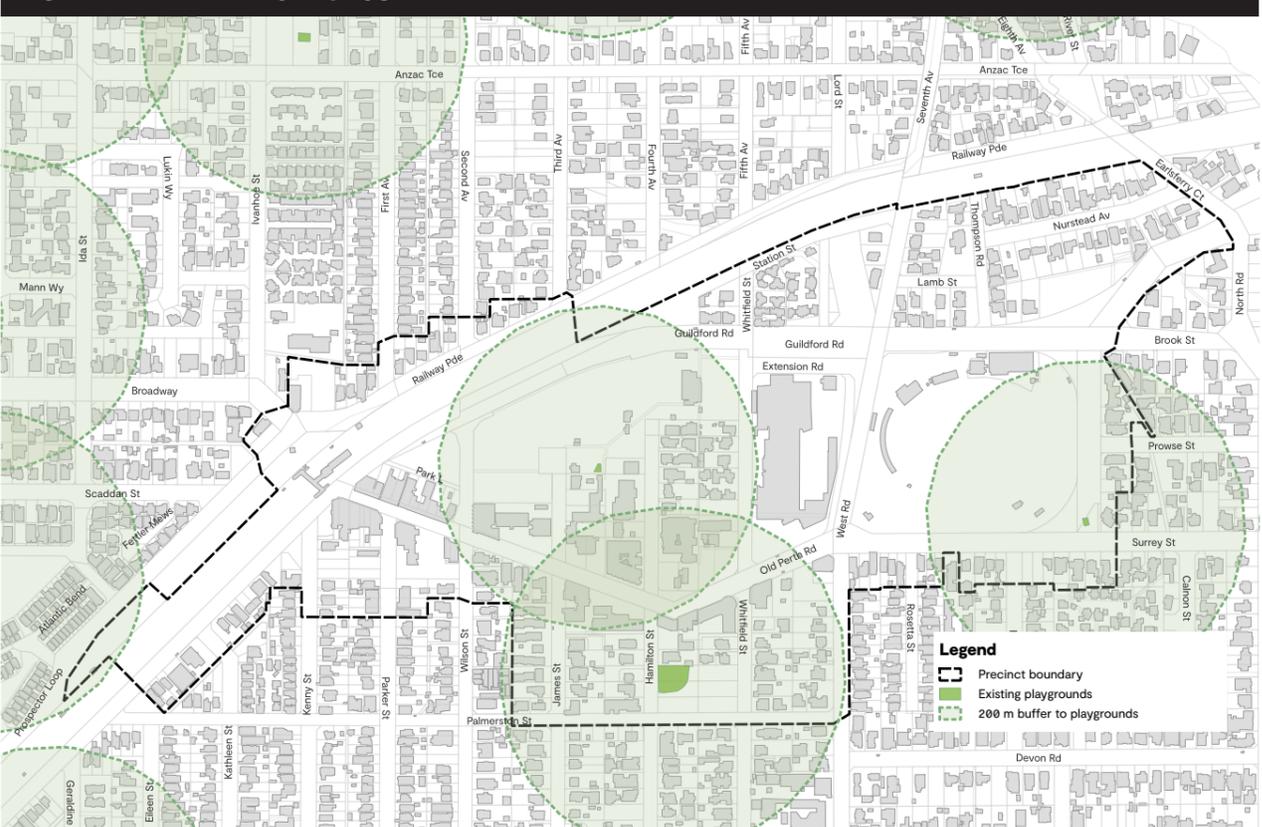


Figure 8 Proximity to playgrounds



2.3.8 Community facilities

With the projected population growth, greater pressure will be placed on existing infrastructure to accommodate community needs. This Precinct Structure Plan enables to continued operation and development of numerous community facilities already present within the precinct.

The Town is pro-actively planning for additional community facilities within the Precinct, predominately through the Bassendean Oval: Concept Masterplan. The Concept Masterplan anticipates provision of additional recreational, cultural, civic and community facilities at the existing Bassendean Oval site.

Table 13 Community facilities

| Type | Examples |
|--|--|
| Community centres and civic facilities | Bassendean Memorial Library, Town Administration Centre and Bassendean Seniors and Community Hall |
| Sporting clubs and facilities | Swan Districts Football Club, Bassendean Tennis Club, Bassendean Bowling Club, Bassendean Skate Park |
| Care and worship facilities | Aged care, child care centres and various place of worship facilities |
| Cultural facilities | Art Mind Art School, together with other community art establishments (dance school etc). |
| Educational institutions | Saint Michaels School, Bassendean Primary School (adjacent Precinct) |

In addition to facilities within the precinct, residents, visitors and workers also have access to the various community facilities and community spaces beyond the precincts boundary.

2.3.9 People movement

Active transport

Old Perth Road is designed to support pedestrian movement; however, activity and interest along and adjacent to the street are fragmented. This discourages walking and cycling to and within the precinct.

Pedestrian and cycling access across Guildford Road and to the north of the Perth–Midland railway line is limited and generally uninviting. For many, travelling by car from areas north of the precinct remains more convenient than using active transport.

Without a significant shift towards active or public transport, the surrounding road network is expected to experience congestion during peak periods, even with infrastructure modifications.

Public transport

The precinct is currently well served by public transport, and no significant increases in services or infrastructure are anticipated in the future.

Access to Success Hill Station is limited due to its relatively isolated location, lack of attractive pedestrian and cycling connections—aside from a principal shared path on the northern side of the railway—and minimal passenger amenities. The absence of dedicated parking and kiss-and-ride facilities further restricts accessibility.

Vehicle movements

The intersection of Guildford Road and Collier Road is expected to become a significant congestion point in the future. The current crash history at this location warrants a review of traffic safety and consideration of grade-separated infrastructure to improve flow and reduce risk. The Town supports improvements to Collier Road that enhance local connectivity and pedestrian/cyclist safety. However, it opposes any expansion that would increase freight or through-traffic, especially if it undermines the character of surrounding residential

areas. The Town is advocating for intersection upgrades and better integration with public transport.

Movements from Guildford Road onto Old Perth Road are currently limited and are likely to remain so. This presents an opportunity to reconfigure the intersection to support a more attractive public realm and enhance access to Bassendean Station.

Access from Thompson Road onto Guildford Road is heavily constrained during both morning and afternoon peak periods. This issue is expected to intensify with future growth in the Success Hill Frame area, further impacting local traffic conditions.

Vehicle parking and deliveries

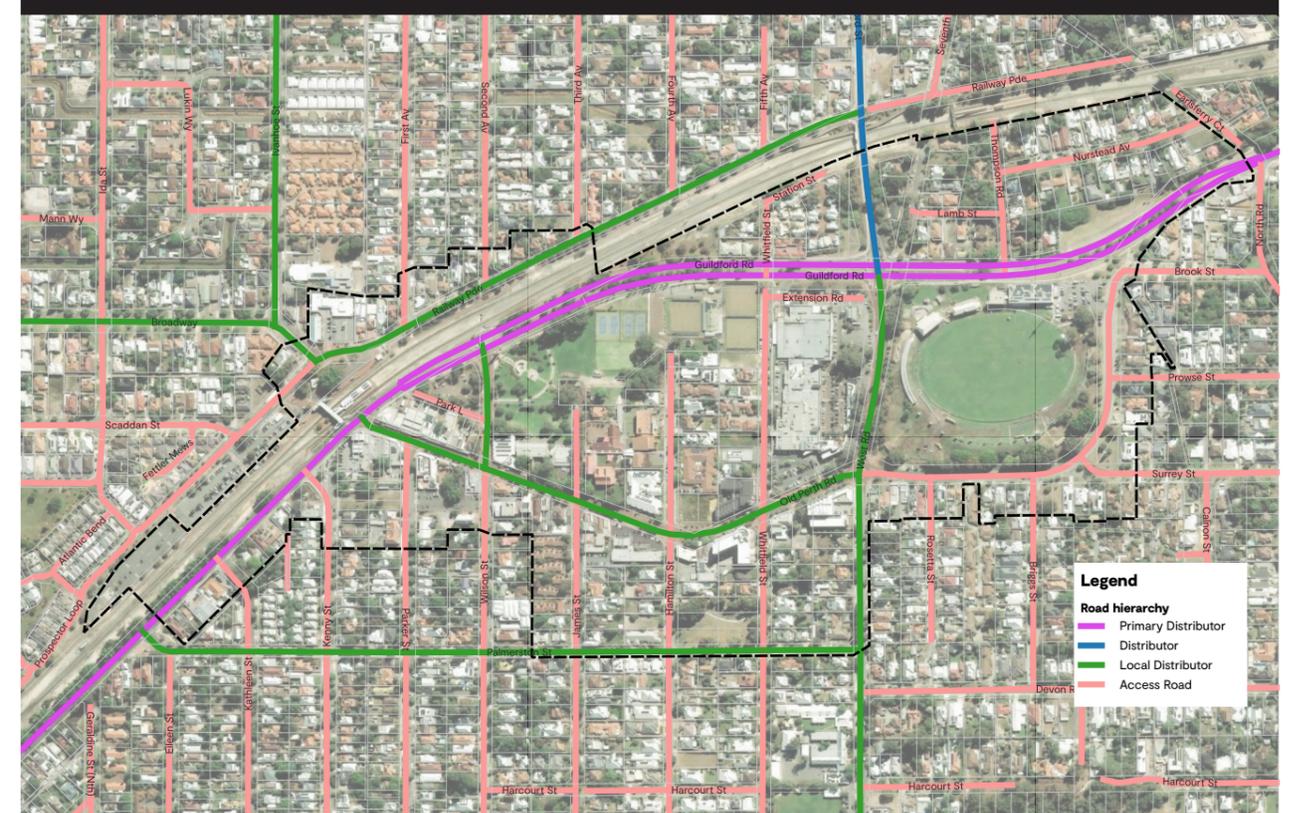
There are over 500 publicly available off-site parking bays within the precinct. These bays are typically occupied at less than 50% capacity during weekdays. In addition to public parking, many customer bays are available on key commercial sites.

The current supply of off-site public parking is sufficient and is expected to meet demand well into the future. Additional public parking would only be required in the event of significant growth in non-retail land uses, such as office or professional services. Given the existing surplus of off-site parking, it is unlikely that parking caps or cash-in-lieu provisions will need to be considered.

Future movement network

The road network surrounding the precinct requires targeted modifications to improve traffic flow, safety, and public realm outcomes. Key proposals include converting a section of Old Perth Road to one-way traffic to enhance parking efficiency and create space for landscaping and activation, and introducing a roundabout at Thompson Road to support access to the Success Hill area. Additional short-term access via Earlsferry Court is possible but limited, with long-term solutions potentially involving a roundabout or signals at the North/Earlsferry/Guildford junction. Guildford Road intersections at Collier Road and West Road/Lord Street are expected to become congested, with level-crossing removal and signal optimisation proposed to manage traffic and improve cycling safety.

Figure 9 Main Roads WA road hierarchy



Parking within the precinct is underutilised, and there is potential to re-purpose existing bays to support other transport modes or public realm improvements, particularly along Old Perth Road.

Public transport facilities at Bassendean and Success Hill stations require upgrades, and there is an opportunity to advocate for redevelopment of the southern end of the Bassendean Station car park to support broader precinct objectives.

2.3.10 Utility networks

To identify implications for current and future infrastructure networks, a Local Infrastructure Servicing Report has been prepared to accompany this Precinct Structure Plan. Servicing implications are summarised in Table 14.

Table 14 Condition of utility networks

| Service | Existing condition | Future condition |
|---------|---|--|
| Sewer | The Water Corporation's existing gravity sewer reticulation system within the Precinct comprises a network of DN150 DN230 and DN305 vitrified clay (VC), polyvinyl chloride (PVC-U), and reinforced concrete (RC) gravity mains, with the majority discharging into a regional DN305 RC sewer located along Guildford Road. | Water Corporation has advised that while the existing DN305 gravity sewer main has been identified for long-term upsizing to DN375, this work currently sits outside of their 5-year capital program and will only be triggered by future development demand. Sewer infrastructure will require incremental replacement/upgrade as the Precinct progressively develops. |
| Water | Existing water supply infrastructure within the precinct comprises a reticulated distribution network consisting of DN75, DN90, DN100, DN150, and DN205 water mains. These assets include a combination of asbestos cement (AC), cast iron (CI), steel (S), and polyvinyl chloride (PVC) pressure pipes. | Based on Water Corporation's preliminary advice, the proposed yields exceed the capacity considered in their current long-term planning. While upgrades have been previously identified, they are not currently included in the Corporation's 5-Year Capital Investment Program. Water infrastructure will require incremental replacement/upgrade as the Precinct progressively develops. |
| Power | The Western Power Network Capacity Mapping Tool report published in October 2024, indicates the Hadfield's (NCR) Zone Substation (Z/S) had approximately 77MVA of total capacity and 20MVA spare capacity with a medium utilisation of 74% in 2024, and a forecast moderately highly utilisation of 82% by 2034. | Western Power requires a minimum design capacity of 200kVA per hectare for commercial developments, equating to 0.360MVA for the proposed commercial area, and approximately 6.42MVA for future residential growth in Bassendean based on current loading assumptions. While the Hadfield's Zone Substation has sufficient spare capacity (20MVA), the key constraint is the 5MVA load limit on each leg of its HV feeder Y-configuration. To support future redevelopment, HV network reconfiguration may be required—such as offloading some feeder loads to adjacent substations. |

| Service | Existing condition | Future condition |
|----------|--|---|
| Gas | An extensive network of medium pressure (MAOP 70kPa) PVC and PE gas reticulation mains are present throughout the Precinct, with service connections provided to numerous properties. | The existing gas distribution network has sufficient capacity to accommodate the additional residential dwellings proposed by this plan. It is noted that there will be no increase anticipated to the number of meters as the commercial properties are not intended to be further subdivided as part of the Precinct's growth. |
| Drainage | <p>The existing storm water drainage infrastructure within the precinct comprises a network of pits and pipes to convey runoff across the area.</p> <p>In addition to the local network, a Water Corporation owned drainage system services the northern catchment. This network traverses the northern extent of the precinct, running primarily along Guildford Road through to Brook Street and extending eastwards.</p> <p>The network within the precinct is generally at capacity, resulting in localised flood risk standing water during major events. Earlier analysis of drainage constraints identified the need to make localised upgrades to pipe networks.</p> | A series of drainage assessments undertaken since 2014 have consistently highlighted deficiencies within the Town's drainage network and identified the need for upgrades to mitigate localised flooding and manage stormwater more effectively. While early recommendations focused on specific pipe upgrades to address existing management issues, subsequent technical studies (2022–2023) reinforced the need for additional capacity through pipe enlargements, subsurface storage within Success Hill Reserve, and provision of an overflow pathway to the Swan River for extreme events. Despite these findings, localised flooding continues to be observed, particularly around Old Perth Road, Whitfield Street, and Bassendean Oval. Targeted upgrades are required to reduce flood risk and ensure the network can adequately support both current and future conditions. Improvements to infiltration rates (both in the public and private domain) aim to incrementally reduce drainage flowrates over time. |

Figure 10 Drainage network

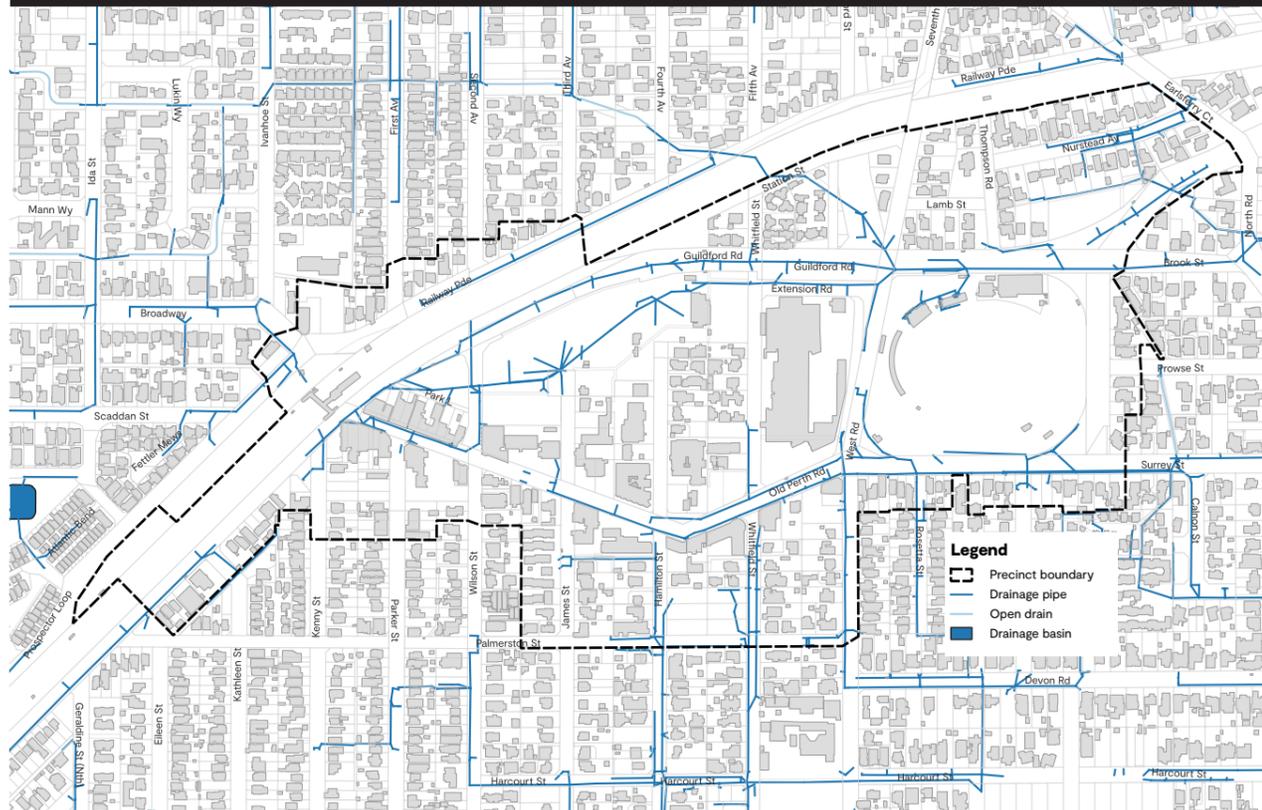
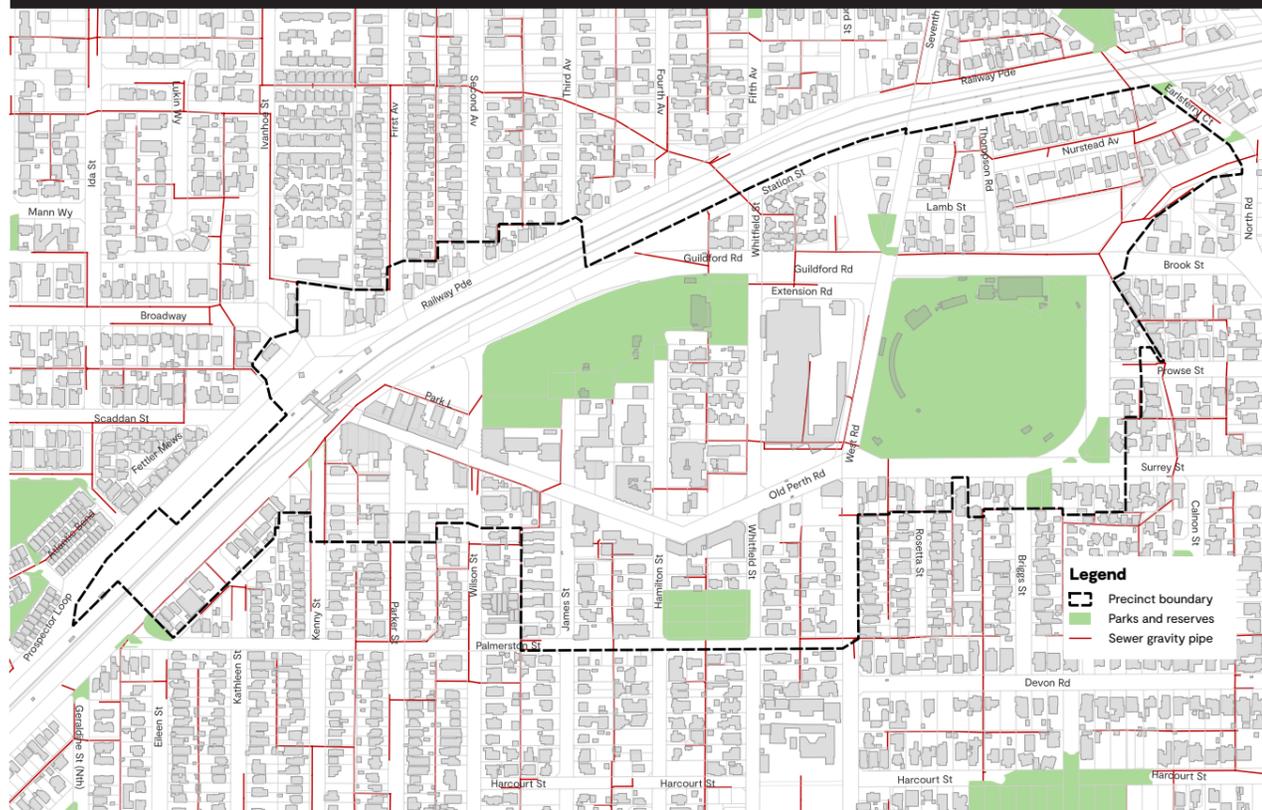


Figure 11 Sewer network



2.4 Governance context

The Precinct's governance context guides key decisions relating to land use intensity, environmental resource management and strategic planning alignment.

2.4.1 Strategic planning context

Perth & Peel @3.5 million and Central Sub-Regional Planning Framework

The Perth and Peel@3.5million suite of strategic land use and infrastructure plans, including four Sub-regional Planning Frameworks (north-west, north-east, central and south metropolitan Peel), seek to guide the future growth of the Perth and Peel regions as a compact, consolidated and connected city that can accommodate a population of 3.5 million by 2050.

The Central Sub-regional Planning Framework provides high level guidance for the growth of the Central sub-region of the Perth metropolitan area, and forms part of the 'Perth and Peel @3.5 Million' suite of strategic land use and infrastructure plans. The Central Sub-regional Planning Framework provides high-level guidance regarding where new homes and jobs will be located, how to make best use of existing and proposed infrastructure and how best to protect the natural environment to allow sustainable growth within the Central sub-region.

The Central Sub-regional Planning Framework outlines an infill target for the Town of Bassendean on 4,150 dwellings.

Town of Bassendean Local Planning Strategy

The Town's Local Planning Strategy was endorsed in February 2023. The Strategy sets out the long-term planning direction for land use and development within the Town; principle of which is planning for the Precinct.

Consistent with the principles of urban consolidation and the Central Sub-regional Planning Framework, the local planning strategy identifies six planning areas for land use intensification. For the Bassendean Precinct, this includes accommodating between 1,209 and 2,175 additional dwellings by the middle of the century.

The Strategy includes actions to prepare a precinct structure plan for the precinct, based primarily on the findings and community aspirations expressed through the Town Centre Masterplan.

Bassendean Town Centre Masterplan

In 2021, the Town endorsed the Bassendean Town Centre Masterplan following extensive community and external stakeholder engagement. The Masterplan provides a high-level vision for the precinct and is intended to be implemented through preparation of this Precinct Structure Plan.

Key components of the Masterplan include redevelopment of a portion of the Swan District Football Club, creation of a centrally located piazza, future redevelopment of key Crown landholdings and closure of a portion of Old Perth Road. A range of heights are indicated, up to 10 storeys, in strategic locations to ensure the retention of heritage and character and having regard to overlooking and overshadowing.

Bassendean Oval Concept Masterplan

The Bassendean Oval Concept Masterplan is a redevelopment proposal aimed at rejuvenating the historic oval and its surrounding precinct. It includes re-orienting the oval to meet AFL standards, which will improve turf conditions and visibility, and upgrading facilities to support professional-level matches. The plan also introduces new community amenities such as inclusive playgrounds, walking tracks, and spaces for youth, aged care, and childcare services.

In addition to sporting and recreational upgrades, the masterplan proposes integrated residential and commercial development to enhance the precinct's long-term viability while respecting its heritage character.

2.4.2 Statutory planning context

Metropolitan Region Scheme

The Metropolitan Region Scheme (MRS) outlines objectives for regional development and provides a statutory mechanism to assist strategic planning, by setting out broad land-use zones, setting aside areas for regional open space, protection of environmental values and other regional infrastructure purposes, and assisting in coordinating the provision of major infrastructure.

The majority of the Precinct is zoned Urban under the MRS, with reserves for Guildford Road (Primary regional road reserve), Lord Street (Other regional road), railways (Perth-Midland railway) and Bassendean Oval (Parks and recreation).

Town of Bassendean Local Planning Scheme

LPS11 applies to the whole of the Precinct. LPS11 provides site, development and land use requirements that apply to any application for development approval. Most of the Precinct is zoned District Centre, with no residential density coding applied. The District Centre zoning focuses on properties near to Old Perth Road, including the Bassendean shopping centre. Other zones and density codings that apply within the Precinct are depicted on Figure 12.

This Precinct Structure Plan specifies zoning and coding outcomes for the Bassendean Precinct that are to be implemented as part of a future amendment to the local planning scheme.

State Planning Policy 4.2 – Activity Centres

SPP4.2 ensures that the planning, development and decision making relating to the distribution, function, land use, access and urban form of activity centres is coordinated. SPP4.2 describes the Bassendean Activity Centre as a District Centre and gives the following guidance to its development:

District centres have a greater focus on servicing the daily and weekly needs of residents. Their relatively smaller scale and catchment enables them to have a greater local community focus and provide services, facilities and job opportunities that reflect the needs of their catchments.

Many district centres also have a specialisation such as tourism or entertainment which serves a wider catchment. District centres with a rail station should be a focus for medium and high-density housing and employment growth.

Implementation guidelines accompanying the SPP provide direction to form, density and land use. The Implementation Guidelines further specify that to achieve an average residential density of +25 dwellings within a walkable catchment, codings of R60/40+ within 200 metres and R40+ within 400 metres would be appropriate.

Table 15 State planning policies

| Policy | Summary | Plan response |
|---|---|--|
| SPP7.2 – Precinct Design | SPP7.2 provides guidance for precinct planning with the intent of achieving good planning and design outcomes for precincts. The policy recognises that there is a need to plan for a broader range of precinct-based contexts and conditions to achieve a balance between greenfield and infill development. | This Precinct Structure Plan is aligned to the guidance and criteria of SPP7.2, with the rationale for design outcomes described in Part 3 – Opportunities and constraints. This plan considers holistic design responses to urban structure, built form, integration with the public realm and the protection of ecological values. |
| SPP7.0 – Design of the built environment | SPP7.0 sets out the objectives, measures, principles and processes which apply to the design and assessment of built environment proposals through the planning system. It applies to activity precinct plans, structure plans, local development plans, subdivision, development, and public works. The policy contains 10 design principles: context and character, landscape quality, built form and scale, functionality and build quality, sustainability, amenity, legibility, safety, community, and aesthetics. | Preparation of this Precinct Structure Plan has considered the criteria of SPP7.0 in formulating the design responses set out in Section 3.0 – Design Responses. Preparation of this Precinct Structure Plan has included engagement with the Town’s Design Review Panel. The Panel has provided feedback and guidance on the emerging Precinct Structure Plan approach, which is described further in this section. |
| SPP 2.0 – Environment and Natural Resources | SPP 2.0 is a broad sector policy and provides guidance for the protection, management, conservation and enhancement of the natural environment. The policy promotes responsible planning by integrating environment and natural resource management with broader land use planning and decision-making. SPP 2.0 outlines general measures for matters such as water, air quality, soil and land quality, biodiversity, landscapes and energy efficiency. | Redevelopment of the Precinct presents a significant opportunity to achieve high levels of triple-bottom line sustainability, reduced resource used and efficiency in urban development. This Precinct Structure Plan is supported by a Environmental Assessment and Local Water Management Strategy that outlines approaches to environmental sustainability, mature tree protection and resource conservation. |

| Policy | Summary | Plan response |
|---|---|--|
| SPP 2.8 – Bushland Policy for the Perth Metropolitan Region | SPP 2.8 provides a policy and implementation framework that will ensure bushland protection and management issues in the Perth Metropolitan Region are appropriately addressed and integrated with broader land use planning and decision-making. The primary purpose of the policy is to secure the long-term protection of biodiversity and associated environmental value sites, being Bush Forever areas. | The Precinct does not contain any Bush Forever areas or areas of significant remnant native bushland. However, the precinct is nearby to regionally significant bushland at Ashfield Flats and at various other points along the Swan River. Mature trees within and nearby to the precinct form part of an ecological corridor between these significant areas and other natural areas across the Swan River Estuary, Helena River and Swan Valley. |
| SPP 2.9 – Planning for Water | SPP 2.9 ensures that planning and development considers water resource management. The policy establishes objectives relating to improving environmental, social, cultural and economic values of water resources. | This Precinct Structure Plan is supported by a Local Water Management Strategy which outlines the approach to management of water. This includes the effective management of storm water, as well as water conservation in new developments. |
| State Planning Policy 3.6 – Infrastructure Contributions | SPP 3.6 sets set out the principles and requirements that apply to the establishment and collection of infrastructure contributions in new and established areas. | This Precinct Structure Plan contemplates the provision of significant publicly accessible open areas, together with improvements to the surrounding streetscape, as a result of development. The accompanying Local Infrastructure Servicing Report contemplates the potential for future upgrade of reticulated water and sewer networks, both by the Water Corporation and development proponents. Other infrastructure upgrades are contemplated and described in section 2.3.10 – Utility networks. |

Figure 12 Existing Local Planning Scheme zones and reserves

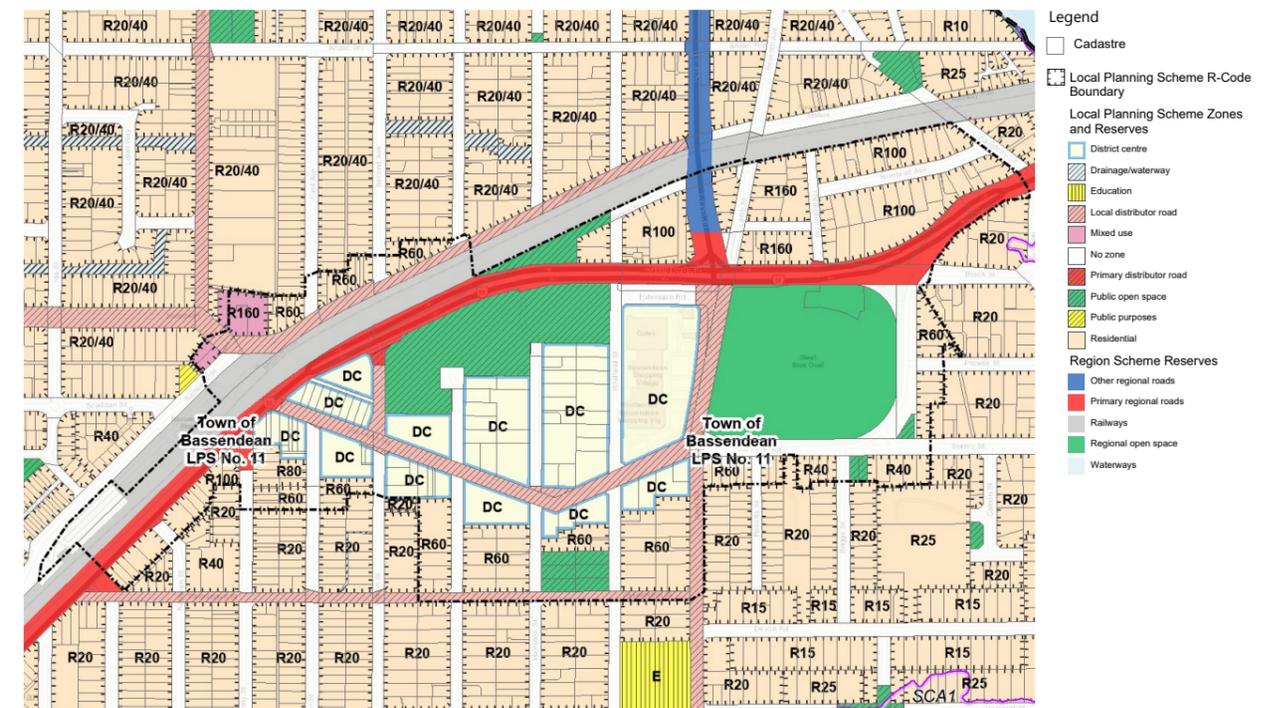
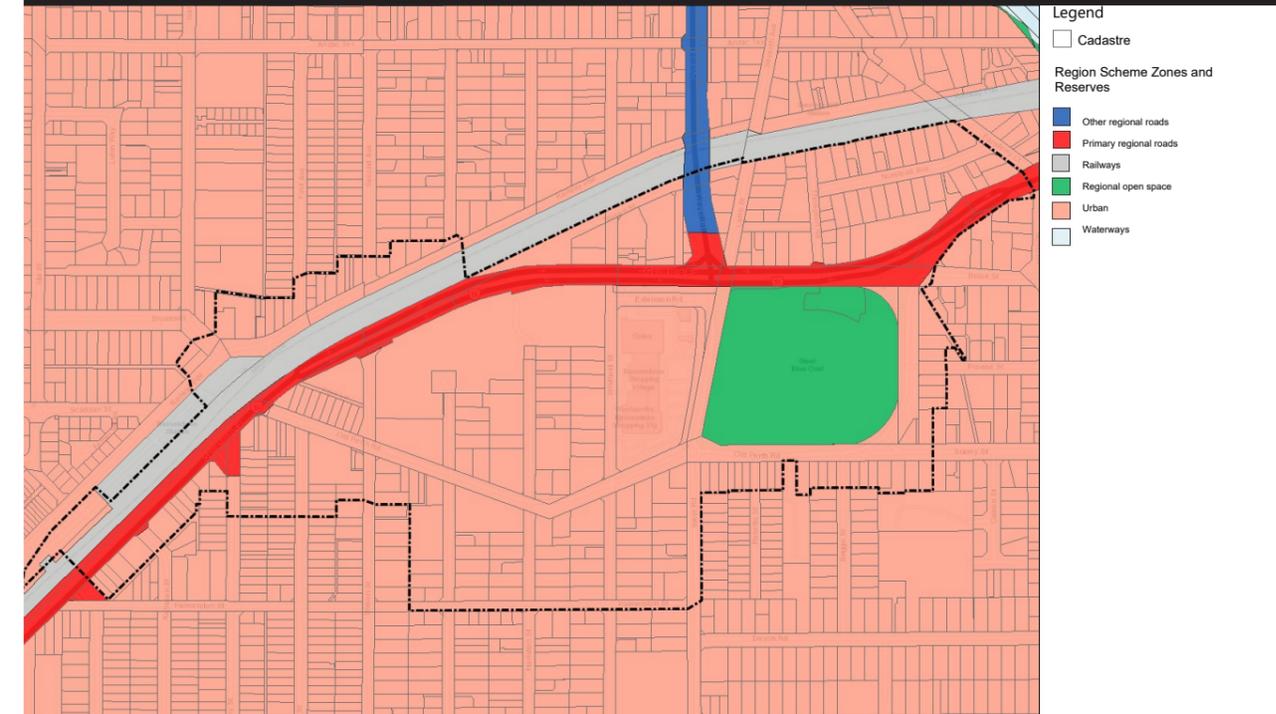


Figure 13 Existing Region Planning Scheme zones and reserves



| Policy | Summary | Plan response |
|-------------------------------|--|---|
| SPP 5.4 – Road and Rail Noise | SPP 5.4 provides guidance for the performance-based approach for managing and mitigating transport noise associated with road and rail operations. This policy applies where noise sensitive land uses are located within a specified distance of a transport corridor, new or major road or rail upgrades are proposed or where works propose an increase in rail capacity resulting in increased noise. SPP 5.2 is supplemented by the Road and Rail Noise Guidelines. | This Precinct Structure Plan proposes intensification of sensitive land use nearby to existing railways and main roads where noise sensitive land use is not recommended. This includes land near to the Perth–Midland railway and Guildford Road. SPP5.4 identifies the need to consider land use separation (at least 10m), ‘Quiet House’ design attenuation measures and preparation of a noise management plan to accompany development applications. In these locations, residential dwellings and other sensitive receptors already exist. Consideration of noise attenuation of buildings should be considered in line with SPP5.4 when assessing development applications. |

Local Planning Policies

The Bassendean Precinct is subject to a range of adopted Local Planning Policies which form part of a Town’s Local Planning Framework. It is anticipated that a range of amendments and revocations to established policies will be required to ensure consistency with this Precinct Plan.

Table 16 Local Planning Policies

| Policy | Summary and plan response |
|---|--|
| LPP1 – Bassendean Town centre | LPP1 provides an interim local planning framework to guide development, until such time as the area is subject to a Precinct Structure Plan. The policy seeks to ensure development addresses the street and progressively facilitates continuous and activated streetscapes. The key principles of LPP1 have informed this Precinct Structure Plan. |
| LPP2 – Sustainable development | LPP2 aims to clearly outline the criteria and design standards for the sustainable design of residential development and encourage sustainable development throughout the Town. LPP2 does not extend to dwellings developed under SPP7.3 – Residential Design Codes (Volume 2 – Apartments). This Precinct Structure Plan includes provisions that seek to improve the built sustainability of the Precinct. |
| LPP4 – Heritage character | LPP4 aims to conserve and protect places and areas of heritage and / or character significance. The Precinct contains a number of places on the State Register of Heritage Places and the Town’s Heritage List. Site and development requirements of this Precinct Structure Plan have regard to the cultural heritage and character values of the Precinct, including along Old Perth Road and in the Success Hill Frame. |
| LPP8 – Car parking and end of trip facilities | LPP8 stipulates the minimum number of parking and end of trip facilities to be provided to support development. This Precinct Structure Plan is supported by a Transport Impact Assessment and Parking Management Plan (refer Appendix D) that describes the recommended approach to movement and parking within the Precinct. |
| LPP9 – Design review panel | LPP9 ensures that major development within the Town are subject to design review and are designed to be consistent with SPP7.0 – Design of the Built Environment. This Precinct Structure Plan contains provisions relating to design quality, ensuring new development positively contributes to the future of the Precinct. |
| LPP15 – Public art | LPP15 provides for the provision of public art contributions (monetary, or in-kind) where the value of a development exceeds \$2.0 million. |
| LPP16 – Advertising signs | LPP16 provides development standards for advertising signs. The provisions ensure new signage is consistent with character, expected future amenity and relate to the premises. |
| LPP13 – Tree retention and provision | LPP13 provides for the retention of ‘regulated trees’, limiting removal of regulated trees and providing for new trees where SPP7.3 – R-Codes (Volume 1) apply. SPP7.3 – R-Codes (Volume 2 – Apartments) includes provisions relating to tree protection and planting that are augmented by this Precinct Structure Plan. |

2.5 Stakeholder engagement

The engagement goal for the Bassendean Precinct was to develop a Precinct Structure Plan that reflects the community's values and priorities by fostering an informative and inclusive planning process. This section summarises the key activities and findings of engagement with a wide variety of stakeholders that has informed preparation of this plan.

2.5.1 Local Planning Strategy engagement

In March 2019 the Town launched *BassenDream Our Future: A community engagement project to develop a vision for the future of Ashfield, Bassendean and Eden Hill* (BassenDream). The project team has worked alongside the local community and other stakeholders to understand community values; the character and spirit of the Town; and ideas for an overarching future vision. Following this initial visioning process, the engagement process used this initial feedback to inform discussions regarding aspirations; opportunities; and ideas for specific planning ideas/policies for the future of strategic planning within the Town of Bassendean.

- Vibrancy and activity in Old Perth Road, Ashfield town centre and Eden Hill's commercial areas
- Enhanced connection to greenery and the natural environment, in particular the Swan River
- Retaining the strong sense of community and small country town feel
- Desire for a more modern and progressive place, whilst preserving the strong connection to history and heritage
- A leader in environmental sustainability

2.5.2 Town Centre Masterplan engagement

In mid-2020 the Town hosted a series of focus groups and interviews with community members who have a stake in the town centre (such as property owners, businesses, community groups, those who registered to be involved in engagement from BassenDream, local residents, State Government and potential investors). These sessions built on the detailed findings of the BassenDream engagement process to inform the development of some draft concepts for the future revitalisation of the Bassendean Town Centre. These engagements helped to inform the development of the Bassendean Town Centre Masterplan.



A liveable town centre



A town centre that is open for business



An accessible town centre



A mix of old and new



A green and shaded town centre

2.5.3 Citizens Panel

Consultation with stakeholders and the local community is an integral part of the planning process. Participatory involvement of the community was a key element of preparing this Precinct Structure Plan.

Preparation of Precinct Structure Plans can be complicated, involving many competing interests and outcomes. A Citizen's Panel approach enables a diverse range of voices to be heard, while allowing the panel to explore issues, opportunities and outcomes in detail.

Participation occurred in the form of the establishment of a 'Citizens Panel'. The Panel met on three occasions to consider the challenges, opportunities and design outcomes for the Precinct in depth. The Panel meetings presented opportunity for all participants to hear diverse perspectives and deliberate on the appropriate pathways forward.

| Priorities and opportunities | Summary of messages |
|--|---|
| Enhancing the green network | <ul style="list-style-type: none"> — Desire to see existing mature vegetation retained with new opportunities created for additional canopy tree. — Adding to the existing green network over time to reinforce Bassendean's character as a leafy, naturally landscaped locations that contributes to habitat, nature and sustainability |
| Respecting landscape and topography | <ul style="list-style-type: none"> — Build upon strong existing landscape elements and ensure key sites improve how buildings and spaces relate to topography, open space and places of activity. — Plan a sustainable precinct through effective management of water, waste and energy. |
| Incentivising renewal of key sites | <ul style="list-style-type: none"> — Concentrating development in appropriate locations will achieve a balanced development outcome. |
| Achieving mixed use outcomes | <ul style="list-style-type: none"> — Ensure existing diversity and vibrancy of land use mix continues. — Achieve a vibrant neighbourhood with a mix of business, entertainment, civic and cultural activities supported by diverse housing. |
| Improved streetscapes, paving, landscaping and shading | <ul style="list-style-type: none"> — Support new developments, use and enjoyment through incremental improvement to the comfort and aesthetics of the public realm. |
| Keeping a village feel | <ul style="list-style-type: none"> — Ensure new public realm spaces are well connected, scaled and relate to the fine-grain form of development and land use already present in Bassendean. — Spaces provide for a variety of users, at varying times of the day. — Spaces are safe in use and prioritise pedestrian and cycling movement. |
| A focus of pedestrian and cycle movements | <ul style="list-style-type: none"> — Support long-term mode-shift towards walking and cycling as a means of reducing pressure on the road network and making the Precinct inviting to all. |
| Providing safe, connected travel for all people | <ul style="list-style-type: none"> — Good design of clear, direct, safe and enjoyable movement routes. — Improving routes for walking and cycling, connections to key community facilities and schools. |

| Priorities and opportunities | Summary of messages |
|--|---|
| Improving access to existing public transport infrastructure | <ul style="list-style-type: none"> While the Precinct is well served by existing public transport services, access to these services can be challenging. Improving the limited pedestrian crossings, lack of station facilities, and indirect walking routes to existing public transport infrastructure. |
| Improving connection across key barriers at Guildford Road and the Perth/Midland railway | <ul style="list-style-type: none"> A focus on reconnecting people north of Guildford Road with the Town Centre through improved quality of existing connections. |
| Lively businesses, hotels, cafes and restaurants | <ul style="list-style-type: none"> Create conditions where businesses can be readily established, thrive and generate interest beyond the local area. |
| Greater variety of shops, retail and supermarkets | <ul style="list-style-type: none"> Ensure a diversity of retail spaces is available to support a growing local population. Support improved quality of retail at the western end of Old Perth Road. |
| Greater mix of housing type and choice | <ul style="list-style-type: none"> Provide opportunities for a variety of building forms, with housing types and sizes that reflect Bassendean's diverse community. |



2.5.4 Landowner engagement

Engagement with external stakeholders was conducted prior to preparation of the Precinct Structure Plan. Meetings were held with key landholders to assist in informing initial plan opportunities.

2.5.5 Elected Member engagement

Engagement with the Elected Members of the Town occurred across two (2) Elected Member Briefing sessions. Briefing sessions with Elected Members allowed Councillors to review information, ask questions and discuss approaches to the preparation of the Precinct Structure Plan, prior to its drafting and presentation to Council.

2.5.6 Design Review

A draft of this Precinct Structure Plan was considered by the City of Bayswater Design Review Panel in November 2025. The panel considered the plans responses to the ten design principles outlined in State Planning Policy 7.0. The panel provided feedback on elements of the draft plan that were supported, required further information or where not supported. Key elements of feedback raised by the panel are summarised as follows:

- The panel noted the constructive engagement between the proponent and stakeholders throughout the design review process.
- Endorsed the inclusion of incentives aimed at delivering positive outcomes, such as heritage retention, sinking of power infrastructure, and sustainable design measures.
- Supported provisions that enable flexibility in building design to accommodate changing uses over time.
- Agreed with the approach to maintain a mix of uses along the main street to encourage activity throughout the day.
- Supported the concept of linking activity nodes within the precinct to improve pedestrian movement and foster community interaction.

2.5.7 External engagement

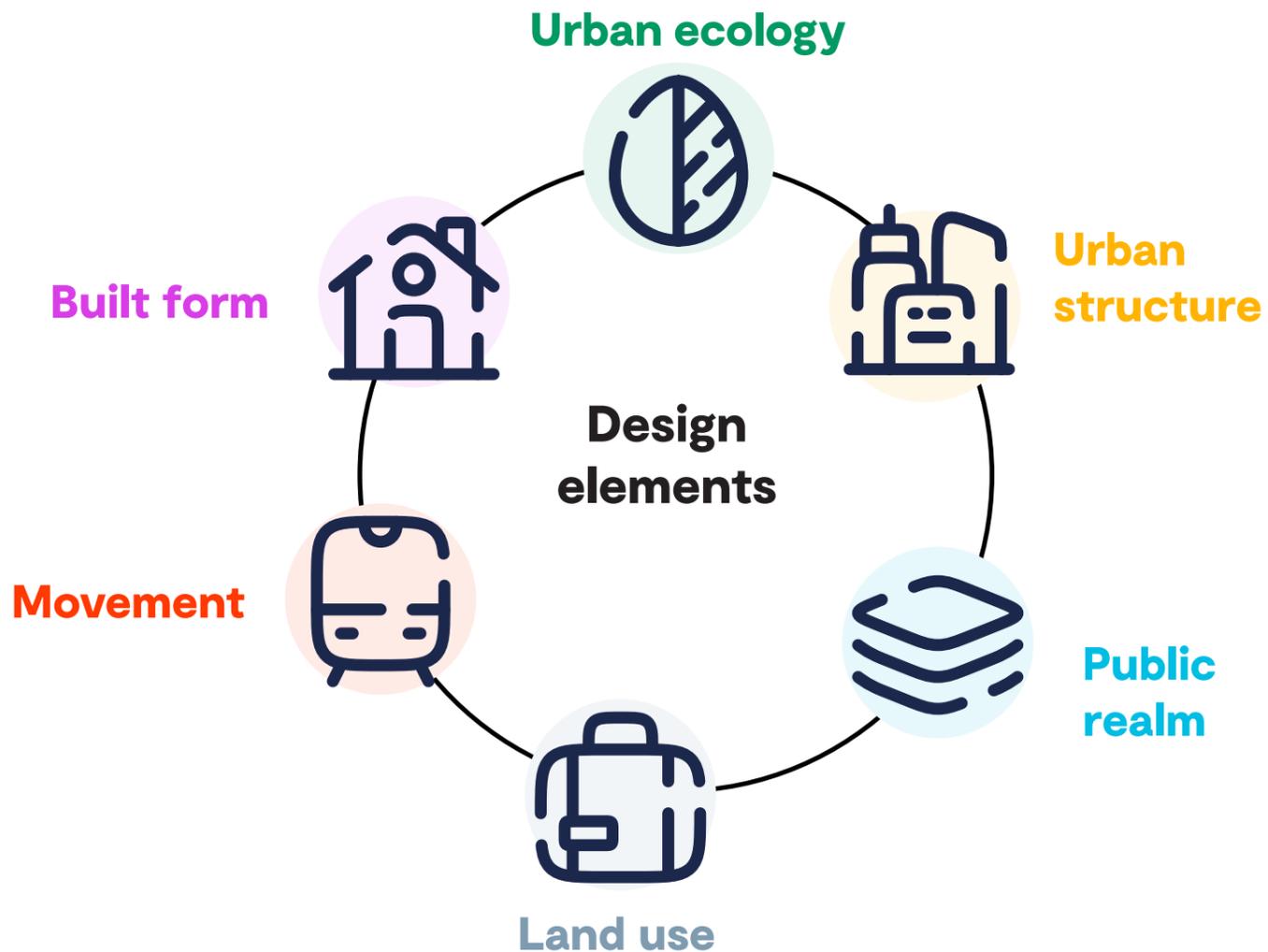
External engagement with key government stakeholders, the Town's Executive and Elected Member body, Design Review Panel and key landowners/community stakeholders is included in Appendix A.

3. Opportunities and constraints

Planning for the future of the Bassendean Precinct will be successful if it responds well to its context, existing challenges, community and is aligned with a broader land use planning strategy.

The design responses identified in this Precinct Structure Plan are a product of community and stakeholder engagement, planning strategy and analysis of place context. The Precinct Structure Plan reflects the community views expressed through the Town's adopted Local Planning Strategy, Town Centre Masterplan, and the voice of the Citizen's Panel. This feedback, together with a robust assessments of urban ecology, urban structure, public realm, movement, land use, and built form have shaped the outcomes of the plans design responses.

Everyone involved in the future planning of Bassendean wants the place to be successful, interesting and a great community asset. Designing for the future of the Precinct will involve a range of challenging and related considerations that need to be balanced. This section describes those challenges and how the plan addresses long-term need.



3.1 Urban ecology

Consideration of urban ecology supports the protection and enhancement of ecological systems within precinct design. This involves developing an understanding of the natural environment of the Precinct within its broader context.

Opportunities and constraints

The Bassendean Precinct is one of the eastern suburbs most unique and desirable neighbourhoods. It has a richness in ecological values – across streets and public spaces – that is defined by generous landscaping (both native and ornamental), varied topography and a strong connection to Aboriginal and historic heritage.

However, parts of the Bassendean Precinct are characterised by inconsistent tree canopy and water management risks. Current sustainability requirements are not best practice, while management of storm water and drainage does not maximise opportunities to improve the local water-cycle. Recognition of Aboriginal heritage is also limited.

Key priorities

Community engagement and the Town's broader strategic framework highlight the following stakeholder priorities for the precinct.



Enhancing the green network A strong desire to see existing mature vegetation retained with new opportunities created for additional canopy tree. Adding to the existing green network over time to reinforce Bassendean's character as a leafy, naturally landscaped locations that contributes to habitat, nature and sustainability



Respecting landscape and topography Build upon strong existing landscape elements and ensure key sites improve how buildings and spaces relate to topography, open space and places of activity. Plan a sustainable precinct through effective management of water, waste and energy.



Celebrating cultural heritage Celebrate Bassendean's cultural heritage and character through preservation of protected places, integration of built-form and landscape character elements, and celebrate local history and culture through arts, activation and design.

Landscape and topography (C1.1.1)

Town Centre

The landscape and physical character of the Town centre varies significantly between street blocks. Some areas open generously to open space areas like BIC Reserve and Bassendean Oval and have a strong connection to landscape. Other streets, including sections of Old Perth Road lack space for greenery, while changes in topography from Old Perth Road to Guildford Road are not necessarily apparent in the areas built-form or movement network.

This Precinct Structure Plan responds to these challenges through bespoke development provisions for key sites linking to open space areas, such as the Council site opportunity site and Bassendean shopping centre opportunity site, together with public realm improvements to sections of Old Perth Road.

Success Hill

The Success Hill Frame area has varying levels of landscape character. Land along Thompson Road benefit significantly from mature trees on private land that extend over the public street. Mature native trees to rear yards and along Nurstead Avenue are visible from various points in the public realm.

Screening landscaping to rear boundaries facing Guildford Road, Lord Street and the railway provide a sense of seclusion within the area. A lack of public and private landscaping on Whitfield Street/ Station Street significantly diminishes landscape amenity west of West Road/Lord Street.

Specific built-form and landscaping provisions relating to the Success Hill Frame area are described further in this section.



The relationship between key public spaces and areas of activity could be improved



There are pockets of the Town Centre with high ecological and landscape character



Streets in the Success Hill Frame have a well defined landscape character

Green network and water cycle (C1.1.2, C1.1.3, C1.3.3, C1.3.4)

Green network

The green network in the Precinct contributes significantly to the town's identity, shaped by its proximity to the Swan River, tree-lined streets and inter-connected parks and reserves. Insight on the existing green network is provided in the Environmental Assessment at Appendix C. An important challenge facing the Town's green network is balancing urban infill, densification and development with the protection of existing green corridors and canopy coverage. As the Precinct develops, there is a risk that increased development may fragment or reduce green networks; even if existing green spaces are protected. This Precinct Structure Plan safeguards existing networks by:

- Maintaining and enhancing the network of mature trees, particularly along Old Perth Road.
- Creation of additional green spaces (refer Public Realm Plan) and improved access to existing green areas at the Council site and Bassendean Hawaiians shopping centre.
- Introduction of green building and sustainability rating requirements for major new developments.

Ground and surface water

The water cycle is the movement and management of water within urban environments. It plays an important role in the Town's environmental health and green network particularly due to the Town's proximity to the Swan River. The Precinct's shallow water table results in design challenges for new buildings seeking to construct basement structures (refer Local Water Management Strategy at Appendix F). This plan responds to this challenge by applying water sensitive urban design principles to the planning of developments, and through considering groundwater conditions in evaluating development yields.

Habitat conservation (C1.1.4)

Habitat conservation in the context of the Precinct relates mostly to the protection of mature habitat trees. Figure 14 depicts mature vegetation over 3.0 m in canopy height. Trees exceeding 3.0 m are most likely to provide fauna habitat. The majority of these trees are on public open space reserves, residential properties within the Success Hill Frame and areas of parking adjacent to key sites like the Bassendean Hotel and Bassendean Hawaiians shopping centre (along West Road). Protection of this potential habitat has informed planning for Opportunity sites (refer 3.2 – Urban Structure).

Cultural heritage (C1.2.1, C1.2.2)

The heritage values of the Precinct are described in section 2.3.6, with places already listed on the State Register of Heritage Places and the Town's Heritage List (refer Figure 6).

The two biggest challenges are the retention and conservation of the existing cultural heritage values located along Old Perth Road and the integration of built-form character elements that reflect Bassendean's cultural history. This is particularly challenging in the Success Hill Frame where protected places are limited but there are evident character values. Some properties along Old Perth Road are not included on the State Register of Heritage Places or the Town's Heritage List. These buildings represent the traditional form of commercial tenancies and contribute to Old Perth Road's role as a traditional high street. This Precinct Structure Plan includes provisions that seek to reinforce key cultural heritage and character values though:

- Provisions relating to street wall/façade heights, ground floor design treatments and scaling of buildings along Old Perth Road and nearby active areas (refer section 3.6 – Built form).
- Bespoke provisions for the design and siting of new development in the Success Hill Frame (and the Nurstead Heritage Area specifically) aimed at preserving key character values relating to built form scale and landscape character.

Energy and resource conservation (C1.3.2, C1.3.5)

Incorporating environmentally sustainable design features in new buildings helps to reduce environmental impacts. Emphasising energy efficiency and other sustainable design elements in new developments is essential for lowering the Precinct's greenhouse gas emissions and moving towards a low-zero carbon future. This Precinct Structure Plan introduces requirements for developments to achieve higher environmental performance, aiming to reduce greenhouse gas emissions. These requirements also offer flexibility to enhance performance, such as promoting the use of rooftops for biodiversity or energy generation and encouraging identified Opportunity sites (refer section 3.2 – Urban Structure) to meet elevated environmental performance standards. Mode-shift targets identified in section 3.4 – Movement aim to reduce resources used for transport.



Conservation and celebration of cultural heritage is a key outcome of this plan



Character of non-protected places should be reflected in the design of new development



The green network consists of a range of public open space areas, together with significant vegetation on both public and private land

Tree retention (C1.3.1)

Vegetation cover and tree canopy are vital for enhancing ecological quality, mitigating the public health effects of climate change, and improving the visual appeal of spaces. To achieve these benefits, additional greening within the Precinct should be pursued.

Section 3.3 - Public realm describes the key greening initiatives along Old Perth Road, through creation of new public spaces that support landscape quality and amenity. Incentives for Opportunity sites (refer Section 3.2 - Urban structure) considers how new development can contribute to increased canopy in the public realm and protection of significant vegetation on key sites. This plan promotes the use of water-sensitive urban design principles, such as rooftop gardens and rain gardens, to actively integrate nature into urban settings.

Further, the Public Realm Plan together with the benefits framework for Opportunity sites will seek to deliver enhanced tree retention and new canopy in key locations. This includes through new linear open space adjacent to the Whitfield Street and Bassendean Hawaiians shopping centre.



Opportunities for new canopy can be created at key locations along Old Perth Road



Canopy can be incrementally improved along some streets

Figure 14 Parks and mature vegetation (+3m in canopy height)



3.2 Urban structure

A well-considered urban structure is established through careful consideration of the Precinct's physical, cultural and economic context. Urban structure should be flexible and legible, and designed to support the intended built form of the precinct.

Opportunities and constraints

The urban structure of the Precinct is defined by series of nodes. The character of each node reflects the areas cultural heritage, approach to land use and built-form and relationship to public spaces established by earlier land fragmentation. The Precinct's urban structure is anchored by two distinct nodes: the Bassendean railway station and the Bassendean Hawaiians shopping centre. The shopping centre, while offering strong land use mix, employment and activity, disrupts the Precinct's overall cohesion through its dominance. Despite this, the shopping centres size and mid-precinct location offers significant redevelopment opportunity.

The Precinct is divided by Guildford Road, and beyond this, separated from residential neighbourhoods by the Perth-Midland railway. These barriers separate urban form, present movement challenges and generally reduce amenity and accessibility.

Key priorities

Community engagement and the Town's broader strategic framework highlight the following stakeholder priorities for the precinct.



Incentivising renewal of key sites The Town Centre Masterplan identified several key locations where greater height and development intensity was preferred. Concentrating development in appropriate locations will achieve a balanced development outcome. Key Opportunity sites have the greatest potential to transform the Precinct in the short-term.



Achieving mixed use outcomes Bassendean is already a vibrant mixed use area with diverse land use, activity and housing. Ensuring this diversity continues into the future is a key priority of stakeholders. Achieving a vibrant neighbourhood with a mix of business, entertainment, civic and cultural activities supported by diverse housing is a key priority.



Leveraging development to contribute to improved public realm Improving the place quality of the Precinct relies on great buildings, successful land use, easy movement and quality public realm. To ensure all these aspects work together, major new developments are incentivised to contribute to improvement of the public realm.

Intended future character (C2.1.1)

Development in the Precinct is defined by the linear structure of Old Perth Road. This form results in growth opportunities that border sensitive lower-density residential areas, including unique areas of intact heritage and character fabric, as well as public open spaces. The definition Old Perth Road provides to arrangements of land use, built form and movement provided is a key part of the Precinct's identity. The Precinct's boundary is deliberately constrained to only a small number of properties south of Old Perth Road to preserve this linear form and protect in-tact fabric. Overall, the design responses of this Plan seek to:

- Focus movement and activity along Old Perth Road and West Road.
- Consolidate activity at either end of Old Perth Road.
- Continue the existing character of compact, mixed-use form along Old Perth Road, together with specific built-form responses for Opportunity site.

The linear form of Old Perth Road presents challenges in ensuring continuity in built-form and good levels of activation. Guildford Road and the Perth-Midland railway act as a barrier to the continuation of the Town Centres urban structure. This Precinct Structure Plan, primarily through interventions in the public realm and movement network, plans for a series of nodes aimed at providing focal points along the existing linear form.

- **Activity node** Providing a focal point at the key arrival space to the Precinct adjacent to Bassendean station. Intended to support the majority of night-time activity, the node will be people and activity focused.
- **Village node** A series of spaces that link key civic and community functions with activity and business spaces to the west and east. Adjacent to the Council site, public realm improvement at the Village node provide an opportunity for an improved relationship with BIC Reserve.
- **Mixed street node** Provides a connection between Bassendean shopping centre and Bassendean Oval, supporting future improvement at the Oval and recognising the relationship between active land use and recreational spaces.

The design and intended character of these nodes is expressed in more detail in section 3.3 – Public realm.

Street blocks, destinations and focal points (C2.1.2, C2.1.3)

Street blocks in the Precinct are designed around a modified/interrupted linear grid, with Old Perth Road and West Road forming key axis. The interruption of BIC Reserve, Bassendean Oval and Guildford Road result in a series of cul-de-sacs at James Street, Whitfield Street and Hamilton Street. Most street blocks are permeable, through a combination of laneways, arcades and intersection with public open space. However, the lack of mid-block connections between Hamilton Street and Bassendean Oval (north of Old Perth Road) diminishes connectivity between these areas.

The built form analysis contained in this Precinct Structure Plan demonstrates that existing block patterns are regular and adaptable enough to support redevelopment and intensification. It is important that prominent large lots are protected from subdivision and lots with amalgamation potential are identified. Provisions ensuring the appropriate subdivision of land, in accordance with the objectives of the Precinct Structure Plan are contained in Part 1 – Precinct Structure Plan.

A combination of development incentives, more generous (reduced) setbacks and public domain interface provisions, this Precinct Structure Plan ensures development is focused at the following locations:

- Old Perth Road between Bassendean railway station and James Street
- Old Perth Road between Whitfield Street and West Road, and West Road to Guildford Road.



Continuous, well-shaded connections promote movement, activity and interest



Break in built-form and activity on Old Perth Road lead to reduced movement and activity

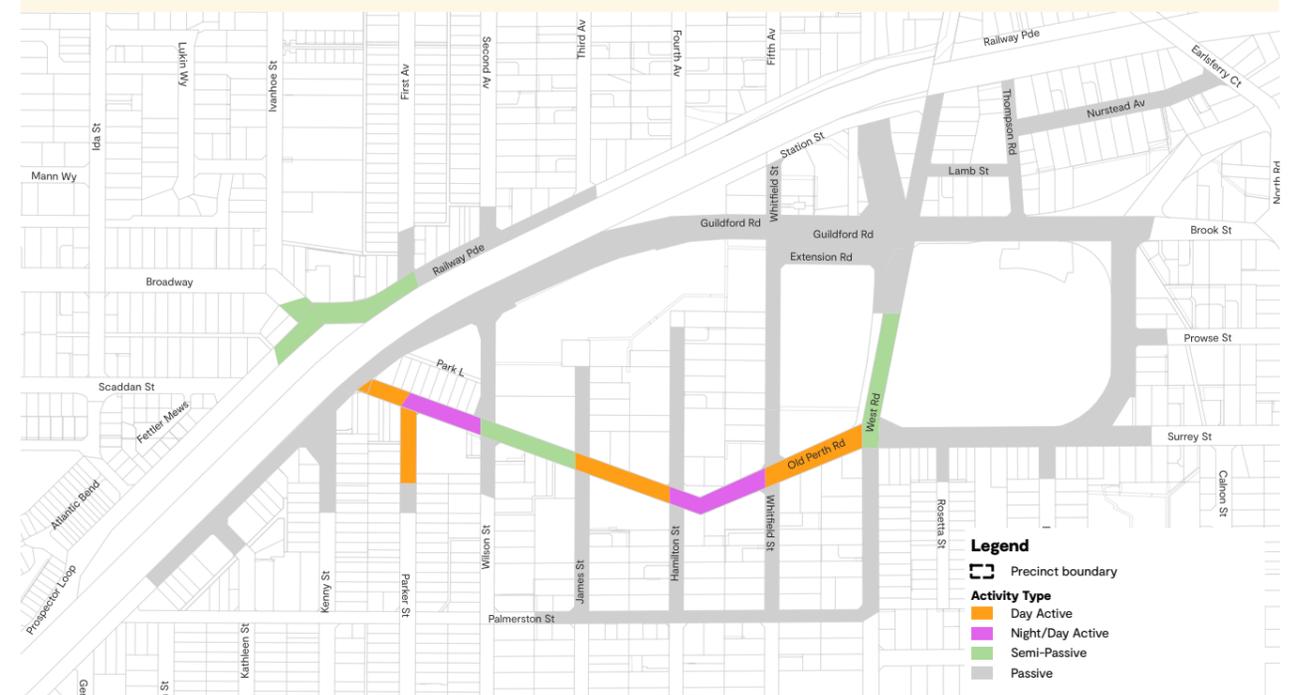
Accommodating intended land use (C2.3.1, C2.3.2, C2.3.4)

This Precinct Structure Plan applies to land within the precinct boundary; defined primarily by the earlier Bassendean Town Centre Masterplan. Consistent with the Town's Local Planning Strategy, testing of the development heights and plot ratio demonstrate a maximum developable yield of approximately 1,436 dwellings in addition to the approximately 557 dwellings in the Precinct at present.

Large format uses (C2.3.3)

The Precinct accommodates existing large format use at the Bassendean Hawaiians shopping centre (currently Coles supermarket). The site is serviced by Extension Road, and access via Whitfield Street and via a slip lane directly from Guildford Road (westbound). This Precinct Structure Plan envisages the expansion of the Bassendean Hawaiians shopping centre to accommodate additional retail and commercial floorspace, together with high-density residential dwellings. This may include further large-format space (potentially a second supermarket), though significant reconfiguration of the shopping centre would be required. Other forms of large-format uses (i.e. bulky goods showrooms) are not expected within the Precinct.

Figure 15 Activity area types



Integration with public space (C2.3.5)

This plan anticipates various interventions into the public realm, designed to ensure integration with built-form. This includes new connections to spaces, interaction between open space areas and ground floor tenancies and future mid-block links connecting key areas. These interventions are described in section 3.3. – Public Realm. These interventions will ensure a place-responsive urban structure which celebrates key destinations and links movement between destinations with appropriate landscaping, activity and built-form.

Opportunity sites (C2.4.1, C2.4.2)

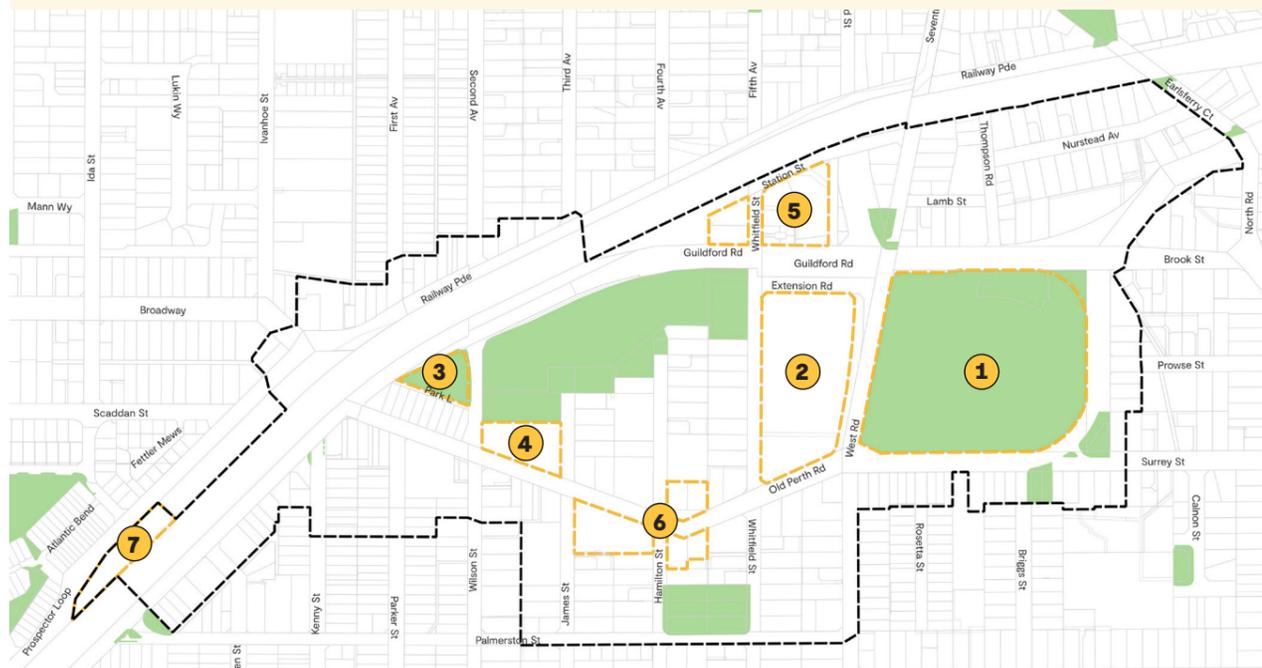
There are several sites within the Precinct that present significant opportunity for development in the short-term. These sites are described as Opportunity sites and have been identified for their potential for redevelopment, large size, simplified tenure arrangement and capacity to accommodate more intense development outcomes. It is crucial to ensure that development on Opportunity sites achieves a high design standards, offers diverse and affordable housing options, and includes cultural and commercial amenities to create a balanced community outcome. Specific controls will be required to ensure these sites contribute value to the Bassendean precinct and enhance its distinctive character, while also accommodating significant development that delivers new amenities and community priorities.

In recognising the need for high quality outcomes, development incentives for community benefits are offered in Part 1 for each of the Opportunity sites. The development incentives are in the form of additional building height. This additional benefit is incentivised against direct or in-kind contributions towards:

- Improvements to the public realm in the spaces nearby or adjacent to each Opportunity site.
- Provision of public open space areas and/or mid-block links on specific Opportunity sites.
- Provision of public facilities on land where relevant to the functioning of adjacent public land, such as community facilities.
- Provision of diverse and/or affordable housing.
- Conservation of the environment through higher design and environmental sustainability standards.

Provision of these benefits is consistent with the provisions of the R-Codes (Volume 2) and SPP3.6 – Infrastructure contributions in that the contributions are voluntary and relate to the additional development potential available to each site.

Figure 16 Opportunity sites



1 - Bassendean Oval

Bassendean Oval is the most significant asset within the Precinct. It provides extensive open space, as well as a place for recreation, sports and events.

The Town has endorsed a Concept Masterplan for redevelopment of Bassendean Oval. The Concept Masterplan identifies redevelopment of existing sporting facilities at the site, to be provided with new open space, pedestrian connections and opportunities for cultural, civic, commercial and residential living opportunities.

Redevelopment on the Bassendean Oval site presents opportunities to enhance community access to the existing reserve, provide new housing and improve the spaces relationship with the surrounding area; in particular the Bassendean Hawaiians shopping centre.

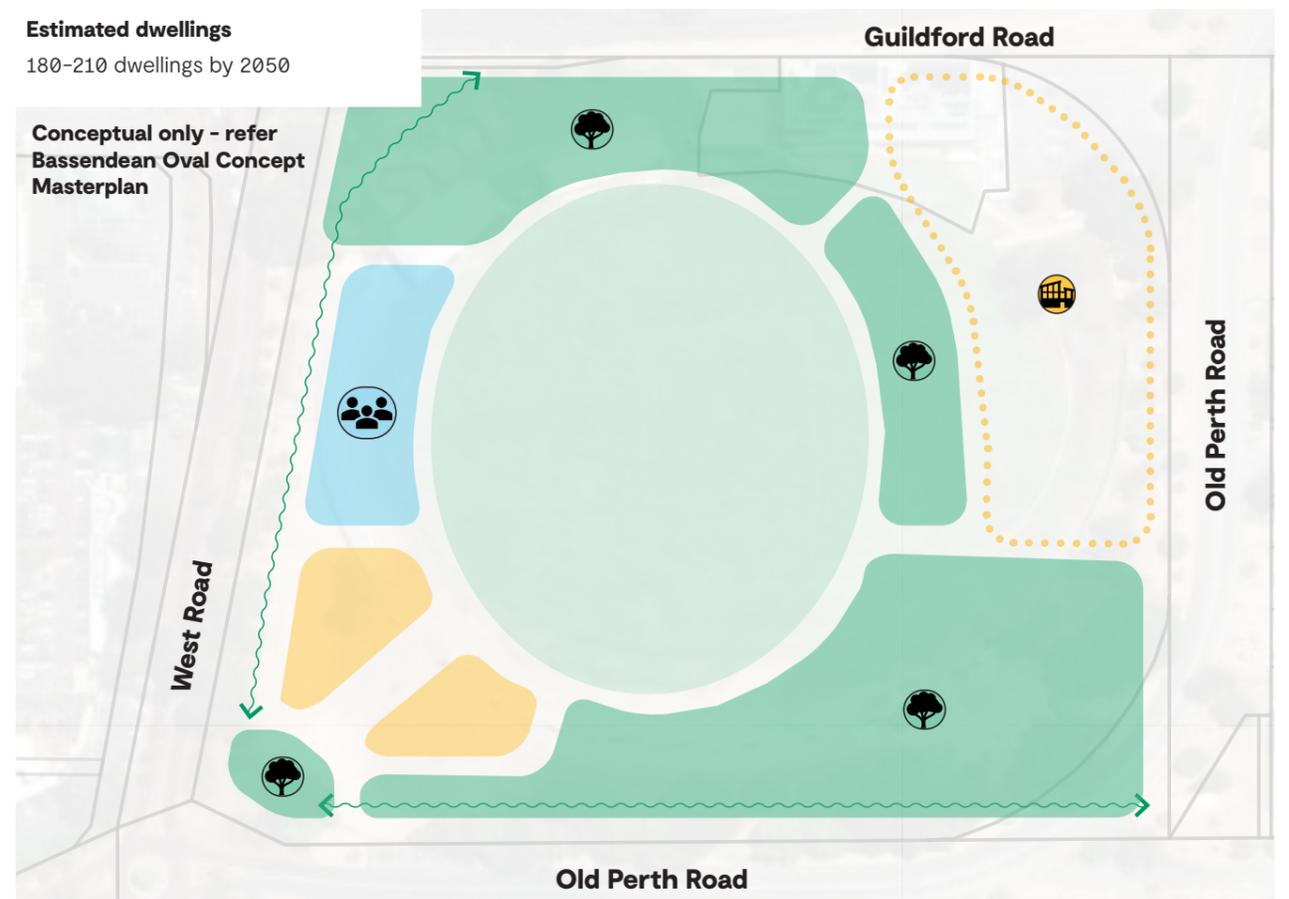
Desired outcomes

- Retention and protection of existing cultural heritage places, including grandstand and entry gates.
- Achieve redevelopment of existing sporting facilities, in conjunction with new spaces for cultural, civic, commercial and residential opportunities.
- Strengthen connection and relationship with Bassendean Hawaiians shopping centre and interface with West Road.
- Act as a catalyst for further transit oriented development within the wider Precinct.
- Increase the quality and functionality of open space offered on the site, including greater access to green spaces for the public.

Estimated dwellings

180–210 dwellings by 2050

Conceptual only - refer Bassendean Oval Concept Masterplan



Legend

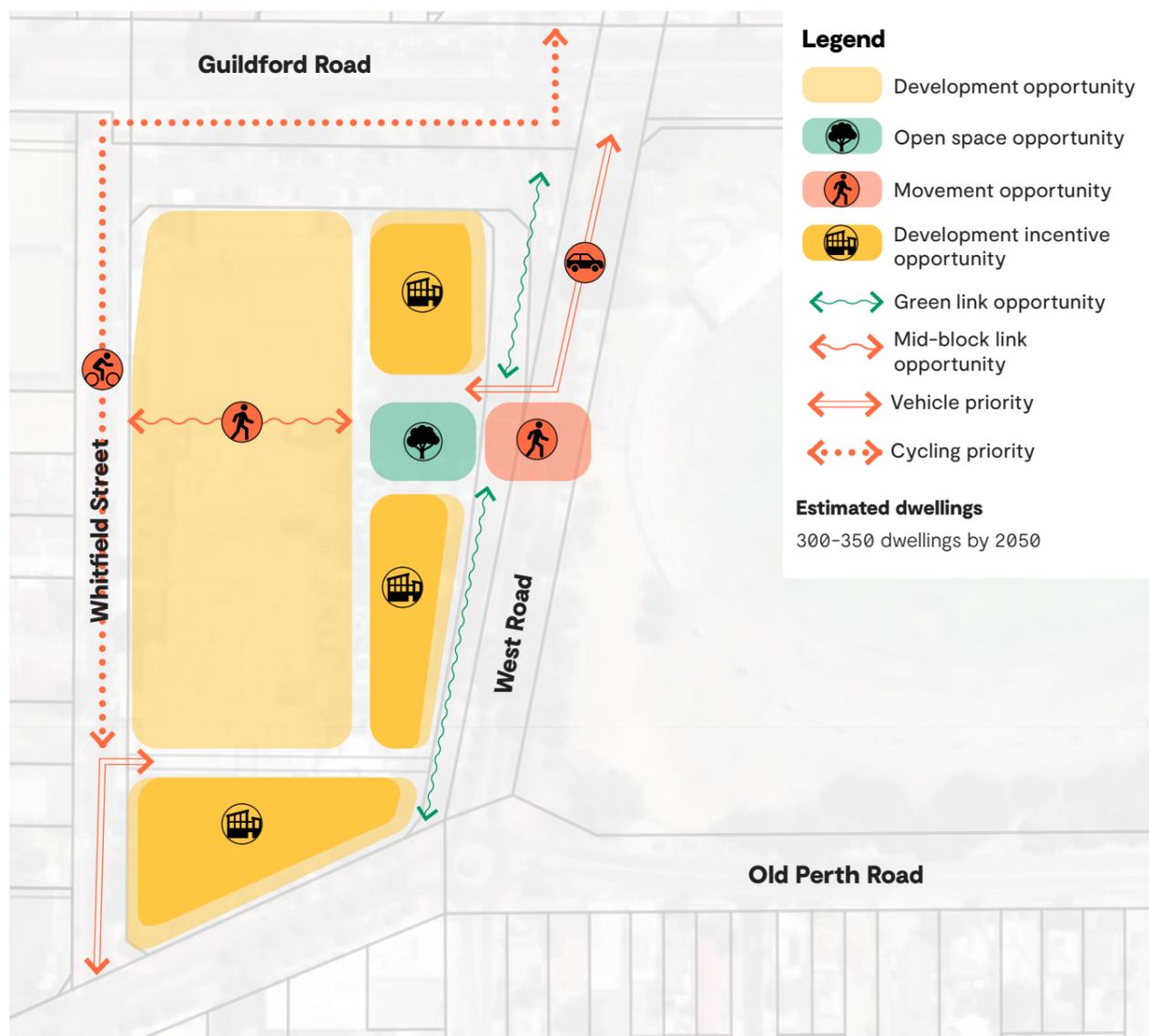
- Development opportunity
- Open space opportunity (includes parking and heritage retention)
- Sporting/community facility opportunity
- Potential development opportunity
- Green link opportunity

2 - Bassendean Hawaiians shopping centre

Bassendean Hawaiians shopping centre is a critical economic asset to the Precinct. It's on-going operation and longer-term expansion provides access to goods, services and employment for the neighbourhood. There are currently extensive surface car parking areas surrounding the centre that present opportunities for redevelopment, with opportunities to provide for new, mixed-use development and improved interface with West Road and Bassendean Oval.

Desired outcomes

- Redevelopment over existing at-grade parking areas to provide activity edge to Old Perth Road and West Road.
- Upgrade West Road to act as key activity area, interfaced with existing retail and mixed use development. Sleeve existing shopping centre with mixed use development. Incorporate new retail and parking entries. Create new dynamic street edge with southern portions of West Road.
- Improve connectivity and accessibility across West Road, providing an interesting interface with new development at Bassendean Oval and provide a mid-block link to Whitfield Street.
- Preserve existing mature trees at West Road and on the site.

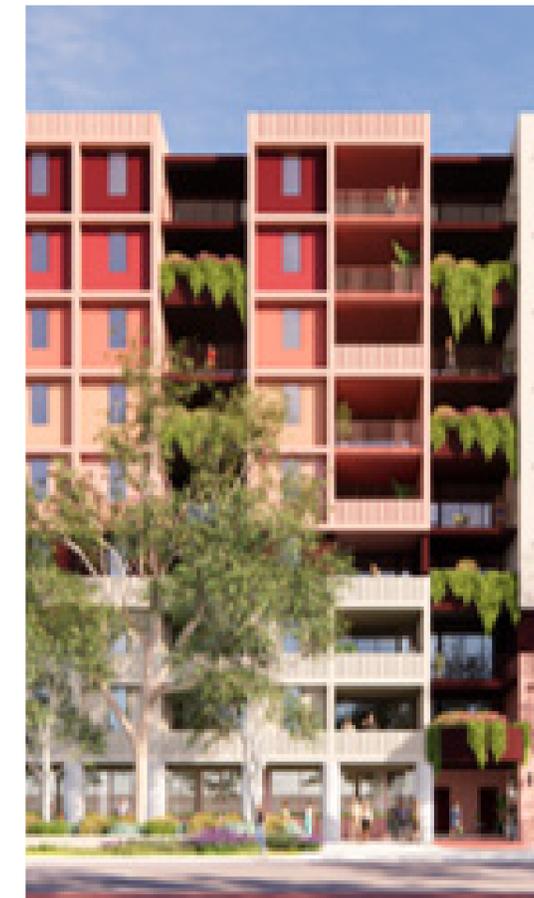


3 - Park Lane

Park Lane provides an opportunity to transform an existing underutilised parking area into a significant mixed use development providing a mix of housing, including social and affordable housing. This plan anticipates retention of some existing mature vegetation on the site, together with strong built-form relationship to Wilson Street and BIC Reserve. A high water table and construction cost considerations means parking is likely to be at grade, under-croft or in a mezzanine level.

Desired outcomes

- Incentives focused on provision of diverse housing.
- Provide strong definition of ground floor use to Wilson Street, to promote interaction with BIC Reserve.
- Improve the amenity of Park Lane, recognising its service function, but also the potential for land use activation at the rear of existing buildings fronting Old Perth Road.
- Encourage residential dwellings, that provide a mix of housing choice, limit private vehicle use and protect on-site vegetation.

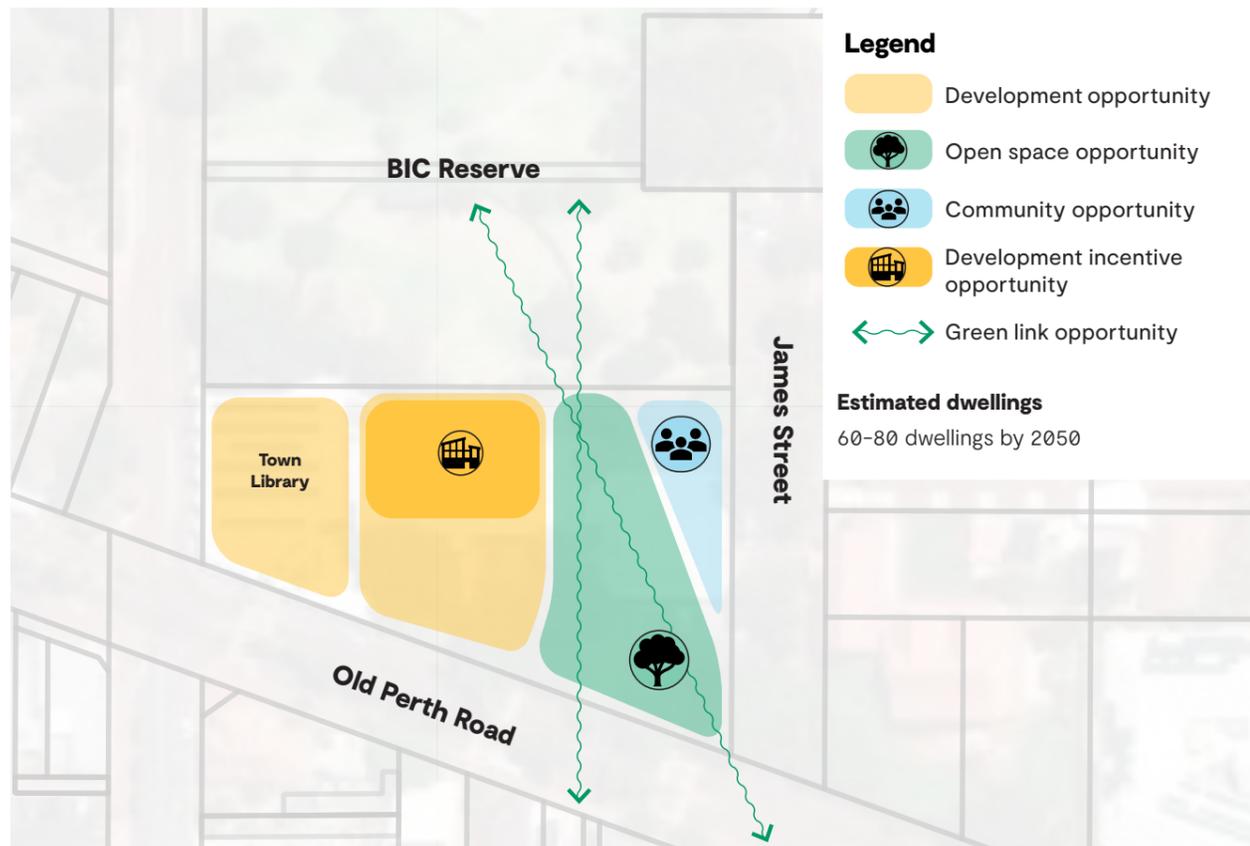


4 - Council site

The Bassendean Community Hall, Senior Citizens and Community Centre and Council offices (46 Old Perth Road) will likely reach end-of-life during the implementation of this plan. This plan contemplates redevelopment of the site, with the potential to integrate existing civic functions with a broader range of land use and the excellent nearby amenity at BIC Reserve.

Desired outcomes

- Retain some existing mature trees and green space fronting Old Perth Road.
- Ensure an improved built-form relationship to BIC reserve to promote diverse activity and improve surveillance.
- Provide landscaped connection between Old Perth Road and BIC Reserve.
- Design the public realm to negotiate changes in temporary and integrate public realm activity with adjacent educational, civic, hospitality and retail land use.
- Provide a mix of commercial, residential and civic functions in new buildings.



5 - Whitfield Street

The Whitfield Street Opportunity site currently comprises low-rise residential dwellings predominately offered as public and social housing. The Whitfield Street area is isolated from nearby activity areas, open space and railway transport due to an absence of safe/formalised crossing points. Redevelopment in this area should be delivered in conjunction with movement improvements, and improvements to the public realm.

Desired outcomes

- Create an opportunity for diverse, high-density housing nearby to public transport infrastructure.
- Improve accessibility and the quality of the public realm north of Guildford Road through creation of a linear parkland from West Road through to the Perth-Midland railway.
- Ensure new development mitigates amenity impacts, including noise and vibration exposure from the Perth-Midland Railway and Guildford Road.
- Provide opportunities for social gathering and for improved access to Success Hill station through a pedestrian underpass below Lord Street, connecting to a new station arrival linear parkland.



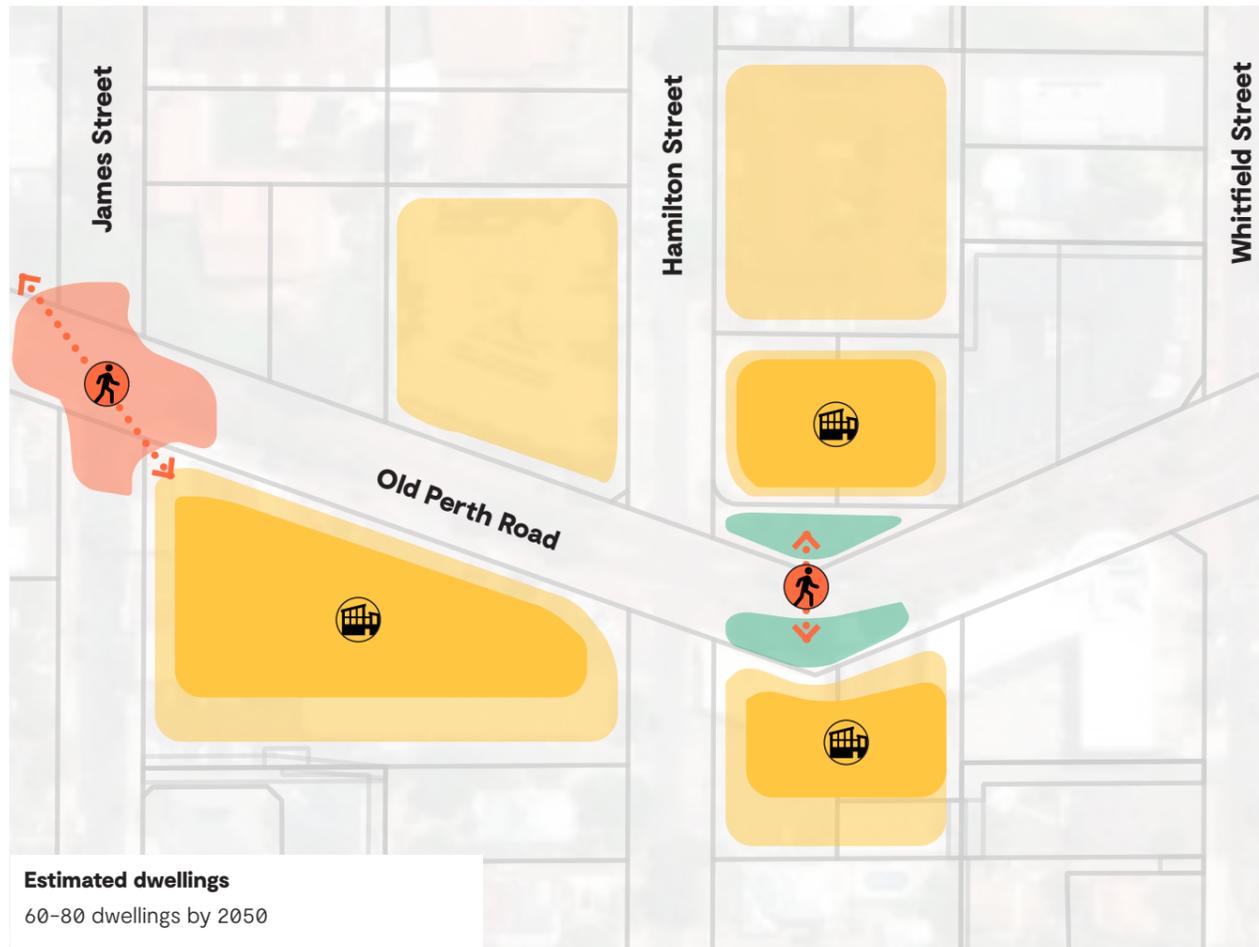
6 - Village sites

47 Old Perth Road is a substantial landholding located centrally on Old Perth Road. It sits at an equal distance to the activity node closer to Bassendean railway station and Bassendean Hawaiians shopping centre. The site is joined by other key landholdings on the northern and southern corners with Hamilton Street. Existing low-rise commercial buildings and parking areas offer a mixed relationship to the street. Redevelopment of the land as a modern mixed-use development, supported by improvement to the adjacent public realm presents opportunity for the site to link activity along Old Perth Road.

Desired outcomes

- Ensure sunlight access to adjacent properties to the south is reasonably preserved.
- Ground floor active uses including shop retail and hospitality venues supported by weather protection and landscaping improvements in the adjacent public realm.
- Street façade heights and arrangement to reflect existing fine-grain of Old Perth Road and provide transition from mid-rise buildings to the east.
- Provide for pocket park at intersections with Hamilton Street and James Street to enable connection to broad public realm network.

Legend



Estimated dwellings
60-80 dwellings by 2050

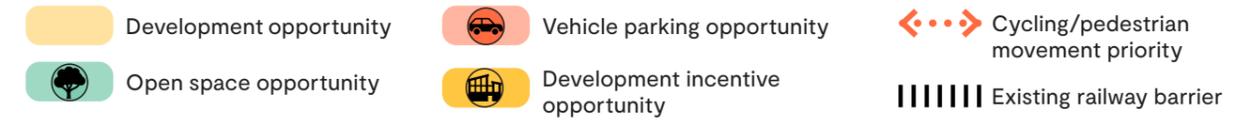
7 - Station carpark

Bassendean station is serviced by a substantial surface car park to the north of the Perth-Midland railway. With the opening of the Morley-Ellenbrook railway line, demand for parking from users in suburbs north of Bassendean has diminished. There is an opportunity to re-imagine a portion of the parking station for a greater use.

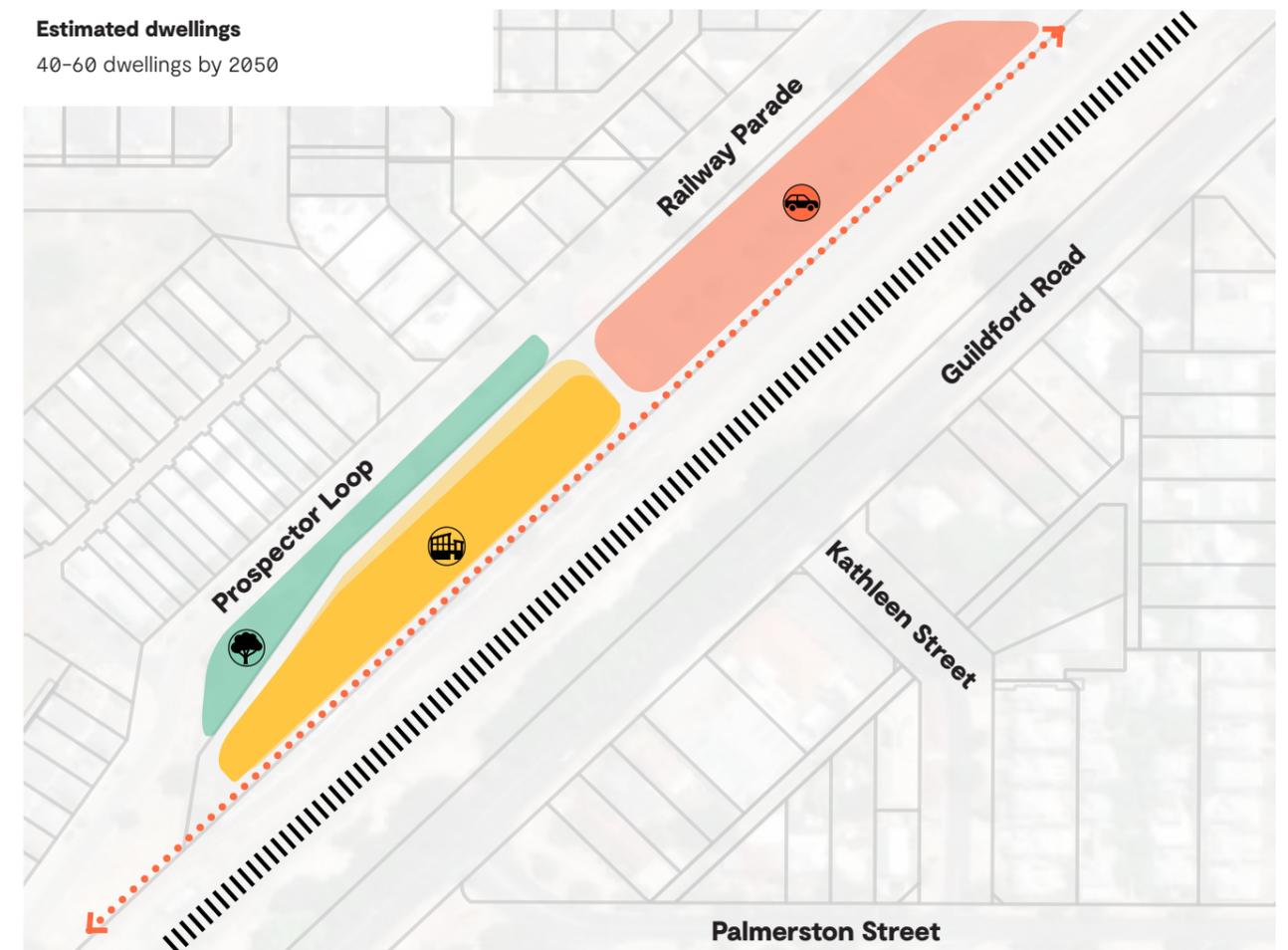
Desired outcomes

- Residential development in a variety of housing forms to transition to existing lower-density residential development.
- Ensure new development mitigates amenity impacts, including noise and vibration exposure from the Perth-Midland Railway and Guildford Road.
- Adoption of shared parking arrangements, enabling spilt use between commuters and residents.
- Improved landscape amenity to adjacent streets and enhanced pedestrian connectivity to existing cycle paths.

Legend



Estimated dwellings
40-60 dwellings by 2050



3.3 Public realm

A well designed public realm supports community well-being. It provides spaces and opportunities for relaxation, recreation, contemplation and connection to nature, and can promote walking and cycling.

Opportunities and constraints

The public realm quality of the Precinct is defined through the area's rich history, traditional urban structure (high street, dedication of public parkland) and consistent application of materials and landscaping. Significant mature trees along West Road, generous landscape character at BIC Reserve and Bassendean Oval and consistent design treatments along Old Perth Road are a major public realm asset to the Precinct.

However away from these areas, this consistency and strong character diminishes. Guildford Road and Lord Street have poor public realm qualities, while an absence of public spaces in the Success Hill Frame is at odds with the area's strong movement potential. The quality of public realm north of the railway is interrupted by a large area of passenger car parking and bus entries that make the area uninviting to pedestrian movements.

Key priorities

Community engagement and the Town's broader strategic framework highlight the following stakeholder priorities for the precinct.



Improved streetscapes, paving, landscaping and shading Support new developments, use and enjoyment of the Precinct through incremental improvement to the comfort and aesthetics of the public realm.



Keeping a village feel Ensure new public realm spaces are well connected, scaled and relate to the fine-grain form of development and land use already present in Bassendean. Ensure spaces provide for a variety of users, at varying times of the day. Ensure spaces are safe in use and prioritise pedestrian and cycling movement.



Celebrating cultural heritage and protecting trees Plan the public realm to respond to Bassendean's rich cultural heritage, both historic and Aboriginal, and create new opportunities to grow the urban canopy.

Providing a diversity of public open space (C3.1.1, C3.1.2)

The Precinct is well served by public open space. Diverse parkland (BIC Reserve, Palmerston Square), active sports zones (Bassendean Oval) and micro-open space areas including parklets and alfresco provide a diversity of spaces within the precinct. The Town's Public Open Space Strategy identifies both an inadequate provision of open space, and elements of improvement to the quality of some open space areas.

Bassendean Oval has enormous potential as a key public space, providing a range of functions. Its current form as a closed active sports and events space limits its value to the wider community. Vegetation framing the oval's reserve provides visual definition to Old Perth Road and West Road and provides landscape value to the wider Precinct.

The Bassendean Hawaiians shopping centre provides a different form of public realm; that is internalised to the site, both inside and outside. An absence of public open space limits public realm amenity in the Success Hill Frame, despite proximity to Bassendean Oval.

This Precinct Structure Plan includes provisions that will enable transformation of the public realm, aligned with redevelopment on Opportunity sites. Provisions ensure new development is incentivised to contribute to public realm improvements, which in-turn support the diversity and success of land use on the adjacent land. The strong relationship between built-form, public realm and land use improvement ensures public and private investment works together to improve the precinct.



Public realm that enhances access to Bassendean and Success Hill stations



Integration of built-form and land use with the enjoyment of the public realm



Providing spaces that promote sustainable movement and longer stays



Spaces that provide for all user types, and enjoyment at different times of the day

Activity node and link

The activity node forms the most recognisable and active area of the precinct. It is anchored by existing entertainment and retail businesses that can support activity in the public realm. Interventions to the design of Old Perth Road adjacent to the existing and future development will support greater variety of land use, encourage activation and events, support alfresco dining and allow for greater greenery.

Modifying Old Perth Road west of Wilson Street will enable the provision of greater outdoor space, improved landscaping and tree canopy, while still providing short-term parking for adjacent businesses.



Figure 16 Activity node and link

Village node

Village node forms the mid-precinct connection between the activity node and Bassendean Oval; which are separated by over 500 m. Public realm improvement in this location includes an enhanced relationship between Old Perth Road and BIC reserve, together with pocket parks adjacent to opportunity sites. These interventions will improve movement relationships between spaces, promote integration of public/civic and private land use and provide a mid-precinct focal point that balances areas of high activity to the west and east.



Figure 17 Village node

Mixed street node

Built-form on the Bassendean Hawaiians shopping centre site should be shaped to improve the connection with Bassendean Oval, and provide new, generous, opportunities for privately owned public realm.

In conjunction with changes to built-form on the shopping centre site, modifications to West Road should occur to promote safer pedestrian movement, retained and improved greenery and landscaping and to provide new external public realm areas. Mature trees should be preserved and become the focus of a future on-site plaza and green link along West Road.

Legend

- 1 Improved pedestrian crossing
- 2 Public plaza opportunity
- 3 Green-link opportunity



Figure 18 Mixed street node

Success Hill node and link

Improving access to Success Hill station, as well as to the Whitfield Street opportunity sites was a key elements of stakeholder feedback. The absence of public open space north of Guildford Road and limited access to Success Hill station provides opportunity to improve the public realm. In conjunction with redevelopment at Whitfield Street, this precinct structure plan provides for a linear greenway between Guildford Road and Success Hill station, supported by an improved station arrival space. Appropriate interface with redevelopment of sites on Whitfield Street and Station Street will support surveillance and use of the linear greenway.

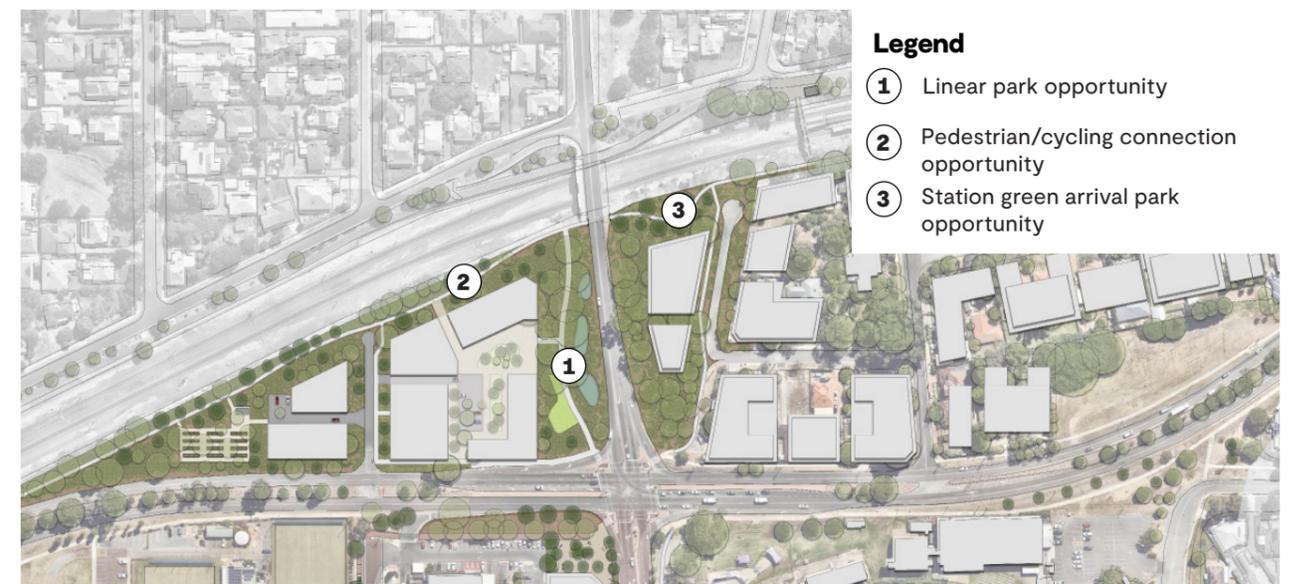


Figure 19 Success Hill node and link

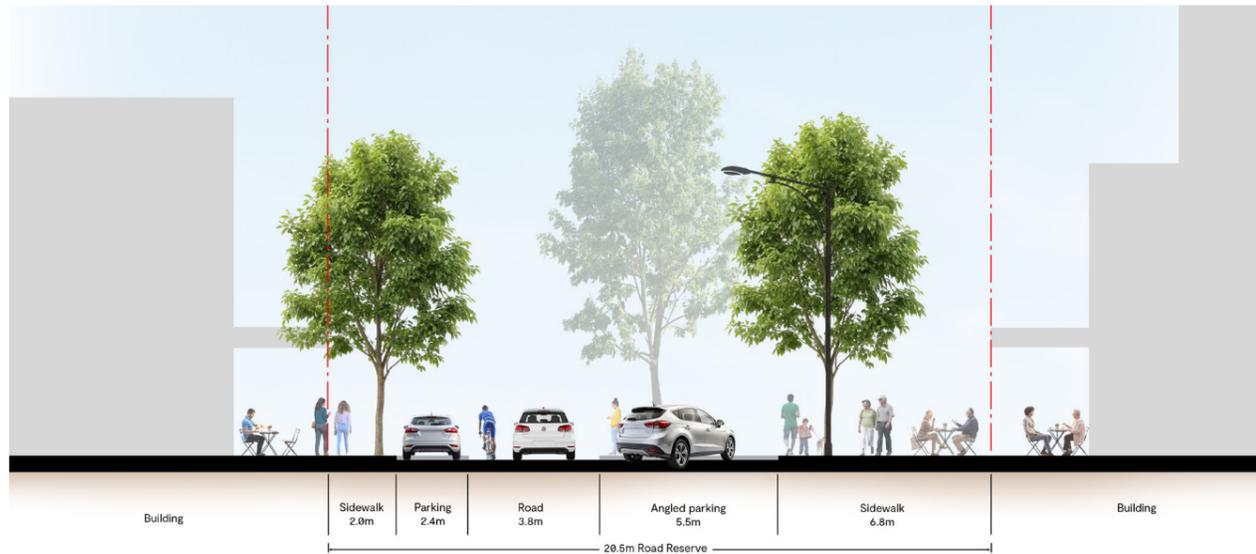


Figure 20 Activity node (section)

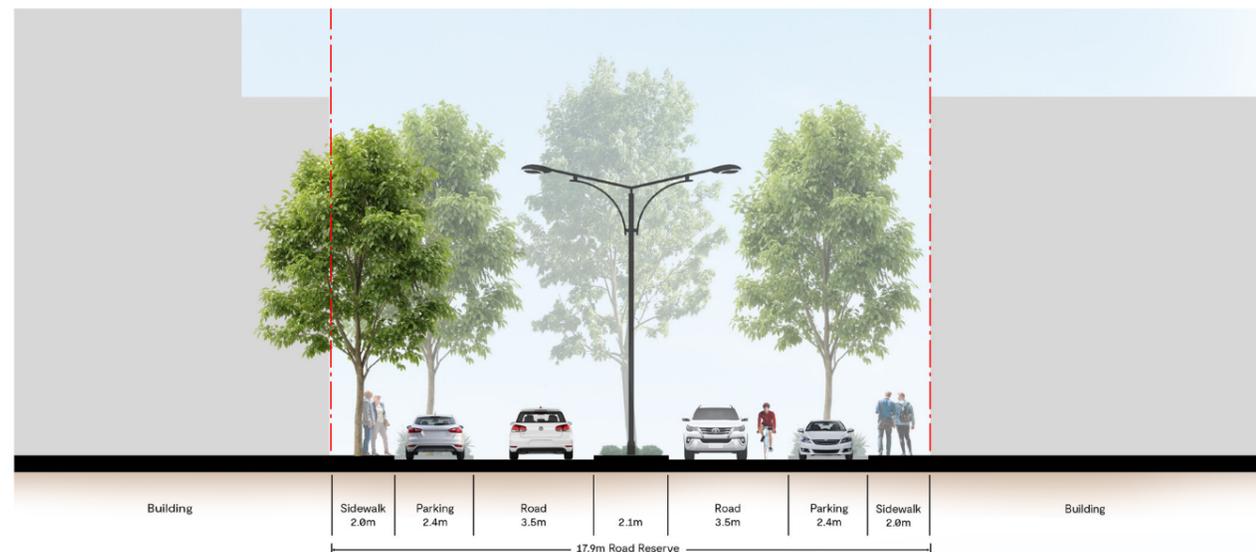


Figure 21 Village node (section)



Figure 22 Mixed street node (section)

Comfort, accessibility and safety in the public realm (C3.1.3, C3.4.1, C3.4.2)

To enhance user comfort in public spaces, one of the primary design considerations in the precinct is to improve shade and ameliorate high temperatures exacerbated by the urban heat island effect. The following initiatives are proposed along key pedestrian and cycle pathways:

- Improved tree canopy in public and private spaces, particularly along Old Perth Road and West Road.
- Built form responses that provide shade through structures such as awnings.
- Building scale/massing that provides space of existing trees, and balances shade and light access.

The Town's Public Open Space Strategy and Disability, Access and Inclusion Plan both acknowledge the need to improve the level of disability access in public open space and public realm areas. Improving existing public open space will also require considerations for how to better incorporate CPTED principles, where developments backs onto open spaces. Planning for opportunity sites has specifically considered how the relationship of built-form to public space can improve surveillance, use and sense of safety. Public realm interventions that support use of existing spaces, such as adjacent to the Council Site Opportunity site will improve accessibility, safety and enjoyment for all user types.

Design of public realm and celebration of culture (C3.2.1, C3.2.2)

Old Perth Road has a consistent design palette from Guildford Road through to West Road. Red hues, natural tones and native vegetation provide a consistent streetscape experience.

The Precinct's public realm contains significant public artworks, heritage features and Aboriginal significance. This precinct structure plan highlights the importance of a cohesive and deliberate strategy for public space design, incorporating materials and plant selections that enhance the precinct's cultural identity and sense of place.

Incorporation of environmental features (C3.3.1, C3.3.2, C3.3.3, 3.3.4)

Planning for the public realm integrates existing environmental features through respect for changes to topography, retention of existing mature trees (and provision of new deep-soil areas) and integration of existing cultural heritage fabric.

This precinct structure plan includes specific provisions to the integration of cultural heritage and character elements within the Success Hill Frame, as well as built-form provisions that contribute to the conservation of heritage listed places adjacent to Old Perth Road.

Opportunities for urban greening (C3.3.5)

Tree canopy within the Precinct varies significantly, with some areas (particularly along Old Perth Road and Guildford Road) severely lacking in mature trees. The overwhelming majority of this canopy is located on public reserves (BIC Reserve, Bassendean Oval, street reserves) and lower density properties in Success Hill.

This precinct structure plan provides for substantial urban greening opportunities along Old Perth Road, along key links and through the protection of existing mature trees along West Road and within key redevelopment areas.

Integration of utilities and services (C3.5.4)

Where major public realm enhancements are proposed, the Town will engage with service providers to ensure utility upgrades align with planned improvements. The Local Infrastructure Servicing Report (refer Appendix E – Local Infrastructure Servicing Strategy) identifies the extent of infrastructure upgrades expected during the life of this Precinct Structure Plan.

3.4 Movement

A well-integrated movement network should be developed that responds to the identified movement and place functions of the Precinct and that provides for a range of transport modes including walking, cycling, public transport, on-demand services, cars and delivery vehicles.

Opportunities and constraints

The Bassendean Precinct sits along key road, rail and active transport networks, which simultaneously improve accessibility while challenging the place and amenity value of portions of the Precinct. The Perth-Midland railway and Guildford Road divide the Precinct in two and strongly discourage pedestrian and cycling movement. Quality of access to Bassendean station is relatively poor, given its fantastic proximity to business and activity on Old Perth Road.

Access to Success Hill Station is significantly better, though it's catchment for passengers is significantly smaller than Bassendean station and limited predominately to residential commuters.

Key priorities



A focus of pedestrian and cycle movements Supporting long-term mode-shift towards walking and cycling as a means of reducing pressure on the road network and making the Precinct inviting to all.



Providing safe, connected travel for all people Ensuring good design of clear, direct, safe and enjoyable movement routes. Improving routes for walking and cycling, connections to key community facilities and schools.



Improving access to existing public transport infrastructure While the Precinct is well served by existing public transport services, access to these services can be challenging. Access can be improved by improving pedestrian crossings, delivering new station facilities, and providing direct, interesting, and safe walking and cycling routes.



Improving connection across key barriers at Guildford Road and the Perth/Midland railway A focus on reconnecting people north of Guildford Road with the Town Centre through improved quality of existing connections.

Future access needs (C4.1.1)

A Transport Impact Assessment (refer Appendix D) has analysed the capacity of the movement network to support future growth. The assessment considered transport modes including vehicles, pedestrians, cyclists and public transport. Key findings include:

- The Precinct has excellent access to public transport, with rail services at Bassendean and Success Hill, proximity to major transport interchanges at Midland, Morley and Bayswater, and bus services on Guildford Road. Despite this access, daily patronage is relatively low, with boardings at Success Hill station particularly low.
- There is need for long-term improvements to the road network including design intervention (possible grade separation) at Guildford Road and Collier Road, improved pedestrian accessibility at Guildford Road and Old Perth Road and consideration of intersection treatments at Thompson Road and Guildford Road.

This Precinct Structure Plan supports increased land use and dwelling density to support use of existing transport infrastructure. Interventions in the public realm, particularly at Old Perth Road, West Road and Lord Street, and improved quality of pedestrian and cycle routes, and end-of-trip facilities, aims to reduce car dependency.

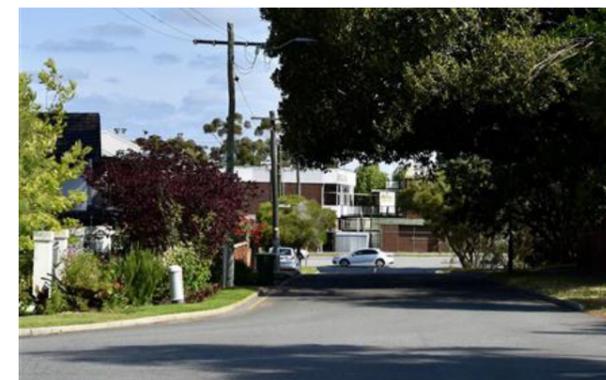
This Precinct Structure Plan has a clear objective to reduce private vehicle use in the future, especially for localised trips. With increased densities proposed, it is important to understand whether the existing traffic network can accommodate additional demand on the road network. The Transport Impact Assessment identifies that, developed to its fullest extent, an increase of 1,910 trips taken during the AM peak and 3,988 trips during the PM peak. This section identifies a series of measures, integrated into the Precinct Structure Plan, aimed at decreasing overall demand for private vehicle use, encouraging more comfortable and safe movement environments and enabling downward pressure on the road network.



Weather protection and shading along key pedestrian routes



Improvements to station access and arrival spaces to promote use



Improved infrastructure for access between the Town Centre and Success Hill



Prioritised pedestrian movement at junctions to promote lower speeds

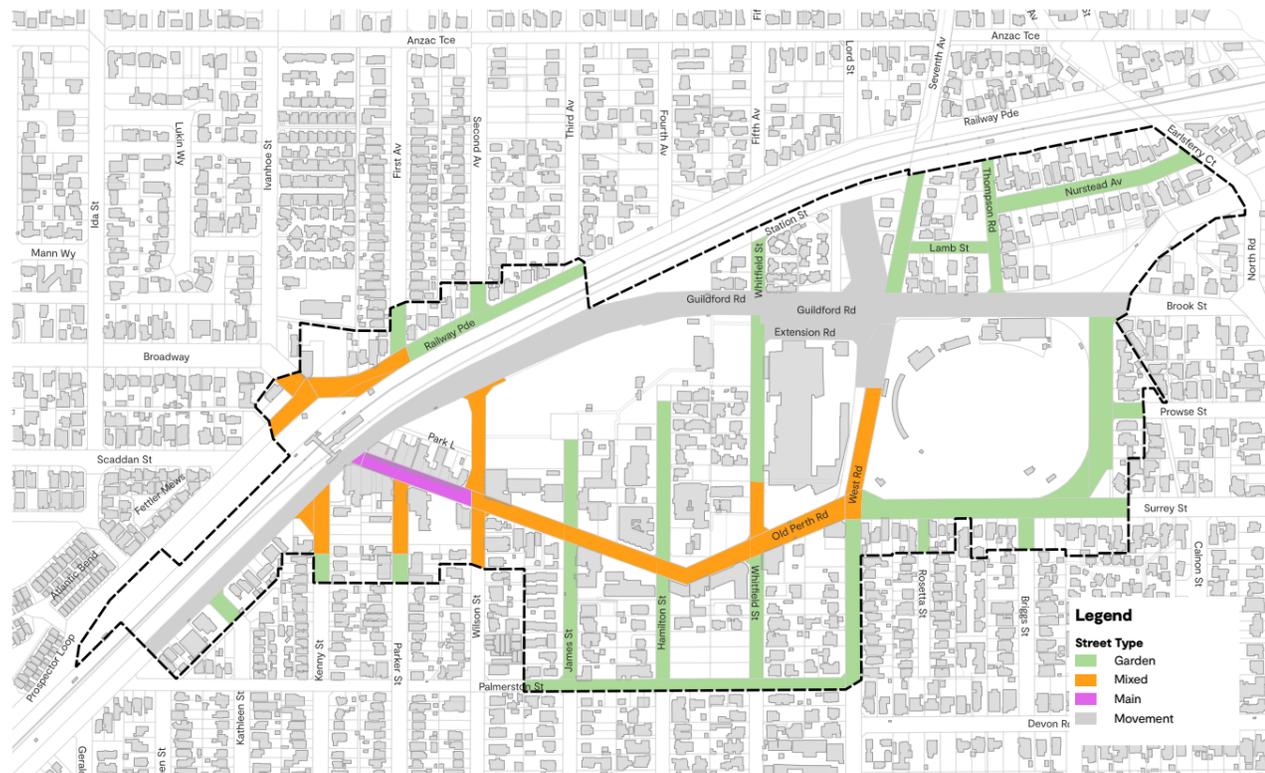


Figure 23 Street typologies

Movement and place outcomes (C4.1.2)

Buildings, public spaces and streets work together to create varying urban places. At present, an inconsistent mix of land use, streetscape design and movement priority result in competing outcomes and a lack of overall place quality. The Public Realm Plan of this Precinct Structure Plan, together with interventions in land use patterns, land use intensity and built form design aim to improve place quality outcomes. This is achieved primarily through:

- Consolidation of ‘main street’ activities to the western extent of Old Perth Road and at the southern and east frontages of the Bassendean Hawaiians shopping centre.
- Land use interventions to protect solar access to Old Perth Road, preserve existing street trees and landscaping and promote pedestrian comfort.
- Street design that priorities pedestrian movement along Old Perth Road and West Road.
- Improved pedestrian movement priority through sections of the Precinct, with incentivised improved connection between Bassendean Oval and BIC Reserve.

Convenient, safe and comfortable travel (C4.1.3, C4.1.4 C4.3.1, C4.3.2)

Guildford Road and the Perth–Midland railway are significant barriers to travel within the Precinct. Further, the design of the movement network in the Precinct priorities vehicle movements through:

- Wide crossovers along West Road, where vehicles access the Bassendean Hawaiians shopping centre
- Use of roundabouts and wide/large intersection radii (including at key intersections along Old Perth Road) that promote higher vehicle speeds.
- Absence of footpaths, sufficient shading/weather protection, particularly on streets running north–south in the Precinct.

Development in the Precinct will achieve a built form that is more outward facing, with increased points of arrival for pedestrians and cyclists. This would attract increased visitation by active transport modes.

To facilitate an increase in access by active modes, several infrastructure improvements are proposed, as discussed in the following sections. Proposed improvements to the walking network within and surrounding the Precinct are as follows:

- Significant improvements to the public realm could be achieved through new development that results in:
 - Increased connection between Old Perth Road and BIC Reserve, and West Road and Bassendean Oval and Surrey Street via a series of pedestrian green links.
 - Increased tree canopy and weather protection along Old Perth Road.
 - Increased connection between Old Perth Road and BIC Reserve, and West Road and Bassendean Oval.
 - Improved connection between Whitfield Street and West Road, and Success Hill station.
 - Connection between Whitfield Street (north of Guildford Road) and the existing railway underpass at Wilson Street/Second Avenue. Previous conceptual designs for this underpass considered extension under Guildford Road to BIC Reserve.
 - Mid-block links between Bassendean Oval and BIC Reserve.
- Incentives to development that provide for facilities that support greater use of walking and cycling as a mode of transport.

Public transport integration (C4.3.3)

Public transport to the Precinct is currently of a high quality and typical frequency, based upon Bassendean’s position in the metropolitan region as a comparatively smaller employment and activity district. Existing services are comparable to other locations where a high proportion of the resident workforce works in key centres of Perth, East Perth and Midland. These places are well connected to Bassendean via the Perth–Midland railway.

Some potential additional improvements to public transport provision that could be considered for implementation at the Precinct include:

- Improve pedestrian safety and legibility when accessing Bassendean station by expanding the existing arrival space and improving hard-scape treatment connection to Old Perth Road.
- Improve access to Success Hill by constructing access via Whitfield Street and providing a station arrival forecourt and facilities at Thompson Street and Lamb Street (see further discussion).
- Increased frequency of existing services, in both peak and off-peak periods.
- Locate bus stops away from loading bays and close to a safe crossing point (e.g. near signals) to discourage mid-block (unprotected) crossings.
- Improvement to quality in bus stops, particularly where there is opportunity to integrate shade, shelter and safety measures adjacent to new development.
- Removal of bus-embayments on Old Perth Road to provide more generous footpaths, landscaping and greater bus movement priority.
- Include public transport way-finding throughout the Precinct so it is promoted as a viable means of accessing the centre.

Car dependency minimisation and mode-share targets (C4.2.1, C4.2.2)

Despite excellent access to public transport facilities, the rate of journey-to-work trips made by public transport within the Precinct is 8.8%, compared to those that travel to work by private vehicles (77.8%). This highlights the clear preference of those currently living in Bassendean to access employment via private vehicles.

As identified in the Transport Impact Assessment (refer Appendix D), there are genuine challenges facing the road network into the future; even in instances where growth within the Precinct is low. Regional through-movement along Guildford Road will continue to present movement challenges at key intersections at West Road, Old Perth Road and Palmerston Street.

Facilitating greater local movement by means other than a private vehicle is a key aspiration of this Precinct Structure Plan. This will be primarily achieved through physical improvements to the public realm, but also fostered through increased local employment. Improving station access to promote local use will further support the viability of public transport services and reduce regional traffic flows.

Service vehicles and freight network (C4.3.5, C4.3.6)

Increased commercial and residential land use density will drive greater need for service vehicle access. Roads in the Precinct do not form part of the controlled road access network, or local freight routes. As depicted in section 3.2 - Urban structure, planning for the Bassendean Hawaiians shopping centre should ensure freight access is not located on southern portions of West Road, or from Old Perth Road.



Achieving mode-shift in the long-term will contribute to a functioning road network



Comfort and safety to stations is a key challenge identified by stakeholders

Mode share targets *Method of travel to work only



Private vehicles 65% (-13%)



Walking/cycling 5% (+4%)



Public transport 15% (+6%)



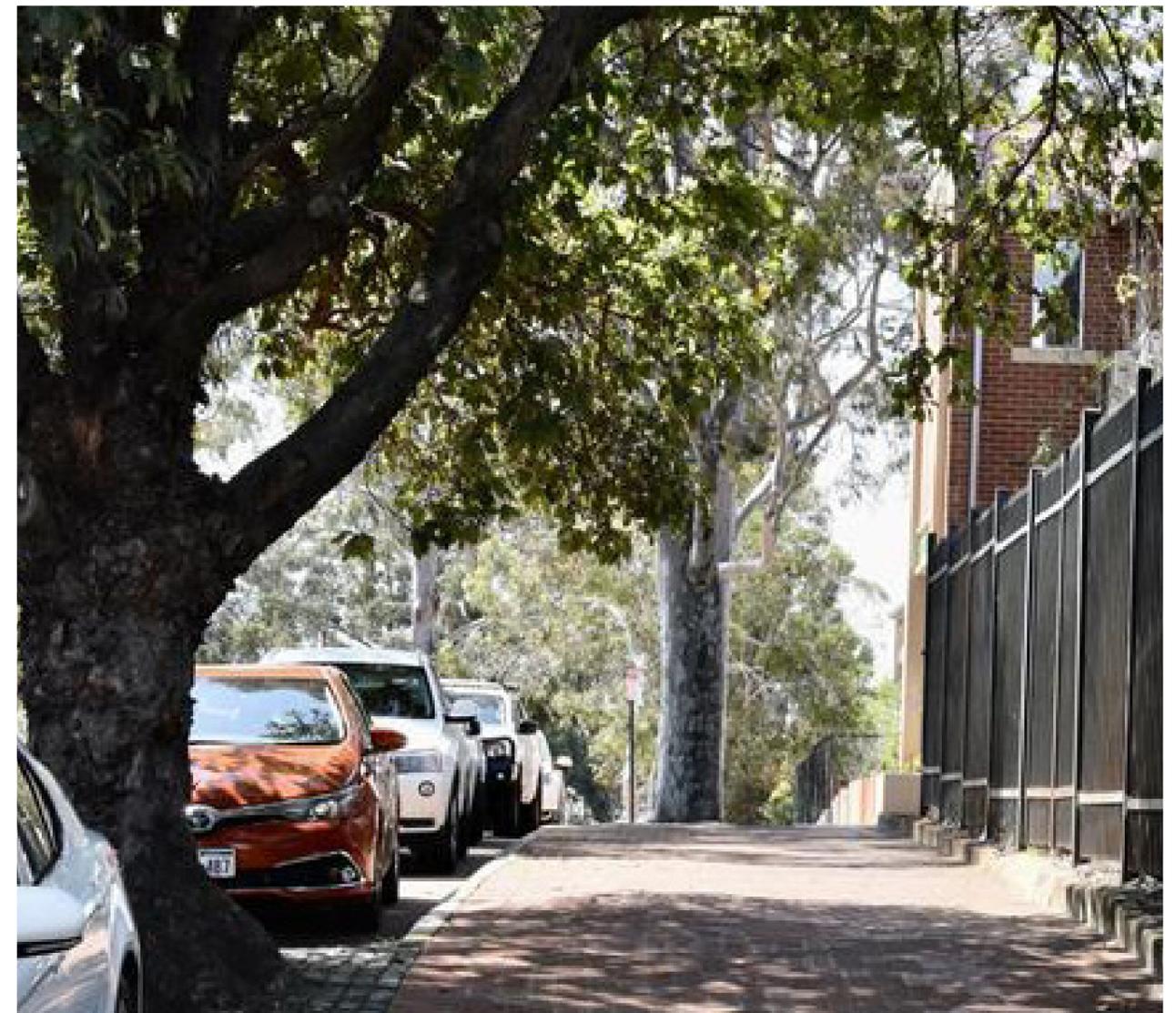
Work from home 15% (+3%)

Parking design and management (C4.4.1, C4.4.2)

An assessment of parking bay numbers using aerial imagery has identified approximately 820 off-street parking bays provided within the Precinct with most off-street parking contained at three key locations: Bassendean Hawaiian shopping centre, Bassendean railway station car park and the Bassendean Hotel. A further 506 bays are available at on-street locations throughout the Town Centre. Occupancy in the Town's publicly managed parking areas is generally low; near or less than 50% on average. This presents an opportunity to re-evaluate the need for parking and re-purpose streets for other use and transport modes.

The Town's LPP8 – Car Parking and End-of-Trip Facilities describes the requirements for parking provision, parking design and the provision of end-of-trip facilities. The policy specifies criteria predominately based on the land use proposed, and on a 'predict and provide' basis. The Transport Impact Assessment recognises that activity centre parking arrangements can be dynamic; involving significant rates of reciprocal parking between uses and at different periods of the day. This Precinct Structure Plan includes several measures aimed at ensuring the efficient provision of parking, including:

- Incentives for the provision of privately owned, publicly accessible; provided as an optional community benefit on Opportunity Sites.
- Provisions that specify a reduced rate of parking for active land use, where customer turnover is expected to be high and there is potential for shared and reciprocal parking.
- Temporary suspension of cash-in-lieu provisions to incentivise short-term redevelopment.



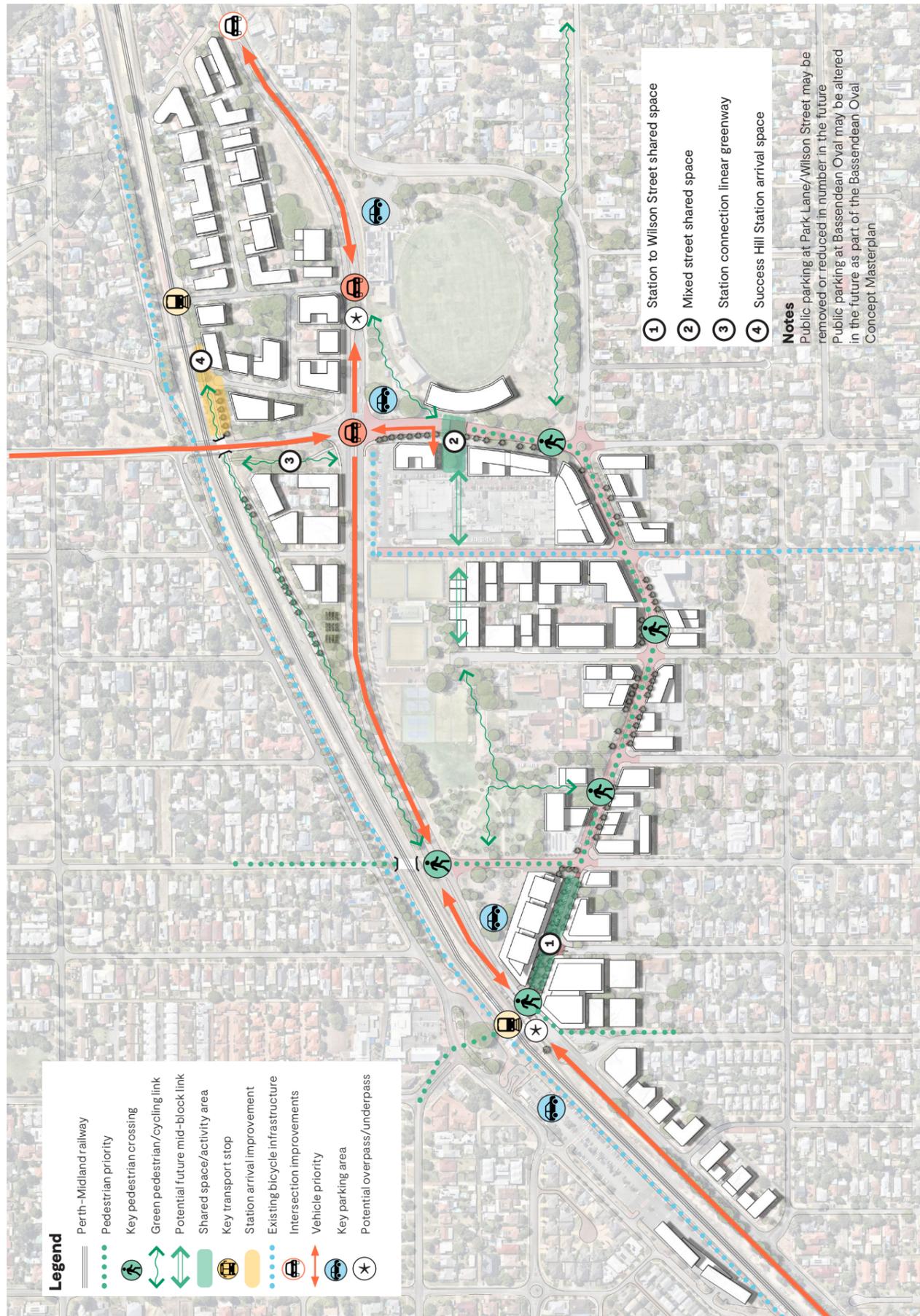


Figure 24 Movement strategy map

3.5 Land use

Land use planning should reflect the role of the Precinct in its wider context. Land use type, proportion, mix and location should respond to community needs; current and intended future activities and functions, alongside broader trends.

Opportunities and constraints

The Precinct currently supports a mix of commercial, retail, and residential land uses, reinforcing its role as one of Perth's most unique activity centres. Retail and hospitality are concentrated in nodes along Old Perth Road, between Guildford Road and Wilson Street, James Street and West Road, and at the Bassendean Hawaiians shopping centre.

Industrial, service and tourism land uses, together with creative infrastructure is relatively limited. The Bassendean Precinct is positioned as a place that serves its local community, with land use to support district wide attractions or employment being limited. Bassendean Oval serves as an outlier to this, with recent successful redevelopment of the Bassendean Hotel another marker of the area's potential. There is an opportunity in the long term for the Precinct to grow into a significant employment district, given its excellent public transport infrastructure and central location in the eastern region. Leveraging health and education demand from the Midland district will be key to growing local employment beyond retail and hospitality industries.

Key priorities

Community engagement and the Town's broader strategic framework highlight the following stakeholder priorities for the precinct.



Lively businesses, hotels, cafes and restaurants Create conditions where businesses can be readily established, thrive and generate interest beyond the local area.



Greater variety of shops, retail and supermarkets Ensure a diversity of retail spaces is available to support a growing local population. This includes supporting improved quality of retail at the western end of Old Perth Road.



Greater mix of housing type and choice Provide opportunities for a variety of building forms, with housing types and sizes that reflect Bassendean's diverse community.

Proposed zones and land use diversity (C5.1.1)

Zoning within the precinct is already well defined through the Town's Local Planning Scheme 11, with land zoned District Centre, Residential or Mixed Use. Portions of the precinct are reserved for regional roads, railways, public open space and infrastructure. This Precinct Structure Plan does not result in any immediate modifications to the zoning of land.

The majority of new development and land use within the Precinct is expected to be subject to the R-Codes (Volume 2). Consistent with design element 2.8 of the R-Codes (Volume 2), this Precinct Structure Plan includes a regime of development incentives for community benefits. The system of benefits reflect local priorities to see improvement to the public realm, to the diversity of land use and access to public facilities. This plan identifies how specific development locations (Opportunity sites) could be redeveloped and incentivised to contribute to these benefits, through additional building height up to prescribed limits.

In part, the regime of community benefits contributions are also to encourage greater residential dwelling growth in the Precinct, in order to ensure dwelling growth estimates established in the Town's Local Planning Strategy are achieved.

Distribution of residential density (C5.3.2)

Activity centres and station precincts are emphasised as priority areas in Perth and Peel @3.5 million as areas to promote growth and density in order to achieve a more consolidated urban form. There are approximately 557 dwellings within the precinct. This Precinct Structure Plan is estimated to provide for dwelling growth or at least 1,368 additional dwellings by 2050; consistent with the Town's Local Planning Strategy.

Dwelling growth and types within land zoned District Centre is expected to be in the form of mixed-use, mid-rise developments, with ground floor dwellings occurring on streets other than Old Perth Road. In the Success Hill frame, growth is expected to be almost entirely residential, with a mix of mid-rise apartments, town houses and villas to provide dwelling growth.

The Precinct Structure Plan reflects existing coding under the Town's Local Planning Scheme 11, with land zoned Residential coded at least R60 (and R100 in the Success Hill frame), while land zoned District Centre is coded at least R80, as well as a variety of activity centre codes. The Primary Controls of the R-Codes are

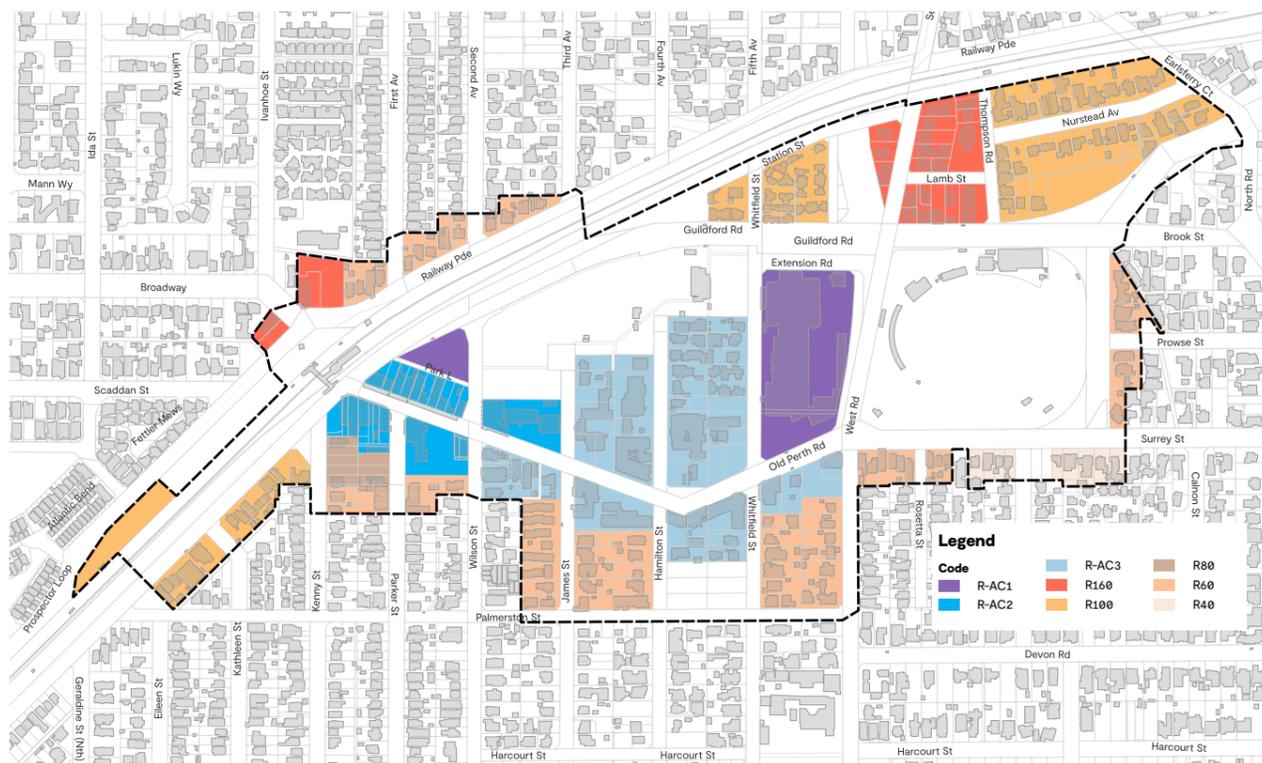


Figure 25 Density codes

modified by provisions of this Precinct Structure Plan in order to achieve desired future character and built-form.

| Code | Additional dwellings (10 year) | Total additional dwellings (ultimate) |
|---------------------------|--------------------------------|---------------------------------------|
| Medium density (R40, R60) | 84 | 141 |
| Higher density (R80+) | 712 | 1,295 |

Provision of floorspace, use mix and addressing gaps (C5.1.2, C5.3.1)

This Precinct Structure Plan is supported by a floorspace gap analysis as part of a broader Needs Assessment and Economic, Retail and Employment Strategy (refer Appendix F). Key land use gaps are identified in section 2.3 – Community context.

Office is the largest commercial land use gap identified for the Precinct. Increasing supply of office floorspace is a genuine opportunity for the Precinct, leveraging its location close to major transport infrastructure and relative position to major institutions and logistics hubs. In addition to office uses, there is clear potential for population driven services, such as medical, dental and real estate services.

Floorspace need for non-shop activities is estimated to be in the order of an additional 16,000 sqm, across a variety of land use by 2050. This Precinct Structure Plan provides for considerable increase in floorspace across a variety of development sites, located along Old Perth Road and adjacent streets. New development on key sites, such as the Bassendean shopping centre are capable of providing for increased non-shop floorspace, should demand materialise.

Local employment opportunities (C5.3.4)

Local employment opportunities will be generated through growing population and increased economic activity. The Needs Assessment and Economic, Retail and Employment Strategy (refer Appendix F) identifies strong need, over the long term, for a range of shop retail and non-shop land uses. This Precinct Structure Plan ensure sufficient zoned land for these uses and encourages redevelopment of key sites through aligned improvements to the public realm and transport infrastructure.



Growth in professional services and office land use is expected over time



Smaller scale shop, retail and hospitality uses provide vibrancy and activation

Land use staging and distribution (C5.1.3, C5.2.1, C5.2.2, C5.2.3)

This Precinct Structure Plan adopts an evidence-based approach to floorspace growth, with growth expected in retail and entertainment uses within the Precinct. Demand for these uses drives a need to ensure growth is consolidated at key nodes aligned to existing active sections of Old Perth Road, and emerging frontages to West Road.

To ensure land use growth reflects desired future character, built-form and public realm outcomes, Part 1 of this Precinct Structure Plan:

- Control ground floor land use, directing active uses to sections of Old Perth Road and West Road where activity is proposed to be consolidated
- Describes public realm design outcomes for different sections of Old Perth Road, West Road and Wilson Street that will encourage interaction between the public and private realm.

Consolidation of activities has the additional benefit of concentrating opportunities for night-time activity in designated locations. Night-time precincts that are compact encourage successful land use, promote safety and become easier to support with improved public realm and events.

Given the significant fragmentation of land within the Precinct, land use staging has been determined in reference to a sites potential for redevelopment. This is informed by assessment of the lands existing scale of development, approximate building height, land tenure and size. Land identified as 'Very high' or 'High' in Figure 26 are considered most likely to redevelop within the initial 10 year operation of this plan. Despite high potential for redevelopment, growth estimates have been rationalised to reflect potential constraints such as market conditions, feasibility and landowner intentions.



Figure 26 Development staging

3.6 Built form

Built form should be developed to support an environment that is place appropriate and functional. Built form should provide choice and affordability in housing; and support the critical mass of residents, workers and visitors.

Opportunities and constraints

Bassendean has a built character defined by character shopfronts, pubs and workers cottages and modern multi storey development that has evolved over time. Its scale is predominantly mid-rise, with landmark buildings that break this convention adding diversity and variety. While design quality varies, recent development have largely succeeded in reflecting the scale and character of existing development.

Away from Old Perth Road and generally east of West Road, buildings are more suburban in form. Streetscapes consist of predominantly single and two storey dwellings – developed over a series of decades. Design language varies significantly, and so does quality and extent of landscaping.

In the Success Hill Frame, a consistent pattern of subdivision and design aesthetic has resulted in a reasonably strong sense of character.

Key priorities

Community engagement and the Town's broader strategic framework highlight the following stakeholder priorities for the precinct.



Incentivising renewal of key sites Concentrate growth in locations with the biggest capacity to accommodate growth and contribute to a growing activity centre. Incentivise growth through improvements to the public realm.



Mixed use development outcomes Support vibrant growth in the centre that mixes new housing with opportunities for employment, access to services, entertainment and enhanced civic and cultural functions.



Development that contributes to streetscape upgrades In return for increased development potential, key sites contribute to shared progress in Bassendean by making community benefit contributions that improve the public realm.

Current situation and plan responses

The current planning controls for the Precinct have not delivered significant built-form change in recent years. Low rates of dwelling growth, inconsistent built form and architectural quality, and limited local employment opportunities drives the need for change. Key issues with the current built form approach for the Bassendean precinct include:

- No specific sustainability standards or outcomes.
- Lack of coordination of building scale, leading to unclear expectations for landowners and community.
- Insufficient incentive for investment by key landowners.
- Limited coordination between the Town's vision for the precincts public realm and planned developments on private land.

Key provisions of this Precinct Structure Plan have been tailored to ensure flexible but robust response to these issues. Providing clear built-form outcomes, that are responsive to context, urban structure and tenure have been key considerations shaping this plan.

Providing a diversity of built form types across the precinct is highly desirable. It has the potential to bring visual variety and interest with the opportunity to frame streets with appropriately scaled buildings.

Driving the design of built-form are factors relating to land use, urban structure and public realm. Built-form provisions of this plan are shaped by the type of activity expected in each development, particularly at ground level, the function of the adjacent public space in providing for movement and space of recreation, and the need to preserve amenity of adjacent buildings and spaces. This is underpinned by an understanding that there is diverse built-form within the Precinct, driven by the relationship between built-form, land use and public realm, rather than distinct sub-precincts. Streetscape types are identified that relate to key controls of land use and built form, being:

- **Active streets:** Where built-form should be tailored to encouraging activity in the adjacent public realm. Frontages should be active and encourage diverse trading activities. Facades are fine-grain to reflect the diversity of activity in land use. The height of buildings at the street reflect a human-scale. Buildings provide well-defined edges to the public realm for interaction with the commercial ground level with a high proportion of clear glazing.
- **Mixed streets:** Activity is more diverse, with opportunities to include residential dwellings at ground level on semi-active frontages. The relationships of buildings to the adjacent public realm will be mixed, according to the needs of the accommodated land use. Along Old Perth Road, buildings near to the street boundary promote a sense of continuation of the active street area near to Bassendean railway station.
- **Garden streets:** Suburban streets where passive land use and limited activation is accommodated. In these locations, buildings are smaller scale and more domestic in design. Refined design details ensure new development in these streets can better complement existing smaller built-form as the precinct goes through transition.
- **Movement streets:** Where built-form relationship to the street is not a high priority and land use is expected to address other frontages of the land.



Passive frontages (Garden street) accommodate generous landscaping and building separation



Movement frontages prioritise transport outcomes over place quality

| Type | Existing built-form | Built-form response |
|-----------------------------|---|---|
| Active | <ul style="list-style-type: none"> — Activity to street boundary with limited/no landscaping between land use and public realm. Street walls (typically 2 storeys) create enclosure — Weather protection (awnings, verandas) supports activity — No vehicle entries from Old Perth Road — Pattern of shopfronts and entries reflects traditional small scale land use | <ul style="list-style-type: none"> — Nil setbacks to street at ground floor and street wall. Active land use required. — Street walls up to 2 storeys at the boundary to ensure light access, with additional storeys recessed. — Continuous weather protection to pedestrian environment — All parking access via secondary streets and screen from view. Minimised in visibility where located in upper levels — Fine-grain façade treatments, universally accessible and highly detailed. |
| Semi-passive | <ul style="list-style-type: none"> — Nil or limited ground floor setbacks, according to land use. Side and rear setback mixed. — Mix in building heights, with some buildings 3-5 storeys. — Facades generally wider and less detailed than in active street areas, owing to the less interaction with passing customers. | <ul style="list-style-type: none"> — Nil-2.0m setbacks where commercial use is proposed. More generous setbacks where ground floor residential use is included. — Buildings retain existing character of 3-5 storeys in height at Old Perth Road, with additional height set back. — Side and rear setbacks defined by need for light access and privacy. |
| Passive (on Garden streets) | <ul style="list-style-type: none"> — Limited building height (1-2 storeys) reflecting residential land use focus — Setbacks increasing away from Old Perth Road (between 2-6m) — Built-form more domestic with vehicle entries to the street — Limited to no interaction between buildings and the public realm, except landscaping and the occasional commercial business. | <ul style="list-style-type: none"> — Building heights to provide transition to surrounding areas. Not a focus for additional height. — Setbacks at least 2.0m providing transition between the public and private realm. — Street, side and rear setbacks more generously landscaped with a focus on landscape retention. — Vehicle entries and parking areas minimised, or domestic in scale. |

| Type | Existing built-form | Built-form response |
|-------------------------------|--|--|
| Passive (on Movement streets) | <ul style="list-style-type: none"> Almost no interaction between built-form and public realm. Built-form is designed for interaction with vehicles Facades lack detail, and are separated from pedestrian areas with parking and wide (but poorly) landscaped areas. High walls protect residential properties for vehicle noise. | <ul style="list-style-type: none"> Building height in mixed to respond to surrounding context. These areas are not a focus for growth owing to low amenity. Frontages to Movement streets to provide for access and services, discouraging active or residential uses at the ground floor. Protection of privacy and from noise impacts a focus for design. |

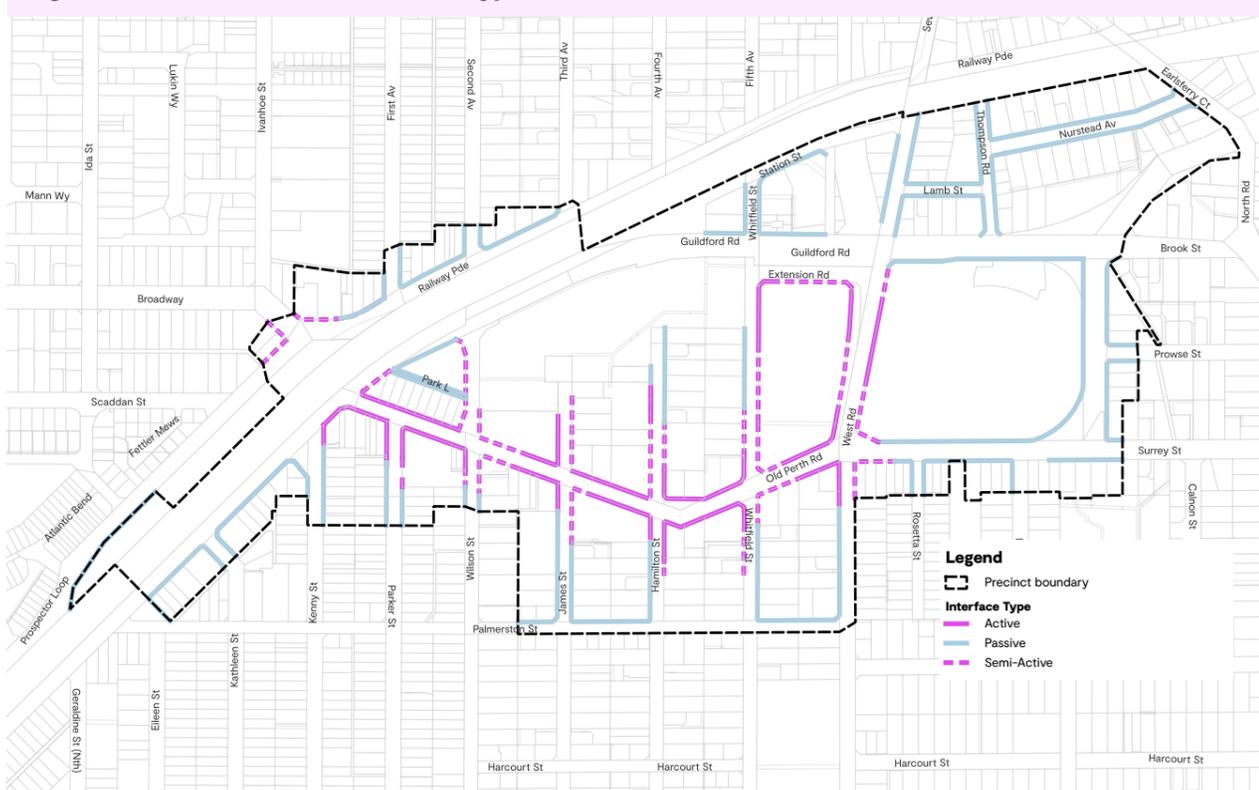


Active frontages are tailored to encourage diverse uses and enjoyment of the public realm



Mixed frontages provide for built-form that relates to the land use accommodated

Figure 27 Public domain interface types



Public domain interface

The interface between public and private spaces is critical to ensuring land use is successful and adds vibrancy, and that ground or lower floor passive uses (including residential uses) are afforded appropriate levels of privacy.

Glazing to commercial tenancies is a key method of achieving high-levels of interaction with the public realm, or in the case of passive uses a balance of privacy. Ground floor land use in active areas should address the street in a way that encourages interaction and achieves pedestrian comfort. Semi-active areas will balance public interaction with the specific needs of the land use accommodated. In passive areas, the greatest focus will be on privacy and incidental surveillance.

Figure 28 Active interface ground floor treatments

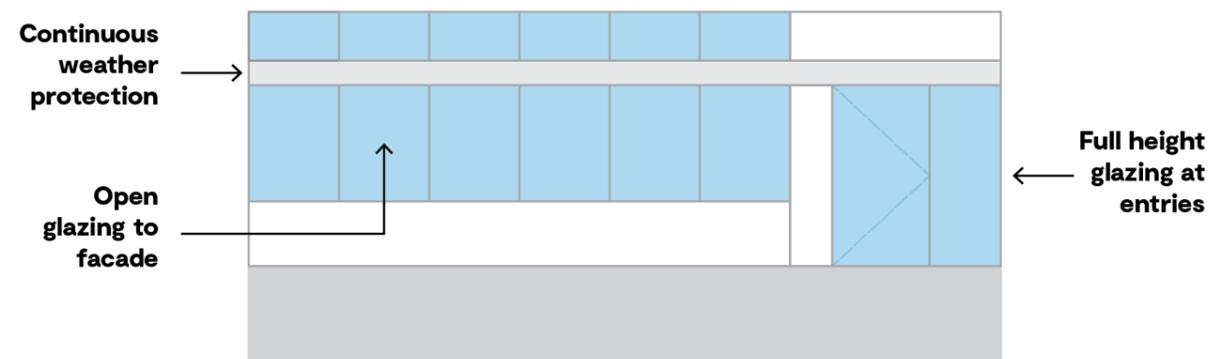
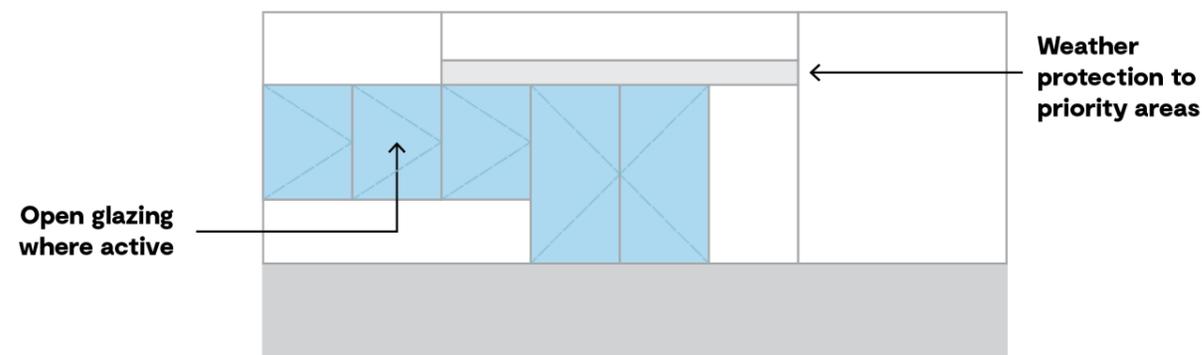


Figure 29 Mixed interface ground floor treatments



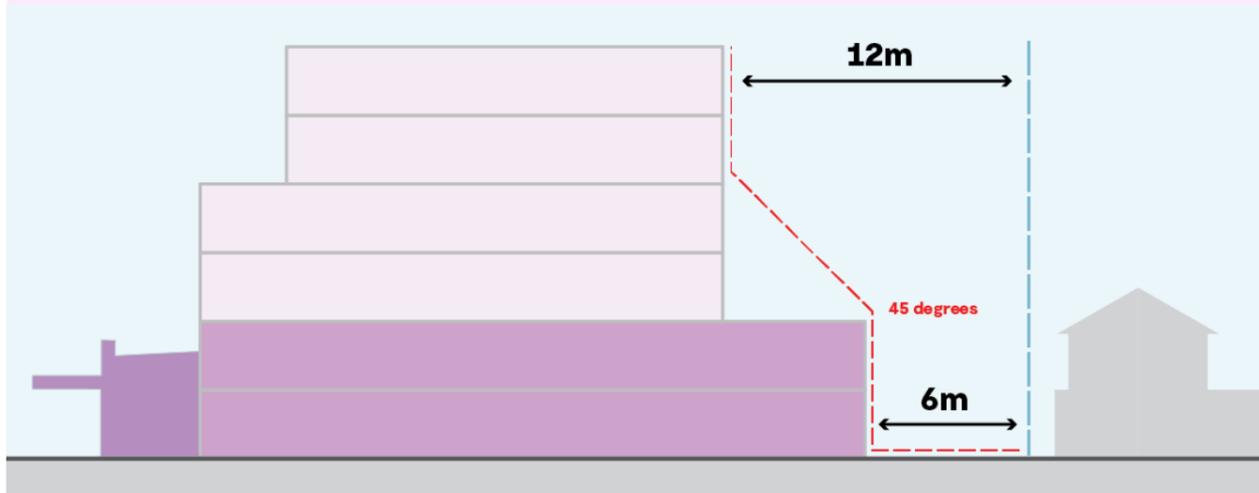
Built-form transition (C6.1.2)

To ensure a seamless integration with adjacent land uses, the transition zones from single residential land to the south must be carefully managed. Transition in building heights proposed by this plan ensure access to light and ventilation is maintained, as well as reduced building bulk and scale.

Addressing inconsistencies in street setbacks, particularly in certain residential streets and along Old Perth Road, will enhance the overall streetscape. Establishing uniform setback requirements will contribute to a cohesive urban environment.

Consistency and coherence within precincts and at transition points are vital for a unified urban fabric. This approach will integrate different land uses and architectural styles effectively. Finally, setting consistent front setback requirements above the street wall height is crucial for maintaining a uniform and visually appealing streetscape.

Figure 30 Built-form transition to sensitive boundaries



Building height, setbacks and plot ratio (C6.2.1, C6.2.2, C6.2.3, C6.2.4)

In areas where land has been consolidated, a mix of low and mid-rise buildings is appropriate. Smaller land parcels should have limited building heights. On parts of Old Perth Road, buildings of 4-6 storeys without upper floor setbacks are suitable. Greater height should be promoted on Opportunity Sites to add diversity and visual appeal. Developments must deliver significant community benefits and public amenities, proportional to their scale.

Additional building height is consolidated within major sites to minimise visual impact and overshadowing, with a suitable transition in scale to adjoining areas. Where buildings are adjacent to key public spaces, such as pocket parks and footpaths in active areas, development on upper floors should be recessed to provide light access. Visual relief should be provided at sensitive boundaries where height limits and development intensity changes.

Figure 32 Street setbacks



Figure 33 Built-form provisions ensure solar access to key public spaces, active streets and mixed streets

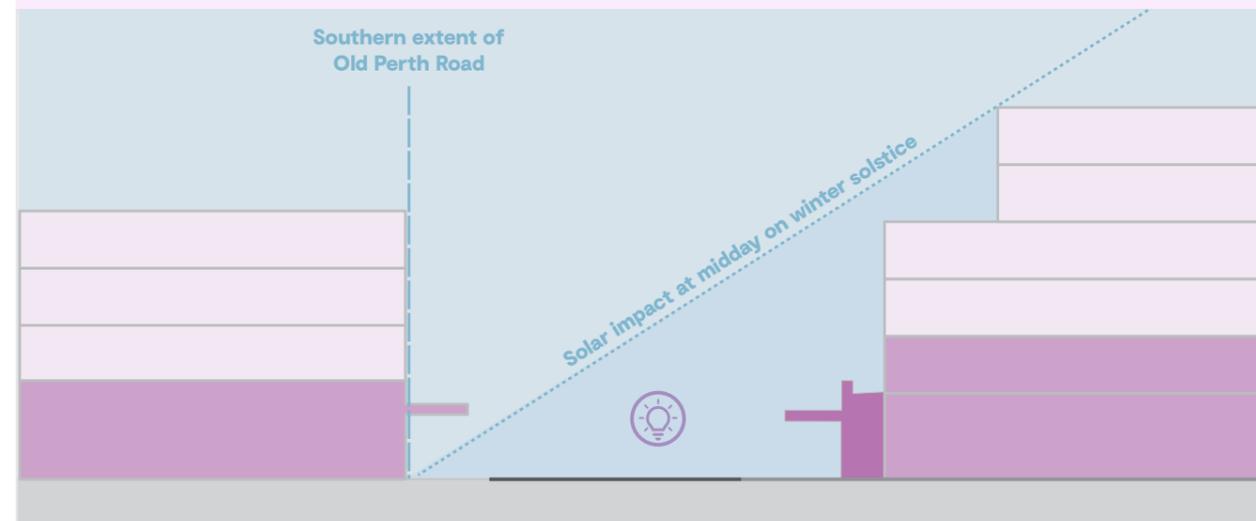
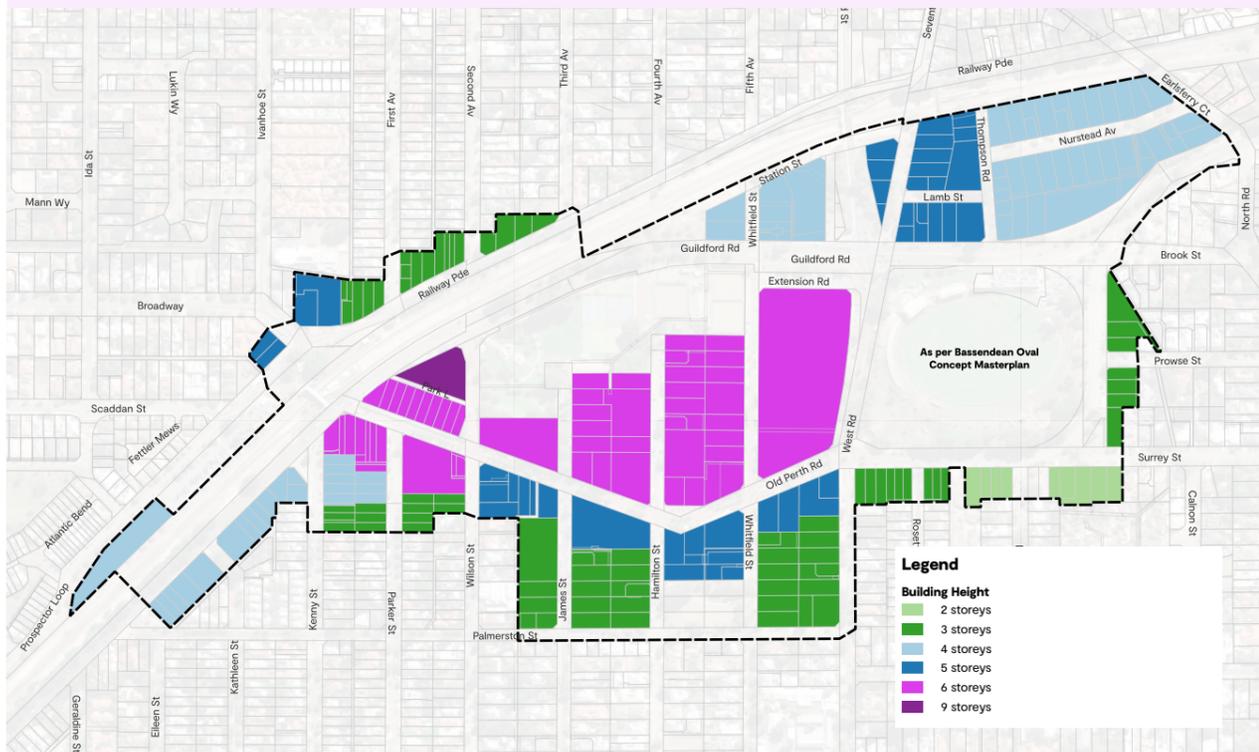


Figure 31 Future building heights (subject to potential additional height on Opportunity sites)



Optimised solar access and ventilation (C6.3.1, C6.3.2)

Old Perth Road benefits from good solar access due to the limited building scale and height. However, the orientation of the land can result in potential overshadowing of adjacent land and public spaces outside the precinct. It is essential to protect important open spaces and key sections of Old Perth Road from overshadowing to maintain their usability and appeal. Specific provisions are included in this plan that protect solar access in active street sections of Old Perth Road. Likewise, massing of building height to Opportunity sites protects light access to key public spaces at the Village node and achieves an appropriate building scale adjacent to open space areas at BIC Reserve and Bassendean Oval.

Diversity and adaptive re-use (C6.1.3, C6.1.4)

Much of the existing place qualities for Bassendean are driven by its built-form diversity. Buildings address the street in numerous ways – often according to building age and original land use. This creates interesting opportunities for adaptive re-use. Earlier civic buildings (fire stations, post offices, religious buildings) have potential to accommodate new land use. Earlier light industrial activity (vehicle services) have left buildings with large volumes capable of re-use. Where existing structures are re-used, the existing rhythm of development (typically established through historic lot patterns) should be reflected in the new built-form; through differentiation in materials and form, and through clear vertical fenestration.

Comfort and safety (C6.4.1, C6.4.2)

Comfort and safety is a key driver of place success within the Precinct given the fragmented levels of activity and interest along Old Perth Road and vehicle focused land use at West Road and in the Success Hill Frame. This Precinct Structure Plan includes provisions relating to ground floor public domain interface, weather protection and public realm improvement aimed at improving pedestrian comfort and promoting greater surveillance to public spaces.

Figure 34 Vertical separation reflecting traditional lot pattern



A lack of laneways and secondary streets for some sites presents challenges for servicing



Built-form that prioritises parking at street frontages diminishes comfort and safety

Development in the Success Hill Frame

It is critical that future development enriches the urban environment Bassendean and Success Hill through careful responses to existing context. The Success Hill Frame has a rich mix of dwelling forms, materials and landscape elements that provide a distinctly different urban environment to the remainder of the Precinct.

Success Hill Frame Heritage Assessment

A Heritage Assessment for the Success Hill Frame was undertaken by Hocking Heritage Studio (refer Appendix G - Success Hill Heritage Assessment). The area contains six places previously designated of heritage value and are included in the Town's Local Heritage Survey.

Two places are included on the Heritage List which provides greater statutory controls over the future development of these places. The heritage assessment recommended:

- Several places be elevated to the Town's Local Heritage Survey.
- Future development respond to existing design characteristics in terms of frontage, vegetation, siting, building height and form, building materials and design detail, front boundary treatments, and car parking structures.

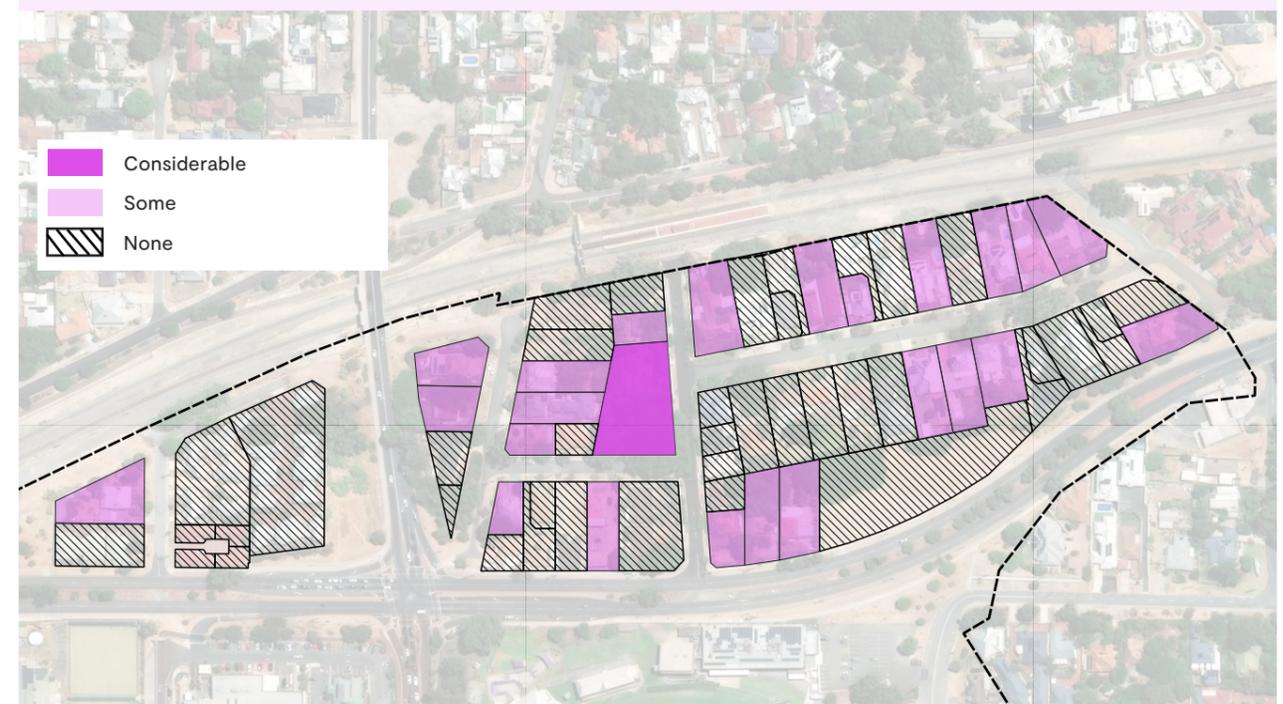
This Precinct Structure Plan designates the portion of the Success Hill Frame sub-precinct east of Lord Steet/ West Road as forming part of the Nurstead Heritage Area. Design criteria are applied in this area to ensure elements of cultural heritage, landscape character and built-form are reflected in new developments.

Assessment of existing character

The Heritage Assessment, together with the earlier Town of Bassendean Built Form and Character Study provide insights into the development of the area and definition as to what built and natural element are present in Success Hill.

This Precinct Structure Plan includes provisions relating to built form (building height, street setbacks), public realm (fencing, landscaping) and materials, details and design elements that aim to celebrate the best elements of the Success Hill Frame, recognising the area will go through significant change to its existing built form.

Figure 35 Places with cultural heritage values (Success Hill Frame)



Existing character qualities in the Success Hill Frame

While the area is expected to undergo significant densification as a result of the existing R100 coding, this Precinct Structure Plan includes specific design interventions to ensure existing character is respected, while the area undergoes change.

| Design element | Existing character |
|------------------------------|---|
| Urban structure and lot form | <ul style="list-style-type: none"> — The structure of land within the Success Hill Frame has been influenced by historical development patterns and the intrusion of infrastructure, such as the construction of the Lord Street overpass. — Lot pattern along Nurstead Avenue is relatively consistent, with regularly shaped lots, consistent street setbacks and frontage width (generally 14–20 metres). This structure extends to portions of Lamb Street and Thompson Road. Except for a small number of battle-axe subdivisions, dwellings have direct street access. — The structure of lots adjacent to Lord Street and Whitfield Street is mixed, with wider frontages and use of internal streets/driveways for access. |
| Public realm and landscaping | <ul style="list-style-type: none"> — Landscaping is inconsistent, with some streets generously landscaped, while sections of others rely heavily on private landscaping or significant street trees. Street setbacks areas are typically well landscaped and open to the street. — Footpaths are provided, in some cases on both sides of the street; inviting walking and discouraging additional crossovers. |
| Built form and scale | <ul style="list-style-type: none"> — All buildings within the Success Hill frame are single or two story's in height. — Spacing between buildings achieves a distinct suburban feel, and while street setbacks vary, all sites offer space generous space for landscaping. Front fencing is generally open. — Roof forms are pitched at generous angles to allow visible of steel roofing materials at street level. |
| Materials and colours | <ul style="list-style-type: none"> — Materials vary, but generally consist of red face brick, render and painted weatherboard. Corrugated steel roofing is consistently used, though some buildings use terracotta or concrete roof tiles. Colours are neutral with a variety of natural hues, set against use of red bricks and timber elements. |
| Building details | <ul style="list-style-type: none"> — Buildings have been developed over an extended period and are therefore mixed in character. This includes Federation, Inter-War, and Post-War dwellings, together with some contemporary dwellings. — Buildings adopt a hip and gable forms with consistent use of verandah and roof awning structures to add detail. These structures are light-weight in design, complementing heritage values in the area. |

Summary of built form design responses for the Success Hill Frame



Generous street setbacks are landscaped to provide transition



Building proportions reflect to the original lot pattern, limiting amalgamation



New development encourages sustainable movement



Materials and colours reflect existing character elements



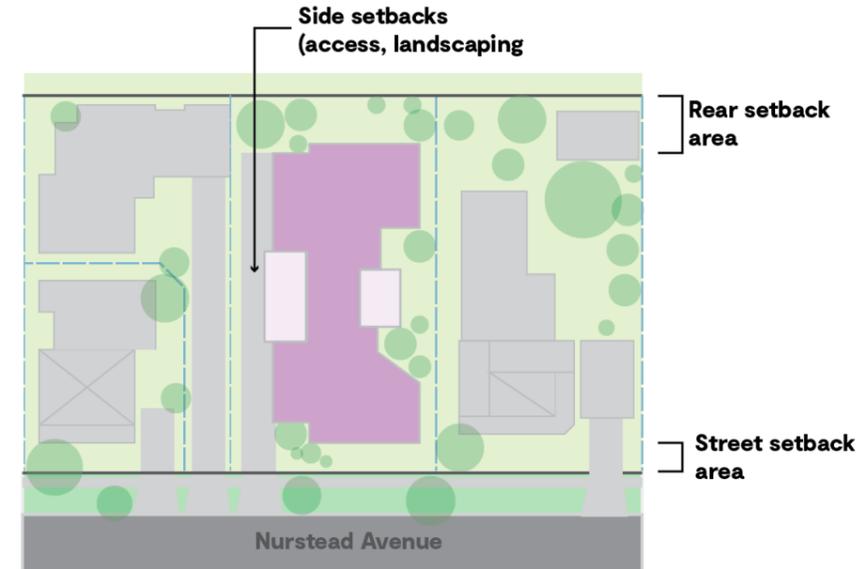
New development retains existing mature vegetation and retains space between buildings



Setbacks areas are free from parking, structures or high fencing

Design responses for new development within the Success Hill Frame

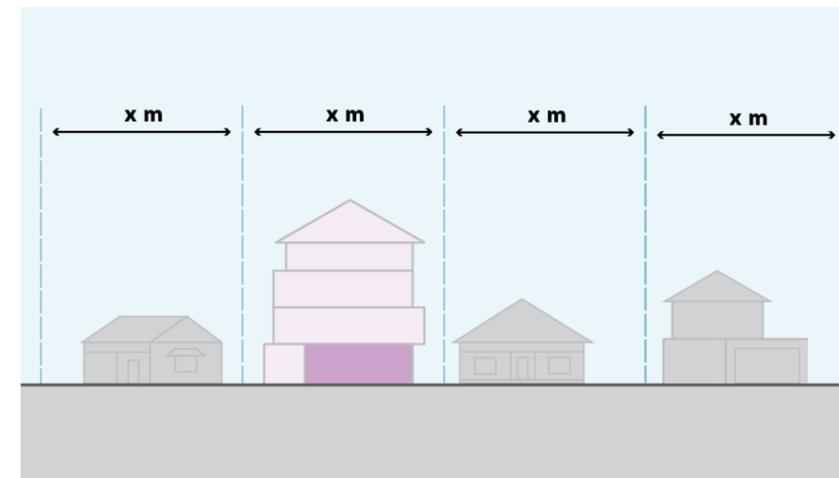
| Design element | Design response |
|-------------------------------|--|
| Street alignment and setbacks | <ul style="list-style-type: none"> Street setback areas are landscape and provide transition between semi-private and public areas. Street setback areas are free from vehicle parking, bin stores or private open space areas with the exception of balconies. |
| Landscaping | <ul style="list-style-type: none"> New development retains existing mature trees where appropriate. New development makes provisions for the provision of medium and large trees within the street setback area. New landscaping preferences native and water-wise species and integrates urban water management principles into designs. |
| Lot rhythm and proportions | <ul style="list-style-type: none"> Building proportions mimic the lot pattern of the adjacent properties by utilising strong vertical elements to define former lot boundaries where amalgamation occurs. Side and rear setbacks provide separation, but are minimised (except to provide for solar access and privacy). |
| Design elements | <ul style="list-style-type: none"> Verandah and awning elements are used over exposed doorways and windows. Vertical columns and verandah/awning structures are light-weight in design. Buildings adopted pitched roof forms. Where lot amalgamation occurs, use of multiple hips and gables to reflect the scale of existing roof-forms. |
| Materials and colours | <ul style="list-style-type: none"> Buildings utilise traditional brick materials, in appropriate colours, and painted timber elements. Colours reflect the existing use of neutral grey-sand hues, offset against bolder heritage colours. |



| Setback | Minimum setback |
|---------|--|
| Street | 4.0m* |
| Rear | 6.0m |
| Side | Ground - 1.5m Above ground - 3.0m** |

* Except verandahs, awnings, gable ends and other like structures
 ** Side setback areas for access and landscaping. Boundary walls not permitted.

Figure 36 Street, side and rear setback areas (Success Hill)



Development in the Success Hill frame is to reflect the existing spatial characteristics established by the existing lot pattern. Amalgamation should be avoided in order to preserve the domestic scale of development. Limited building depth, together with generous setbacks will achieve built-form that is responsive to context and respectful in transition.

Figure 37 Street elevation (Success Hill)



Design responses for the public realm within the Success Hill Frame

This Precinct Structure Plan includes various provisions for development on privately land. This development is likely to occur over a long time horizon, potentially spanning several decades. With this timespan comes a need to carefully integrate development with improvements to the public realm and support growth with community benefits. For the Success Hill Frame, public realm improvements will encourage new development to positively contribute to context and improve movement.

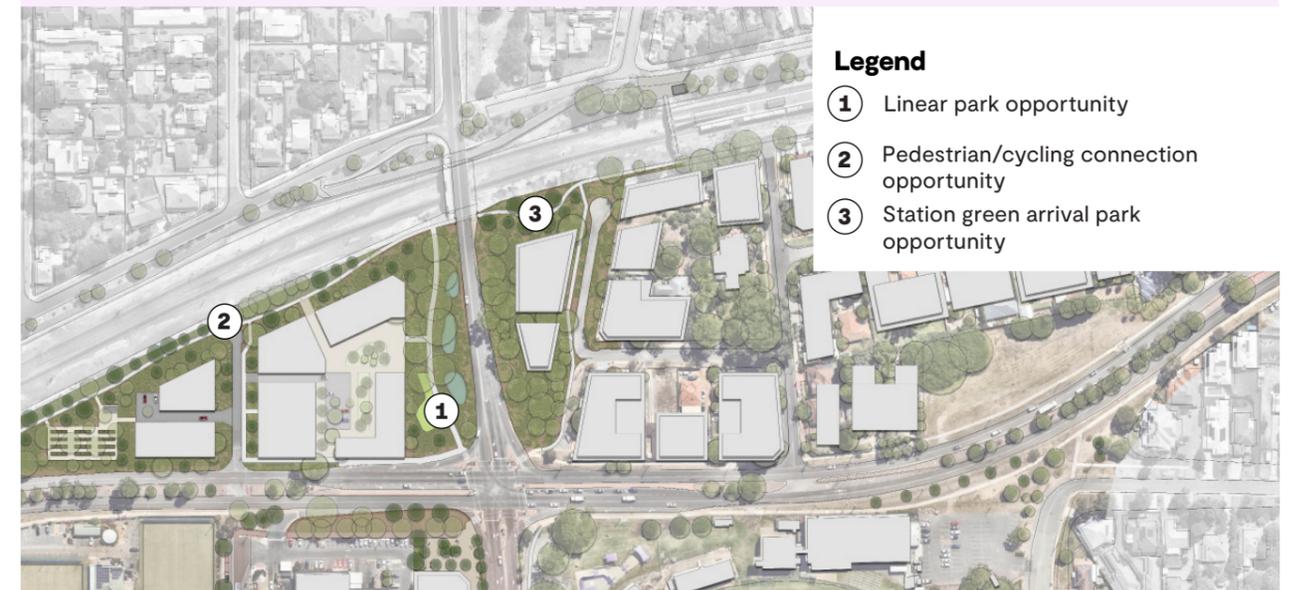
The quality of public realm in the Success Hill Frame is varied, with some areas lacking pedestrian accessibility (absence of connection, or formal pathways), while others are heavily vegetated by street and overhanging trees. The public realm fits the low density residential character that exists in the area today, with basic treatments to roads and verges, with no clear hierarchy of spaces or movement pathways.

Public realm design responses summary

| | | | | | |
|---|---|---|--|--|---|
|  | Hierarchy of movement pathways is clear |  | New public spaces are provided |  | Cultural heritage and character is celebrated |
|  | Existing landscape character is protected |  | Design and use of public realm is safe |  | Access is direct and universally accessible |

| Design objective | Design responses and actions |
|--|--|
| Ensure the public realm is designed to promote community health and well-being. | — Provide open space/pocket park, integrated with movements to/from Success Hill Station, including new footpaths, seating, lighting and shade along Thompson Road. |
| Enable local character and identity to be expressed in public realm design to enhance sense of place. | — Prepare a Public Art Plan for expenditure of contributions received through LPP15. Give specific consideration of the cultural heritage values and history of the Success Hill Frame. |
| Ensure that key environmental attributes are protected and enhanced within the public realm. | — Ensure existing mature street trees are retained and provided with deep-soil and are free from impermeable surfaces. |
| Ensure the public realm is designed to be inclusive, safe and accessible for different users and people of all ages and abilities. | — Provide footpaths to both sides of the street throughout the Success Hill Frame. — Advocate for at-grade pedestrian and cycling access to Success Hill station, support with improved cycling storage facilities. |
| Ensure public realm design is integrated with the built form, movement network and landscape of the precinct. | — Development minimises subdivision and crossovers. — Provide pedestrian/cycling connection between Success Hill Station and Whitfield Street. |

Figure 38 Public realm design responses (Success Hill Frame)



Legend

- ① Linear park opportunity
- ② Pedestrian/cycling connection opportunity
- ③ Station green arrival park opportunity



Landscaping is provided to key movement pathways and aligned with built-form



Design of public spaces, landscaping, streets and pathways is suburban in character



Movement to Success Hill Station is safe, inviting and universally accessible



Open space/pocket park are provided within the Success Hill Frame

Appendices

Appendix A

Stakeholder Engagement Summary

Appendix B

**Retail Needs Assessment & Economic
and Employment Strategy**

Appendix C

Environmental Assessment

Appendix D

Transport Impact Assessment

Appendix E

Local Infrastructure Servicing Report

Appendix F

Local Water Management Strategy

Appendix G

Success Hill Frame Heritage Assessment

Appendix H

Aboriginal Cultural Heritage Assessment