

ATTACHMENT A



ASHFIELD GARDENS

LOTS 9 & 10 PEARSON STREET, ASHFIELD

REVISED STRUCTURE PLAN

(Supersedes Ashfield Gardens Structure Plan December 2005)



prepared by

WHELANS
Town Planning Consultants

August 2006

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1.0 INTRODUCTION

1.1 Purpose

Whelans Town Planning Consultants act on behalf of the owners of Lots 9 and 10 Pearson Street, Ashfield, being Majorbay Pty Ltd to prepare a local Structure Plan for the residential development of the site. The preparation and adoption of a local Structure Plan is a requirement under Part 6 of the Town of Bassendean Town Planning Scheme No. 3, prior to the consideration and/or approval of a subdivision or development application on the site. This report will outline the various elements of the subject site in accordance with Part 6 of Scheme No. 3, which in turn will assist in supporting the future residential development of the site.

1.2 Progress to Date

The subject site was purchased by Majorbay Pty Ltd in October 2004, with the intent of developing the site for residential purposes. In order to achieve residential development, the site was required to be rezoned from its existing zoning of Tavern and Service Station to Residential.

As a result, Amendment No. 67 to the Town of Bassendean Town Planning Scheme No. 3 was initiated by Whelans Town Planning Consultants in October 2004, which proposed the rezoning of the subject site from 'Tavern' and 'Service Station' to 'Residential – R20/R30/R40'. The rezoning proposal also included the addition of a Special Control Area. The Special Control Area provisions gave rise to the preparation and adoption of a local Structure Plan for the site.

Amendment No. 67 was granted final approval by the Hon. Minister for Planning & Infrastructure on 10 May 2005, subject to the deletion of the proposed creation of Special Control Areas in Town Planning Scheme No. 3, because this had already been achieved through another Scheme Amendment (No. 66). Instead, the Amendment was to propose the creation of Special Control Area No. 2 (SCA2) over the site, which was also to be shown on the Scheme Map. The modified Amendment was then granted final approval by the Hon. Minister for Planning & Infrastructure on 16 June 2005 and appeared in the Government Gazette on 1 July 2005.

Under the provisions of Town of Bassendean Town Planning Scheme No. 3, a Structure Plan is required to be prepared for the site before any development or subdivision of the site is able to occur. The original Ashfield Gardens Structure Plan (December 2005), submitted in December 2005 for a 35 dwelling development at a density of Residential R40, was considered by the Council at its Meeting held 25 July 2006. In brief, Council resolved not to support the density at R40 on the basis that it did not comply with the provisions of the Scheme, specifically in relation to the Scheme planning criteria for an R40 development. Subsequently, this revised Structure Plan for Ashfield Gardens is for a 31 dwelling development at a density of R30.

2.0 SITE DESCRIPTION

2.1 Location

The subject site is located approximately 9 kilometres east of the Perth Central Area, to the south-east of Guildford Road on the corner of Pearson Street, Jacqueline Street and Hardy Road, Ashfield. The location of the subject site is indicated in Figure 1.

Lot 9 is a 7998m² parcel of land with frontage to Pearson Street, Hardy Road and Jacqueline Street, whilst Lot 10 is a 1520m² parcel of vacant land located at the corner of Pearson Street and Hardy Road.

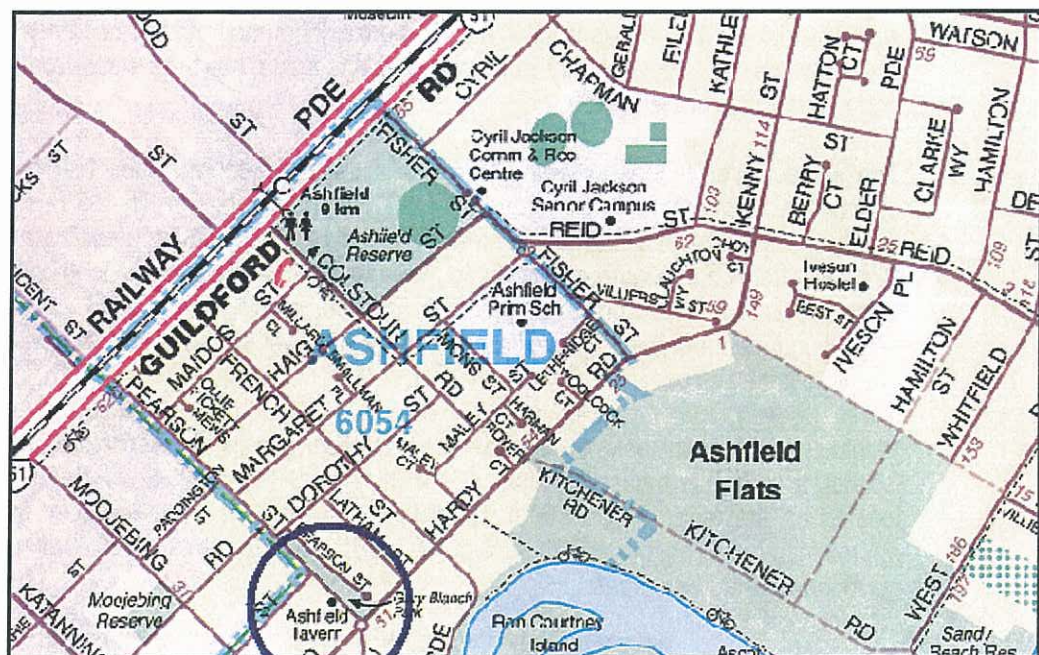


Figure 1: Location of Site

2.2 Legal Description and Ownership

The land that is subject to this Structure Plan is comprised of two lots that are legally described as following: -

- Lot 9 on Diagram 44755 of Volume 2037; Folio 944 – being 7998m².
- Lot 10 on Diagram 44755 of Volume 2037; Folio 945 – being 1520m².

Both Lot 9 and Lot 10 are free of encumbrances and easements that may impact on future development.

2.3 Existing Land Uses

The site is currently vacant land. The former Ashfield Tavern, a 1970's commercial building comprising of public bar access and a drive-through liquor store surrounded by an extensive bitumen car park, was recently demolished.

3.0 CONTEXT AND SITE ANALYSIS

3.1 Walkable Catchments

The walkable catchments for the subject site are displayed in Figure 2. The site is serviced by the Ashfield train station, which is almost within the 800 metre walking distance threshold of the subject site, as indicated in the Commission's Liveable Neighbourhood policy document. The Ashfield station also provides the opportunity for park and ride commuting, however there are currently no formal park and ride facilities at the station.

The subject site is serviced with other public transport opportunities that can be used to access commercial and community services within the Metropolitan region. The 55 Bus Route travels between the Perth Busport and Bassendean Town Centre along Hardy Road, providing an alternative link to the Perth Central Area. Significant land uses within the 400m walkable catchment include the Moojebing Reserve, Gary Blanch Park and the Swan River foreshore, which allow for both passive and active recreational opportunities. The remainder of the area within this catchment is largely residential and light industrial land uses.

Within the 800m walkable catchment of the subject site is the Ashfield Reserve, Ashfield Primary School, and Tonkin Industrial Estate. Cyril Jackson Senior High School and Recreation Centre are located on the fringe of the 800m walkable catchment. The Bassendean Town Centre is located approximately 3 kilometres north-east of the subject site, which is the central community node for the surrounding suburban areas.

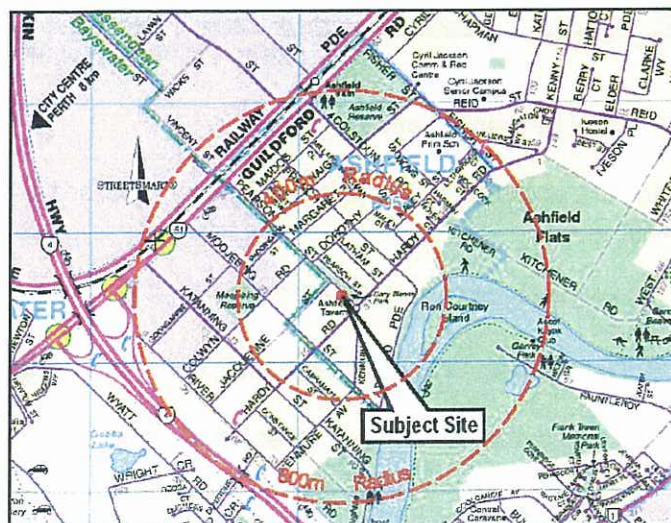


Figure 2: 400m & 800m Walkable Catchment

3.2 Analysis of Surrounding Subdivision and Development

The surrounding area consists of predominantly single storey residences of average quality, varying in age from the 1970's to more recent development. The actual residential density of the immediate locality may be considered to be between R12.5 and R15, despite the zoning of 'Residential – R20' under Scheme No 3. Lot sizes north of the subject site are predominantly between 700m² and 800m², with the areas near the riverfront being characterised by larger lot sizes of 1000m² and upwards. There are a number of infill developments within close proximity to the site, especially adjacent to the subject site on Jacqueline Street.

The existing nature of the road network and lot layout throughout the Ashfield and Bayswater areas largely resemble a modified grid formation of streets that exist on north-west to south-east, and north-east to south-west axes. Further east in Bassendean, the street layout becomes more north-south orientated.

The proposal to redevelop the subject site for residential land uses is a logical continuation of this trend which allows for flexibility in the development of the land and will consolidate and enhance the residential amenity of the locality.

Present redevelopment initiatives in the immediate locality have occurred in the form of duplex grouped housing developments at densities of R20 to replace substandard housing stock. As shown in the aerial photography in Figure 3, there is a significant amount of infill development occurring in the areas adjacent to the subject site, as well as throughout the adjoining suburbs of Bayswater and Bassendean. Land to the south-west, generally around the intersection of Jacqueline and Moojebing Streets, has been cleared and subdivided into lots of approximately 490m² to 650m². New residences are currently being constructed on these lots.



Figure 3: Aerial Photograph showing surrounding infill development with the former Ashfield Tavern building on the site (now demolished and removed from the land)

3.3 Proposed Land Use

It is proposed that the current site be developed for residential purposes at a density of R30. The proposal is for the creation of nineteen (19) green title lots fronting the existing streets, with an internal common accessway servicing 12 survey strata lots (refer to plans in the Appendix). The development overall will comprise of a mix of single and double storey dwellings on these lots. The developer plans to subdivide and construct the dwellings as a house and land package.

4.0 SITE CHARACTERISTICS

4.1 Landform and Vegetation

The subject site is predominantly flat, with an elevation of approximately 10.5 metres above sea level (ASL). The site has a slight decline from west to east to approximately 10 metres ASL at Pearson Street. At present, the site has been largely cleared of any native vegetation.

4.2 Environmental Consideration

The land is located within close proximity to the Swan River. As a result, it has been identified that there is a moderate risk of acid sulphate soil contamination within the site. The areas directly adjacent to the west of the site are identified as high risk. It is anticipated that acid sulphate soils will not pose as a development constraint as there will be limited earthworks associated with the redevelopment. Finished floor levels of the proposed residences will be a minimum of 11.4m ASL, which will negate the requirement for a geo-technical report to demonstrate suitability of the land for residential purposes.

Although Lot 10 was previously zoned for 'Service Station' under the Town Planning Scheme No. 3, the site has never been used for such a purpose, therefore the site poses no environmental concern from a site contamination point of view.

4.3 Cultural and Heritage Consideration

The subject site is not listed in the Heritage Council of Western Australia's Heritage Register, nor the Town of Bassendean Municipal Heritage Inventory.

The site has in the past existed as a cultural meeting place for the local and wider community, however its role as a community node has significantly declined in recent years, and the tavern has subsequently ceased its operations.

5.0 PLANNING FRAMEWORK

5.1 Metropolitan Region Scheme

The subject site is zoned Urban under the Metropolitan Region Scheme. Any development that is to occur on the site would be subject to the provisions of the Residential Design Codes.

5.2 Town of Bassendean Town Planning Scheme No. 3

The subject site exists as a Special Control Area under Town Planning Scheme No. 3. The site is known as SCA2 in Appendix XII of Town Planning Scheme No. 3, which requires the provision of an approved Structure Plan in order to guide the subdivision and development of the area under Section 3.5 of the Scheme No. 3.

Section 3.5 identifies that when split coding is involved, then the lower coding shall apply unless there is specific development criteria in place. In the case of the subject site, the middle R30 coding is being used subsequent to Council's decision on 25 July 2006 to refuse the development application for 35 dwellings at the R40 density.

5.3 Regional and District Planning Studies

Town of Bassendean Local Planning Strategy

The subject site is located with Housing Precinct D under the Town of Bassendean Local Planning Strategy, which was released in February 2005. The Strategy aims to encourage the redevelopment of the area for residential purposes, by allowing for the provision larger housing lots, affected by urban decline, to be redeveloped for medium density housing. It also aims to provide a quality and mix of housing types that will attract population across the age spectrum, including family housing. The Strategy specifically identifies the Ashfield Tavern Site as having the potential as a large grouped dwelling land use.

6.0 STRUCTURE PLAN

6.1 Residential Lot Layout and Yield

This revised Ashfield Gardens Structure Plan proposes redevelopment of the land to provide single and grouped dwelling development at a residential density coding of R30. The total area of the site is 9,518m². The Residential Design Codes specify that for R30 coded areas, a minimum lot size of 270m² and an average lot size of 300m² is required. A total of 31 lots are proposed, ranging in size from 258m² to 312m². The proposed lots satisfy the minimum and average lot size requirement for an R30 development as prescribed under the Town of Bassendean Town Planning Scheme No. 3 and the Residential Design Codes, with the exception of proposed Survey Strata (Grouped Dwelling) Lots 2 and 7, which have areas of 258m² and 260m² respectively.

Clause 3.5.2 'Split Density Code' of the Town of Bassendean TPS No. 3 states:

'3.5.2.2 subdivision or development up to the middle density coding shall be considered to be acceptable to Council where:

- a) in the opinion of Council the lot has a road frontage sufficient to allow at least two homes and a shared accessway to front the street;*
- b) in the case of subdivision, each resulting lot achieves complete frontage to a public road or right-of-way and does not have the form of a battle-axe block;*
- c) the development is completed prior to the issue of any vacant or survey strata title; and*
- d) there is due regard for relevant Local Planning Policies.'*

The proposal complies with the aforementioned scheme provisions, to allow for subdivision and development to achieve the middle density coding of R30 as the lot has three street frontages, there are no battle-axe lots proposed, the development is proposed to be completed prior to the issue of new titles, and due regard has been given for the relevant Local Planning Policies.

Justification for (R30) Lot Size Variation for Survey Strata (Grouped Dwelling) Lots 2 and 7

Survey Strata (Grouped Dwelling) Lots 2 and 7 form a part of the internal grouped dwelling development of the Structure Plan. Clause 3.1.3 of the R-Codes provides discretion to vary the minimum lot size on the basis that the variation does not exceed 5% of the minimum site area requirement, in this case, 5% equates to 13.5m². No grouped dwelling lot within the Structure Plan should fall below 256.5m². Lot 2 has an area of 258m² and Lot 7 has an area of 260m².

The subject site is relatively large, having an area of 9,518m², and frontage to three constructed public streets. Due to the dimensions of the subject site, it is difficult to subdivide the property and create a new internal public street. The most efficient subdivision design, which maximises the land for residential development, involves creating green title lots with frontage to the existing public streets with provision of an internal common laneway to provide vehicular access to residential lots in the middle of the site. Accordingly, nineteen (19) dwellings have frontage to the public streets on the fringes of the site, with the remaining twelve (12) dwellings utilising a common laneway which is

accessed from Pearson Street. The proposed lot layout does not create a battleaxe subdivision design.

The internal accessway requires a substantial amount of land, in order to achieve the minimum standard for turning circles and vehicular manoeuvring. This is one of the primary reasons why it is difficult to achieve the minimum lot size for internal Lots 2 and 7. The variation to the minimum lot size in each case, [being approximately 4.4% or 12m² for Lot 2 and 3.7% or 10m² for Lot 7], is considered to be minor in the context of the overall development and will not have an adverse impact on the streetscape. A two storey dwelling is proposed on Lot 2 and Lot 7 and there is sufficient land to provide a dwelling which complies with the design standards of the R-Codes, including setbacks and open space. Accordingly, it is considered that the variation to the minimum lot area in both instances, will not be detrimental to the adjoining properties, the streetscape or the future occupants of the development.

6.2 Built Form

The resulting built form will consist of a mixture of single storey and two storey dwellings, providing for a diverse and interesting development. The dwellings are intended to be developed as house and land packages, which will assist in providing a quality built form product as well as minimise the need for specific design guidelines to control the style, materials and finishes of the proposed dwellings. As shown on the Development Plan, some of the lots are proposed for two storey dwellings, while the remaining lots are intended for single storey dwellings.

The dwellings are proposed to be constructed from materials such as brick and tile, with contrasting features of timber and colour being used throughout the development to provide a level of interest and harmony of built form.

Based on the Development Plan (refer Appendix C) all lots propose private courtyards (private open space) in accordance with the Residential Design Codes, being a minimum 45% of the site.

6.3 Parking, Refuse Collection and Postal

In accordance with the provisions of the Residential Design Codes, all proposed dwellings will be provided with a minimum of two car parking bays. Formal visitor parking is required to be provided at a rate of 1 bay per 4 dwellings serviced by common access. Three visitor parking bays are provided in a convenient location within the internal access area.

There are a total of 12 dwellings proposed within the central area of the development, accessed by a common property driveway. The other 19 dwellings each have direct access to an existing public street.

An area has been set aside on the Pearson Street frontage, abutting the entrance to the common property driveway, for a designated bin collection area. The 12 lots accessed by the common property driveway require an area to be set aside for the exclusive use of rubbish bins, so that refuse collection services do not need to be entering into the site. Lots with direct street frontage to Pearson Street, Hardy Road and Jacqueline Street will be able to accommodate a bin pad area on the street verge,

consistent with existing residential standards. All refuse bins for proposed dwellings are intended to be retained within the respective lots.

Similarly, postal services have been accommodated by way of a communal letter box area at the entrance to the common property driveway on Pearson Street to serve the central units. Lots with direct street frontage to existing public roads will provide their own letter boxes in accordance with established practices.

6.4 Public Open Space

The revised Structure Plan does not propose any public open space, primarily due to the size of the development area and existing public open space in the neighbourhood. The site has an overall area of 9518m² (being 7998m² for Lot 9 and 1520m² for Lot 10), which would attract a 951.8m² public open space contribution. Any provision of public open space to be accommodated on-site for this site would be deemed unusable and too small for the purpose of which it is intended.

In accordance with the requirements of the Western Australian Planning Commission, a 10% cash in lieu payment is proposed as part of the development. The Gary Blanch Reserve is currently underdeveloped from a local open space point of view, with limited active and/or children's playground facilities. As this reserve is less than 50 metres from the subject site, it would be considered appropriate to use the cash in lieu funds for capital improvements to the Reserve.

7.0 DEVELOPMENT PLAN

The proposed development plan contained in Appendix C shows the general layout of dwellings on the proposed lots, as well as areas for vehicular access, visitor parking, landscaping and private courtyards. This plan is proposed to be submitted to the Town of Bassendean for planning approval and will be considered simultaneously with consideration of this revised Structure Plan.

8.0 MOVEMENT NETWORK

8.1 Internal Movement

Access is proposed to be provided to the central units (proposed Units 11 - 22) via a common property driveway. The driveway has been designed to satisfy the requirements of the Residential Design Codes and the Town's Engineering requirements, and has a minimum pavement width of 5.2 metres, with the majority of the access way being designed at minimum of 6 metres wide where necessary for vehicle manoeuvring.

In order to achieve sufficient space for vehicle manoeuvring within the common property access way to allow all vehicles to enter and exit the site in forward gear, buildings are also proposed to be setback. Not only will this allow for sufficient turning circles to be achieved, a separate pedestrian pathway of 1.2 metres may be accommodated in certain areas within the common property, as well as landscaping on the edges of the driveway.

The width of the driveway is of sufficient size to allow opportunities for vehicles to adequately pass within the accessway. The internal accessway area will be shared by both pedestrians and vehicles, with a different colour of paving being provided to show the pedestrian accessway on the ground.

8.2 External Movement

The subject site is bound by Pearson Street, Hardy Road, and Jacqueline Street. The site is accessible from each of these roads. With the exception of those lots accessing the central common property driveway (referred to above), all units propose direct street frontage to a public road. There are no road widening requirements applicable to Pearson Street, Hardy Road or Jacqueline Street where they adjoin the site. Footpaths exist on both Pearson Street and Hardy Road abutting the subject site.

9.0 SERVICING INFRASTRUCTURE AND UTILITIES

9.1 Power & Telecommunications

The site is currently serviced with overhead power lines, with the main power supply into the site from Pearson Street. Telecommunication services are provided in the same manner.

9.2 Sewer

There are currently connections to reticulated sewer within both the Hardy Road and Jacqueline Street road reserves, on the southern extremities of the site, which are proposed to be extended to connect the five green title lots. There is capacity within the system to accommodate the proposed development.

9.3 Water

The site is also serviced by reticulated water from Hardy Road, with a connection also available from Jacqueline Street.

9.4 Site Drainage

Drainage services are provided from both Hardy Road and Jacqueline Street and also within the Pearson Street road reserve, however stormwater is proposed to be contained on-site in accordance with the Residential Design Codes of Western Australia.

10.0 CONCLUSION

The subject site was purchased by Majorbay Pty Ltd in October 2004, with the intent of demolishing the Ashfield Tavern and developing the site for residential purposes. The site has recently been rezoned to Residential R20/30/40 and Whelans Town Planning Consultants act on behalf of the owners of Lots 9 and 10 Pearson Street, Ashfield, to prepare a local Structure Plan for the residential development of site.

The original Ashfield Gardens Structure Plan (December 2005), submitted in December 2005 for a 35 dwelling development at a density of Residential R40, was considered by the Council at its Meeting held 25 July 2006. In brief, Council resolved not to support the density at R40 on the basis that it did not comply with the provisions of the Scheme, specifically in relation to the planning criteria of the Scheme necessary for an R40 development. This revised Structure Plan for Ashfield Gardens is for a 31 dwelling development at a density of R30.

The lot sizes and density in the proposed revised Structure Plan comply with the relevant town planning provisions, with the exception of Survey Strata (Grouped Dwelling) Lots 2 and 7, which have areas of 258m² and 260m², being short of the minimum R30 lot size of 270m² by 12m² (or 4.4%) and 10m² (or 3.7%) respectively. The report provides justification for this variation under Section 6.1, which refers to Clause 3.1.3 of the R-Codes and the 5% discretion for grouped dwelling minimum lot size.

The developer does not want to produce a development of poor quality and is using latest and up to date technology in building and construction materials. The developer is experienced and fully competent to coordinate and deliver a high quality product. Approval of the revised Structure Plan will provide the developer opportunity to transform the former Ashfield Tavern site, into a house and land package development, with attractive landscaping treatments and as a coordinated modern residential development.

APPENDIX A

REVISED STRUCTURE PLAN



LEGEND

- Grouped Housing
- Single Dwelling
- Common property (driveway)

Revised
Structure
Plan

Job Number: FP-06329-010-REV.1.0
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 Scale 1:500 @ A3
 Date: 11 September 2008
 Drawn By: SJF
 Checked by: JP
 File: jplan0329\working_drawings\060910 structure plan.dwg
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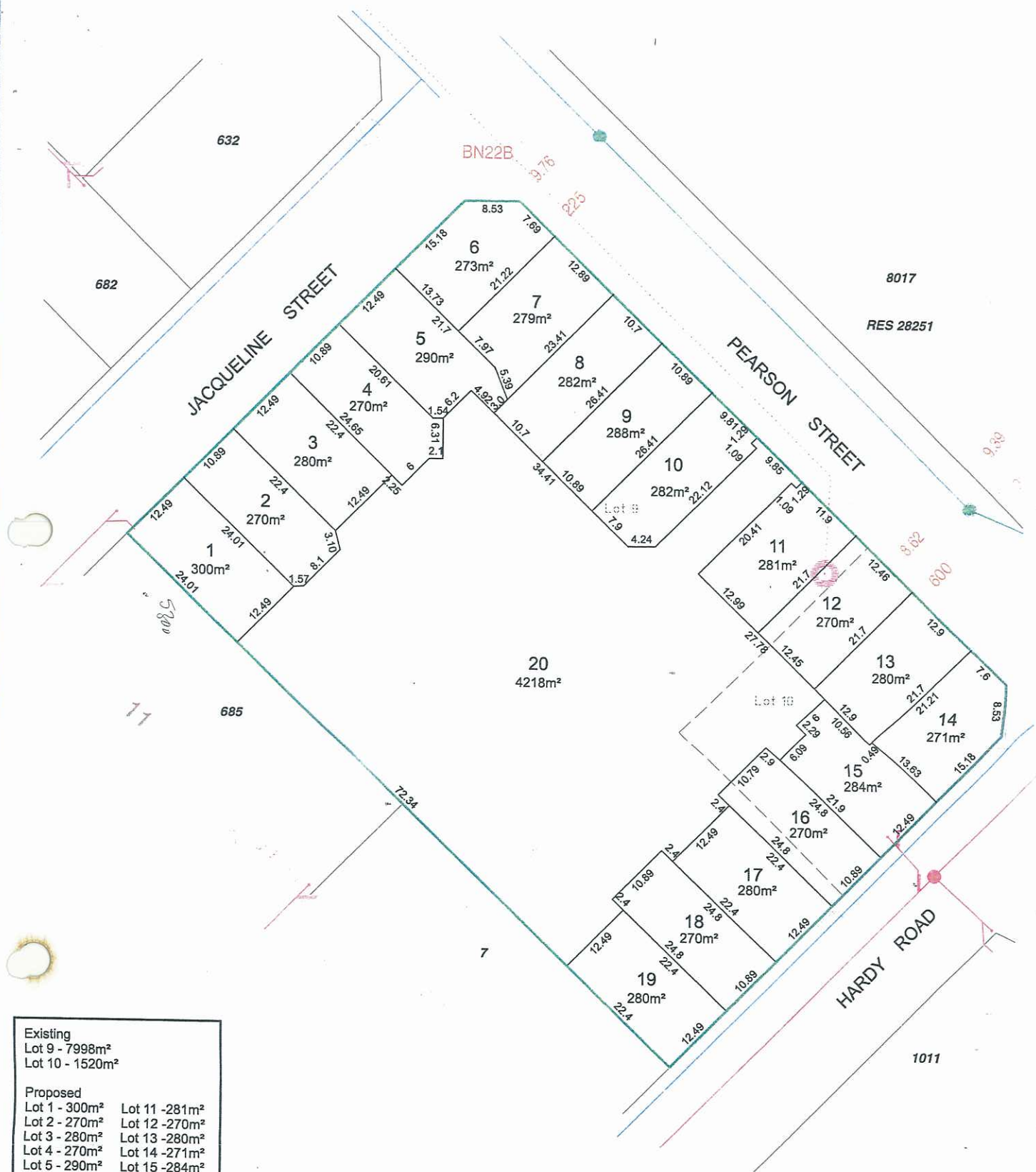
STRUCTURE PLAN

STRUCTURE PLAN LOTS 9 AND 10 PEARSON STREET ASHFIELD

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 Member Practice

APPENDIX B

PROPOSED SUBDIVISION



Existing
Lot 9 - 7998m²
Lot 10 - 1520m²

Proposed
Lot 1 - 300m² Lot 11 - 281m²
Lot 2 - 270m² Lot 12 - 270m²
Lot 3 - 280m² Lot 13 - 280m²
Lot 4 - 270m² Lot 14 - 271m²
Lot 5 - 290m² Lot 15 - 284m²
Lot 6 - 273m² Lot 16 - 270m²
Lot 7 - 279m² Lot 17 - 280m²
Lot 8 - 282m² Lot 18 - 270m²
Lot 9 - 288m² Lot 19 - 280m²
Lot 10 - 282m² Lot 20 - 4218m²

Existing No. Lots 2
Proposed No. Lots 20

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Scale: 1:500 @ A3
Date: 11 September 2006
Drawn By: SJF
Checked by: JP

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PROPOSED GREEN TITLE SUBDIVISION LOTS 9 AND 10 PEARSON STREET ASHFIELD

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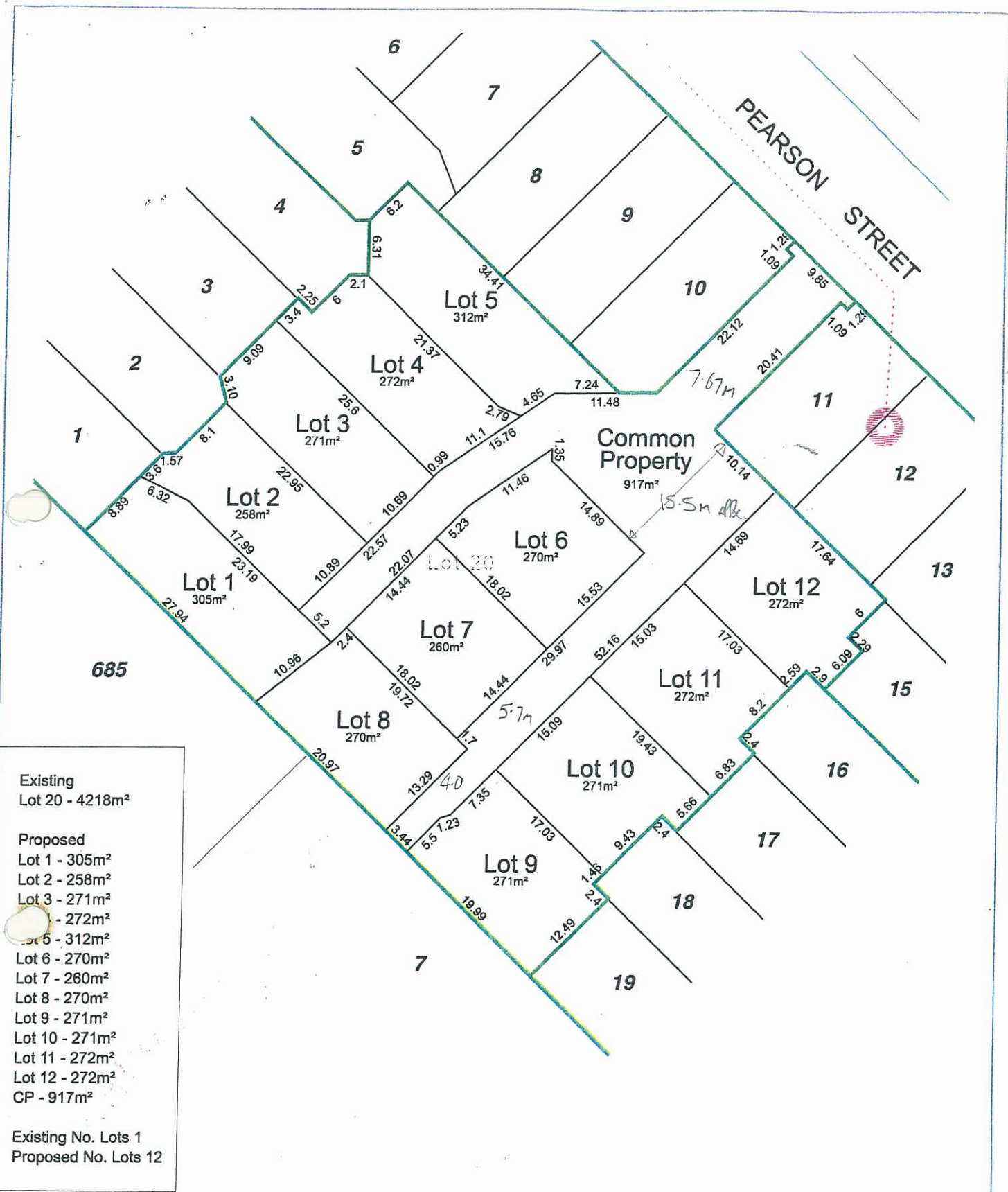
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SUBDIVISION PLAN



Job Number: 08329
Sheet Name: FP-08329-014-REV.1.0

Scale 1:600 @ A4

Date: 11 September 2006

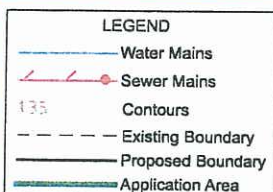
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SURVEY STRATA, GROUPED DWELLING LOT 20 PEARSON STREET ASHFIELD

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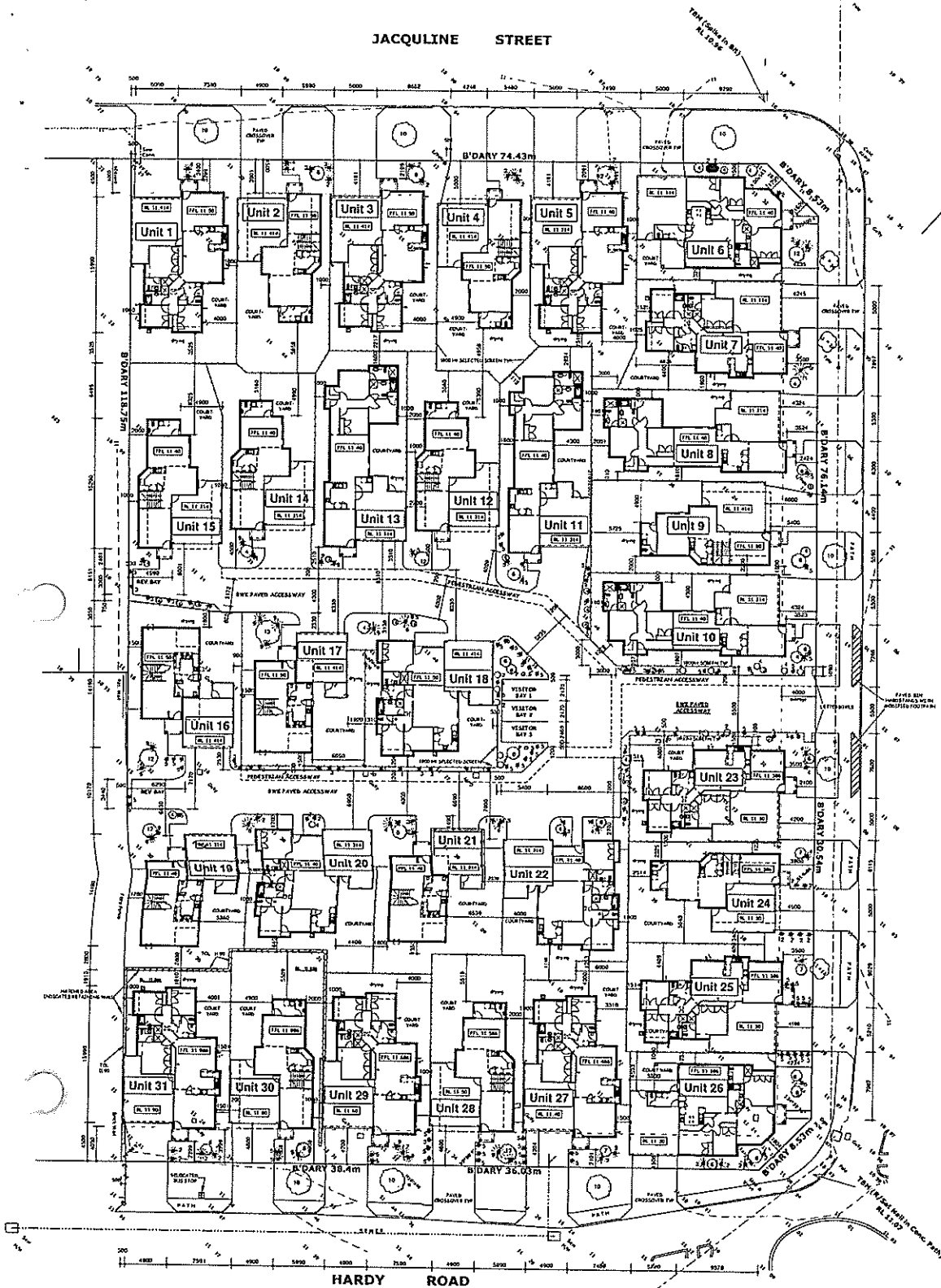
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APPENDIX C

PROPOSED DEVELOPMENT PLAN

JACQUINE STREET



PEARSON STREET

LANDSCAPE KEY

- 1. GREVILLEA ORNITHOXYLIA
 - 2. ACOTYLOS STEREOXYLIA (African daisy)
 - 3. HEALING HERB
 - 4. COLEOPTERIS PULCHRA (fern)
 - 5. AGAVEUS ORIENTALIS (succulent)
 - 6. GARDENIA SP (shrub)
 - 7. ACHILLEA FLORENS (herb)
 - 8. CAMELIA SP (shrub)
 - 9. HYDRANGEA SP (shrub)
 - 10. CALLISTEMON KING OF THE BUSH
 - 11. Grevillea
 - 12. ACOTYLOS STEREOXYLIA
 - 13. DELONIX REGIA (tree)
- PLANT SELECTION SUBJECT TO AVAILABILITY
SPREAD EVENLY BETWEEN PLANTS
PEAK PLANT TO 100mm thick min
LAWN AREA TO BE SELECTED COUGH
LAWN AS RECOMMENDED BY SUPPLIER

SITE CALCULATIONS

TOTAL COMBINED LOT AREA 9518m²

BUILDING AREAS	EXCLUSIVE USE AREAS	OPEN SPACE
UNIT 1 - 153m ²	300m ²	25%
UNIT 2 - 153m ²	270m ²	22%
UNIT 3 - 153m ²	270m ²	22%
UNIT 4 - 153m ²	270m ²	22%
UNIT 5 - 153m ²	270m ²	22%
UNIT 6 - 153m ²	270m ²	22%
UNIT 7 - 153m ²	270m ²	22%
UNIT 8 - 153m ²	270m ²	22%
UNIT 9 - 153m ²	270m ²	22%
UNIT 10 - 153m ²	270m ²	22%
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UNIT 28 - 153m ²	270m ²	22%
UNIT 29 - 153m ²	270m ²	22%
UNIT 30 - 153m ²	270m ²	22%
UNIT 31 - 153m ²	270m ²	22%

UNIT 1 - 153m ²	270m ²	22%
UNIT 2 - 153m ²	270m ²	22%
UNIT 3 - 153m ²	270m ²	22%
UNIT 4 - 153m ²	270m ²	22%
UNIT 5 - 153m ²	270m ²	22%
UNIT 6 - 153m ²	270m ²	22%
UNIT 7 - 153m ²	270m ²	22%
UNIT 8 - 153m ²	270m ²	22%
UNIT 9 - 153m ²	270m ²	22%
UNIT 10 - 153m ²	270m ²	22%
UNIT 11 - 153m ²	270m ²	22%
UNIT 12 - 153m ²	270m ²	22%
UNIT 13 - 153m ²	270m ²	22%
UNIT 14 - 153m ²	270m ²	22%
UNIT 15 - 153m ²	270m ²	22%
UNIT 16 - 153m ²	270m ²	22%
UNIT 17 - 153m ²	270m ²	22%
UNIT 18 - 153m ²	270m ²	22%
UNIT 19 - 153m ²	270m ²	22%
UNIT 20 - 153m ²	270m ²	22%
UNIT 21 - 153m ²	270m ²	22%
UNIT 22 - 153m ²	270m ²	22%
UNIT 23 - 153m ²	270m ²	22%
UNIT 24 - 153m ²	270m ²	22%
UNIT 25 - 153m ²	270m ²	22%
UNIT 26 - 153m ²	270m ²	22%
UNIT 27 - 153m ²	270m ²	22%
UNIT 28 - 153m ²	270m ²	22%
UNIT 29 - 153m ²	270m ²	22%
UNIT 30 - 153m ²	270m ²	22%
UNIT 31 - 153m ²	270m ²	22%

SITE PLAN 1:250



JACQUINE STREET ELEVATION



HARDY ROAD ELEVATION



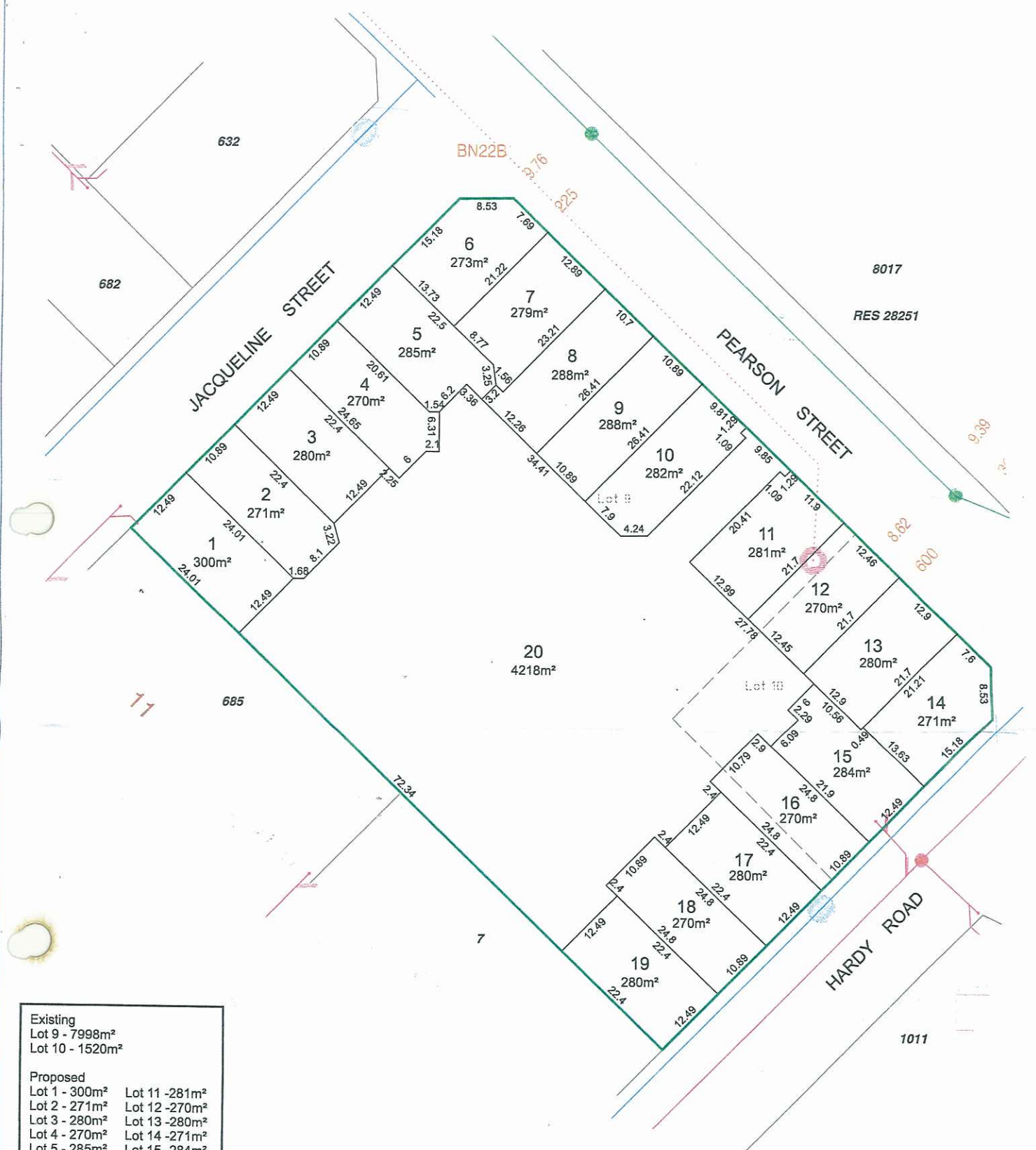
SOUTHERN BOUNDARY ELEVATION



PEARSON STREET ELEVATION

DATE: 21/04/2019	PROPOSED DWELLINGS
SCALE: 1:250	LOT 9 & 10
EXAMINE BY: J. WILSON	PEARSON STREET
DATE: 21/04/2019	ASHFIELD
SHEET NO.: 1 OF 1	

NOTE: (1) Boundary measured from topographic data and field notes.
(2) Boundary is subject to the relevant survey data.
(3) Survey plotted from Water Corporation Data.
(4) Datum is AHD.
(5) Buildings shown as outline only.



Existing
 Lot 9 - 7998m²
 Lot 10 - 1520m²

Proposed
 Lot 1 - 300m² Lot 11 - 281m²
 Lot 2 - 271m² Lot 12 - 270m²
 Lot 3 - 280m² Lot 13 - 280m²
 Lot 4 - 270m² Lot 14 - 271m²
 Lot 5 - 285m² Lot 15 - 284m²
 Lot 6 - 273m² Lot 16 - 270m²
 Lot 7 - 279m² Lot 17 - 280m²
 Lot 8 - 288m² Lot 18 - 270m²
 Lot 9 - 288m² Lot 19 - 280m²
 Lot 10 - 282m² Lot 20 - 4281m²

Existing No. Lots 2
 Proposed No. Lots 20

DEPARTMENT FOR PLANNING
 AND INFRASTRUCTURE

- 1 SEP 2006

FILE 131236

PROPOSED PLAN
 date stamped

- 9 MAY 2006

Job Number: 06329
 Sheet Name: FP-06329-017-REV.1.0
 Scale: 1:500 @ A3
 Date: 28 August 2006
 Drawn By: SJF
 Checked by: JP
 File: jplan06329working_drawing\060628 proposed subdivision.dgn
 All dimensions and areas are subject to survey.
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 This statement must accompany this plan and digital data at all times.



SUBDIVISION PLAN

**PROPOSED GREEN TITLE SUBDIVISION
 LOTS 9 AND 10 PEARSON STREET
 ASHFIELD**

WHELAN'S
 LAND is Our Business

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 133 Scarborough Beach Road
 PO Box 99 Mount Hawthorn
 Western Australia 6016

Telephone: (08) 9443 1511
 Facsimile: (08) 9444 3901
 E-mail: whelans@whelans.com.au
 Web Page: www.whelans.com.au

Member Practice

PEARSON STREET

685

Existing
Lot 20 - 4218m²

Proposed
Lot 1 - 291m²
Lot 2 - 270m²
Lot 3 - 271m²
Lot 4 - 272m²
Lot 5 - 312m²
Lot 6 - 270m²
Lot 7 - 260m²
Lot 8 - 270m²
Lot 9 - 271m²
Lot 10 - 271m²
Lot 11 - 272m²
Lot 12 - 272m²
CP - 917m²

Existing No. Lots 1
Proposed No. Lots 12

DEPARTMENT FOR PLANNING
AND INFRASTRUCTURE

- 1 SEP 2006

FILE 583-06

AMENDED PLAN
superseded plan
date stamped

- 9 MAY 2005

Job Number: 08329

Sheet Name: FP-08329-014-REV.1.0

Scale 1:600 @ A4

Date: 28 August 2006

Drawn By: SJF







Checked by: JP

File: j:\8329\working_drawings\080618_proposed_strata.dgn

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data at all times.

LEGEND

-  Water Mains
-  Sewer Mains
-  135 Contours
-  Existing Boundary
-  Proposed Boundary
-  Application Area

SURVEY STRATA PLAN

SURVEY STRATA, GROUPED DWELLING LOT 20 PEARSON STREET ASHFIELD

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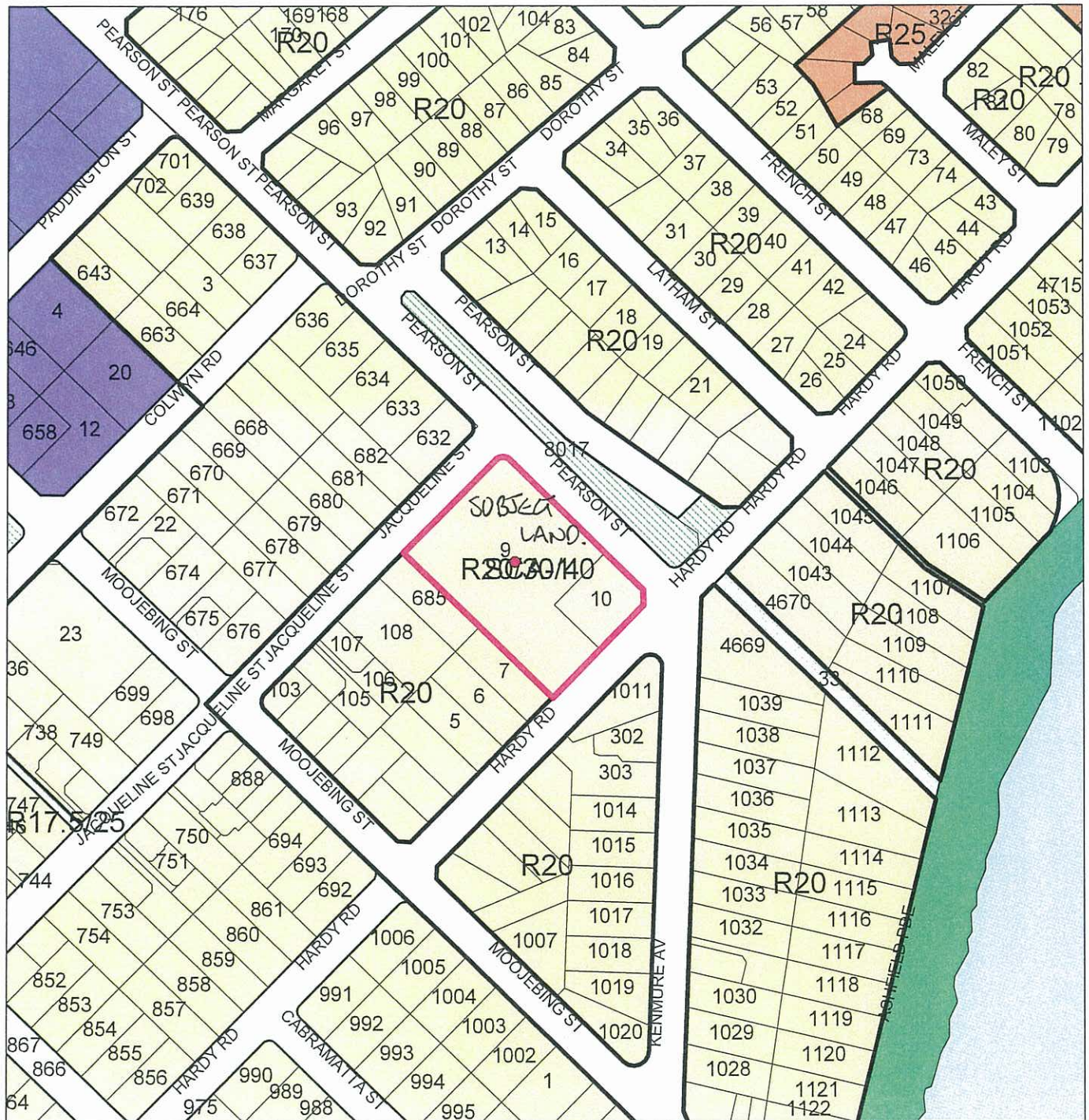
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ccpc
Member Practice





Cadastre with Lot Numbers	DRAINAGE	RESIDENTIAL
SPECIAL CONTROL AREA - DEVELOPMENT AREAS	GENERAL INDUSTRY	SINGLE RESIDENTIAL
TPS - Scheme Boundaries	GROUP RESIDENTIAL	
TPS - R Code Boundaries	LOCAL PUBLIC OPEN SPACE	
WATERWAYS	PUBLIC UTILITIES	
PARKS & RECREATION	RECREATION	

Scale 1:3,400
0 50 m

Prepared by: rcull
Prepared for:
Date: Thursday, November 23, 2006 10:19
Plot identifier: P20061123_1018



Department for
Planning and Infrastructure

DPI INTERNAL USE ONLY

APPENDIX XII – SPECIAL CONTROL AREAS

REF NO	AREA	PROVISIONS
SCA-1 <i>AMD 66 GG 2/11/04</i>	Lots 1, 2, 822, 823, 824, 825, 826, 827 and 7716 Scaddan Street and Lot 839 and Portion Swan Location Q1 Railway Parade and a portion of Scaddan Street road reserve, Bassendean	An approved Structure Plan together with all approved amendments shall apply to the land in order to guide subdivision and development.
SCA-2 <i>AMD 67 GG 1/7/05</i>	Lots 9 and 10 Pearson Street, Ashfield	An approved Structure Plan together with all approved amendments shall apply to the land in order to guide subdivision and development.