



Metro Central Joint Development Assessment Panel Agenda

Meeting Date and Time: 29 May 2017; 10:30am
Meeting Number: MCJDAP/240
Meeting Venue: Town of Bassendean
35 Old Perth Road
Bassendean

Attendance

DAP Members

Mr Charles Johnson (Presiding Member)
Mr Christopher Antill (Deputy Presiding Member)
Mr Luigi D'Alessandro (Specialist Member)
Cr Gerry Pule (Local Government Member, Town of Bassendean)

Officers in attendance

Mr Christian Buttle (Town of Bassendean)
Mr Dylan Stokes (Town of Bassendean)

Local Government Minute Secretary

Ms Amy Holmes (Town of Bassendean)

Applicants and Submitters

Mr Alessandro Stagno (Planning Solutions)
Mr Scott Vincent (Planning Solutions)

Members of the Public / Media

Nil

1. Declaration of Opening

The Presiding Member declares the meeting open and acknowledges the past and present traditional owners and custodians of the land on which the meeting is being held.

2. Apologies

Nil

3. Members on Leave of Absence

Nil



4. Noting of Minutes

Note the Minutes of meeting No.236 held on the 2 May 2017.

The Minutes of meeting No.239 held on 18 May 2017 were not available at time of Agenda preparation.

5. Declarations of Due Consideration

Any member who is not familiar with the substance of any report or other information provided for consideration at the DAP meeting must declare that fact before the meeting considers the matter.

6. Disclosure of Interests

Nil

7. Deputations and Presentations

- 7.1** Mr Scott Vincent (Planning Solutions) presenting for the application at Item 8.1. The presentation will support the officer recommendation and request deletion and modification to conditions.

8. Form 1 - Responsible Authority Reports – DAP Applications

- 8.1** Property Location: Lot 25 (No. 300) Collier Road, Bassendean
Application Details: Proposed Convenience Store
Applicant: Planning Solutions
Owner: HICON (WA) PTY LTD
Responsible authority: Town of Bassendean
DoP File No: DAP/17/01187

9. Form 2 – Responsible Authority Reports - Amending or cancelling DAP development approval

- 9.1** Property Location: Lot 54 (Nos. 72 – 74) Railway Parade, Bassendean
Application Details: Proposed 22 Multiple Dwellings
Applicant: Mr Joe Douglas of Urban & Rural Perspectives
Owner: Mark Francis Hammond & Sandra Lee Hammond
Responsible authority: Town of Bassendean
DoP File No: DAP/15/00740

10. Appeals to the State Administrative Tribunal

Nil

11. General Business / Meeting Closure

In accordance with Standing Order 7.3 only the Presiding Member may publicly comment on the operations or determinations of a DAP and other DAP members should not be approached to make comment.



Minutes of the Metro Central Joint Development Assessment Panel

Meeting Date and Time: 2 May 2017; 2.00pm
Meeting Number: MCJDAP/236
Meeting Venue: City of Canning
1317 Albany Highway
Cannington

Attendance

DAP Members

Mr Charles Johnson (Presiding Member)
Mr Christopher Antill (Deputy Presiding Member)
Mr Luigi D'Alessandro (Specialist Member)
Cr Rebecca Aubrey (Local Government Member, City of Melville)
Cr Tim Barling (Local Government Member, City of Melville)
Cr Pauline Tarrant (Local Government Member, City of Canning)
Cr Margaret Hall (Local Government Member, City of Canning)

Officers in Attendance

Mr Mark Scarfone (City of Melville)
Mr Matthew Brookes (City of Canning)
Ms Kelly Vilkson (City of Canning)

Local Government Minute Secretary

Ms Barbara Rankin (City of Canning)
Ms Karen Farrington (City of Canning)
Mrs Donna Ross (City of Canning)

Applicants and Submitters

Mr David Reynolds (TBB)
Mr Alex Whitely (Niche Living)
Mr Neil Teo (Dynamic Planning & Developments)
Mr Fred Zuideveld (ZMH Architects)

Members of the Public / Media

There were three members of the public in attendance.

1. Declaration of Opening

The Presiding Member, Mr Charles Johnson declared the meeting open at 2.00pm on 2 May 2017 and acknowledged the past and present traditional owners and custodians of the land on which the meeting was being held.

Mr Charles Johnson
Presiding Member, Metro Central JDAP



The Presiding Member announced the meeting would be run in accordance with the *Development Assessment Panel Standing Orders 2012* under the *Planning and Development (Development Assessment Panels) Regulations 2011*.

The Presiding Member advised that the meeting is being audio recorded in accordance with Section 5.16 of the Standing Orders 2012; No Recording of Meeting, which states: 'A person must not use any electronic, visual or audio recording device or instrument to record the proceedings of the DAP meeting unless the Presiding Member has given permission to do so.' The Presiding Member granted permission for the minute taker to record proceedings for the purpose of the minutes only.

2. Apologies

Cr Cameron Schuster (Local Government Member, City of Melville)
Cr Nicole Foxton (Local Government Member, City of Melville)

3. Members on Leave of Absence

Nil.

4. Noting of Minutes

The Minutes of meeting No. 235 held on 27 April 2017 were not available at time of Agenda preparation.

5. Declaration of Due Consideration

All members declared that they had duly considered the documents.

6. Disclosure of Interests

Nil.

7. Deputations and Presentations

7.1 Mr Neil Teo (Dynamic Planning & Developments) addressed the DAP for the application at Item 8.1. Mr Neil Teo (Dynamic Planning & Developments) answered questions from the Panel.

7.2 Mr Mark Scarfone (City of Melville) answered questions from the panel.

7.3 Mr David Reynolds (TBB) and Mr Fred Zuideveld (ZMH Architects) addressed the DAP for the application at Item 8.2. Mr David Reynolds (TBB) answered questions from the panel.

7.4 Miss Kelly Vilkinson (City of Canning) answered questions from the panel.

Mr Charles Johnson
Presiding Member, Metro Central JDAP



8. Form 1 - Responsible Authority Reports – DAP Application

8.1	Property Location:	Lot 552 (No. 25) Willcock Street
	Application Details:	Four Storey Mixed Use Development
	Applicant:	Tuscom Subdivision Consultants
	Owner:	DVT Holdings Pty Ltd
	Responsible authority:	City of Melville
	DoP File No:	DAP/16/01020

REPORT RECOMMENDATION / PRIMARY MOTION

Moved by: Cr Rebecca Aubrey **Seconded by:** Cr Tim Barling

That the Metro Central Joint Development Assessment Panel resolves to:

Refuse DAP Application reference DAP/16/01020 and accompanying plans Site Plan 5429, A1.01, A1.03, A1.04, A1.05, A2.01, A2.02, and A2.03 in accordance with City of Melville Local Planning Scheme No. 6 for the following reasons:

Reasons

1. The proposed development does not meet the general objectives or development standards of the Riseley Centre Structure Plan and would result in a building which has a negative impact on the streetscape and provides a poor level of amenity for future occupiers of the building.
2. The proposed development does not satisfy the Deemed-to-Comply or Design Principles of the Residential Design Codes in relation to 6.1.4 Lot Boundary Setbacks.

REASON: In accordance with details contained in the Responsible Authority Report.

The Report Recommendation/Primary Motion was put and CARRIED UNANIMOUSLY.

Mr Charles Johnson
Presiding Member, Metro Central JDAP



8.2	Property Location:	Lot 99 Portcullis Drive Willetton and Part Lot 130 Woodthorpe Drive, Willetton
	Application Details:	63 Grouped Dwellings and associated Sales Office, and 7889m2 Public Open Space Area
	Applicant:	Mr David Reynolds, Taylor Burrell Barnett
	Owner:	Mr Paul Bitdorf and Mr Ronnie Michel-Elhaj, Geraldton Project Wells Pty Ltd and Freedom Willetton Pty Ltd
	Responsible authority:	City of Canning
	DoP File No:	DAP/16/01135

REPORT RECOMMENDATION / PRIMARY MOTION

Moved by: Cr Pauline Tarrant **Seconded by:** Cr Margaret Hall

That the Metro Central Joint Development Assessment Panel resolves to:

Approve DAP Application reference DAP/16/01135 and accompanying plans including Plan of Survey Strata – Subdivision (ref: 13/046/021H), Indicative Staging Plan (ref: 13/046020C), Site Plan, Ground Floor Plan, First Floor Plan, Street Elevations Plan, Colour Scheme Options, Floor Plans (Type P1: P, PF, SFT, and ST), Elevation Plans (Type P1: P, PF, SFT, and ST), Floor Plans (Type P2: S and SF), Elevation Plans (Type P2: S and SF), Floor Plans (Type P3: S and SF), Elevation Plans (Type P3: S and SF), Floor Plans (Type P4: S), Elevation Plans (Type P4: S), Floor Plans (Type P5: P, PF, S and SF), Elevation Plans (Type P5: P(A), P(B), P(C), PF(A), PF(B), PF(C), S, and SF), Floor Plans (Type P7: PCF, S and SF), Elevation Plans (Type P7: PCF, S, SF(A) and SF(B)), Floor Plans (Type P8: S and SF), Elevation Plans (Type P8: S and SF), Floor Plans (Type P9: S), Elevation Plans (Type P9: S(A) and S(B)), Floor Plans (Type P10: S), Elevation Plans (Type P10: S), Floor Plans (Type P11: S), Elevation Plans (Type P11: S), Floor Plans (Type P13: S), Elevation Plans (Type P13: S), Floor Plans (Type P14: P, PF, ST, STF), Elevation Plans (Type P14: P, PF, ST(A), ST(B), STF(A), STF(B)), Floor Plans (Type P15: P), Elevation Plans (Type P15: P), Typical Signage Plan, Typical Signage Lot 20 Plan, Proposed Sales Office Site Plan, Proposed Sales Office Floor Plan, Proposed Sales Office Elevations Plan, Proposed POS Landscape Plan (ref: 15005 SK02, rev: I), Landscape Proposal Plan (Rev: A), Road Cross-Sections Plan (ref: 15005 SK03, rev: E), Galt Geotechnics Site & Location Plan (ref: J1501059), Pre Development Catchment and Flow Path Plan (ref: 15-101-WM-1), Post Development Catchments and Proposed Basin Plan (ref: 15-101-WM-2), Earthworks Plan Sheet 2 of 3 (ref: C102, rev B), Earthworks Plan Sheet 3 of 3 (ref: C103, rev B), Kerb Gutter Levels Plan (ref: C104, rev B), Combined Services Plan Stage 1 (ref: C105, rev B), Design Vehicle Turning Movements (ref: SK001, rev C), Design Vehicle SISD Verification (ref: SK004, rev B), Street Light Pole Location Sketch (ref: SK005, rev B), Garage Trucks Turning Movements (ref: SK001, rev A), Design Vehicle Turning Movements Lot 200 (ref: SK003, rev A), and Transformer Example Plan in accordance with Clause 68 of the Deemed Provisions of the Planning and Development (Local Planning Schemes) Regulations 2015 and in accordance with the City of Canning Local Planning Scheme No. 40, subject to the following conditions/for the following reasons as follows:

Mr Charles Johnson
Presiding Member, Metro Central JDAP



Condition(s):

1. Prior to the occupation or use of any dwelling, the existing Lot 99 on Plan 2903 and Part Lots 130 on Plan 34511 that are subject of this application are to be amalgamated on a diagram or plan of survey (deposited plan) and a new certificate of title obtained.
2. Prior to the occupation or use of any dwelling, the proposed reserve shown on the approved plan of subdivision as Public Open Space being shown on the diagram or plan of survey (deposited plan) as 'reserved for recreation' and vested in the Crown under Section 152 of the *Planning and Development Act 2005*, such land to be ceded free of cost and without any payment of compensation by the Crown.
3. Prior to the submission of an application for building permit, a notification, pursuant to Section 70A of the *Transfer of Land Act 1893* is to be registered on the certificate(s) of title of the proposed lot(s) affected by noise levels exceeding the noise targets as per State Planning Policy No. 5.4 - Road and Rail Transport Noise and Freight Considerations in Land Use Planning. Notice of this notification is to be included on the diagram or plan of survey (deposited plan). The notification is to state as follows:

"The lot/s is/are situated in the vicinity of a transport corridor and is currently affected, or may in the future be affected by transport noise."
4. Prior to occupation or use of any dwelling, a solid articulated masonry wall (or material of similar acoustic performance) is to be constructed in accordance with the recommendations outlined in the implementation guidelines associated with State Planning Policy No. 5.4 - Road and Rail Transport Noise and Freight Considerations in Land Use Planning to the satisfaction of the City:
 - a. along the full extent of the Roe Highway lot boundary and Lot 800 on Plan 67212; or
 - b. a lesser extent of the Roe Highway lot boundary and Lot 800 on Plan 67212 subject to the applicant submitting an updated acoustic report from a suitably qualified consultant to the satisfaction of the City in consultation with Main Roads WA.
5. Prior to the occupation or use of any dwelling, a solid boundary wall to a height of 1.8 metres is to be constructed along the shared boundary of Lot 55 on Diagram 92673 to provide noise attenuation and privacy as indicated on the Local Structure Plan No. SPN/0753 Map approved by the Western Australian Planning Commission on 28 September 2016 to the satisfaction of the City.
6. Prior to the occupation or use of any dwelling, landscaping is to be installed in accordance with the Landscape Proposal Plan (Rev: A). Landscaping is to be maintained thereafter to the satisfaction of the City.

Mr Charles Johnson
Presiding Member, Metro Central JDAP



7. Prior to the submission of an application for building permit, the land owner is to pay the City for the cost of the:
 - a. Removal of three street trees required to enable construction of new crossovers to Woodthorpe Drive and Portcullis Drive; and
 - b. Provision and installation of three replacement street trees in an alternative location on the verge abutting the subject property.
8. Prior to the occupation or use of any dwelling, detailed design plans and documents for the development of the public open space as depicted on the Proposed POS Landscape Plan (ref: 15005 SK02, rev: I) and providing for a high standard of design are to be submitted and approved to the satisfaction of the City.
9. Prior to the occupation or use of any dwelling, works to develop the public open space are to be completed by the developer at their cost in accordance with the approved detailed design plans and documentation to the satisfaction of the City.
10. Prior to the occupation or use of any dwelling, arrangements being made for the landscaping of the public open space area to be maintained by the developer at their cost for two summers from the time of completion of the first dwelling. The handover of the maintenance of the landscaping to the local authority is to be agreed by the local authority in writing dependent that all species being established and healthy.
11. Prior to the occupation or use of any dwelling, works to allow for a mainline connection and cabling connection to the existing bore on Lot 34 on Plan 16868 to provide for irrigation of the public open space area is to be completed by the developer at their cost and to the satisfaction of the City.
12. Prior to the submission of an application for building permit, a lighting plan indicating lighting to the pathways, communal open space, pedestrian and vehicle entry points, right of way and car parking areas is to be submitted to and approved by the City. Prior to the occupation of the development, the approved lighting is to be installed and thereafter maintained by and at the cost of the owner/developer to the satisfaction of the City.
13. Prior to the commencement of any site or construction works, a Construction Management Plan and Traffic Management Plan in accordance with the requirements of Australian Standard AS1742 Pt 3 are to be submitted by the owner/developer to the satisfaction of the City. The approved plan is to be thereafter implemented for the duration of the construction of the development.
14. Prior to the commencement of any site or construction works, detailed engineering plans and specifications for the private road network within the common property ('Private Road Works') are to be submitted by and at the cost of the owner/developer to the satisfaction of the City.

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15. Prior to the occupation or use of any dwelling, the approved Private Road Works within the common property are to be implemented by and at the cost of the owner/developer to the satisfaction of the City.
16. Prior to the commencement of any site or construction works, detailed engineering plans and specifications for all works within the Woodthorpe Drive and Portcullis Drive road reserves ('Road Works') are to be submitted by and at the cost of the owner/developer to the satisfaction of the City, including:
 - a. The construction of a Cul-de-sac bulb to Woodthorpe Drive;
 - b. The construction of a suitable intersection treatment/crossover between the proposed development and Woodthorpe Drive;
 - c. The construction of a suitable intersection treatment/crossover between the proposed development and Portcullis Drive; and
 - d. The portion of the Woodthorpe Drive surface road base to be removed.
17. Prior to the occupation or use of any dwelling, the approved Road Works within the Woodthorpe Drive and Portcullis Drive road reserves are to be implemented by and at the cost of the owner/developer to the satisfaction of the City.
18. Prior to submission of an application for building permit, a report that meets the geotechnical requirements of the City's Earthworks Policy (ET510) is to be submitted by and at the cost of the owner/developer to the satisfaction of the City.
19. Prior to commencement of construction works, a post earthworks report certifying that the works recommended by the geotechnical report required in Condition 18 have been completed is to be submitted by and at the cost of the owner/developer to the satisfaction of the City.
20. Prior to the commencement of any site or construction works, a stormwater management plan is to be submitted to the satisfaction of the City. The approved stormwater management system is to be implemented and thereafter maintained to the satisfaction of the City.
21. Prior to the occupation or use of the development, the owner/developer is pay to the City a drainage headworks contribution as per clause 29 of the City of Canning Town Planning Scheme No. 24.
22. Prior to the occupation or use of the development, a Waste Management Plan (WMP) is to be submitted to the satisfaction of the City. The approved WMP is to be implemented and thereafter maintained for the duration of the development to the satisfaction of the City.
23. Prior to occupation or use of the development, an easement in gross benefiting the City, must be registered on the strata plan. The deed of easement is to be drafted by the City at the applicants cost, and is to:
 - a. grant the City and its employees, contractors and invitees unobstructed access to those common areas on the strata plan reasonably necessary

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- for the City to access for the purpose of collecting waste, together with any vehicles and equipment reasonably required.
- b. indemnify the City against any damage to any property caused by or attributed to any vehicles or equipment used by the City, its employees, contractors and invitees when collecting waste (excluding any damage directly caused by grossly negligent acts or omissions of the City).
 - c. Require the grantor maintain those common areas in a sound state of repair, and abide with any waste management plan.
24. Prior to the occupation or use of any dwelling, 13 visitor car parking bays are to be permanently marked, sign posted and thereafter maintained at all times for use by residential visitors to the dwellings and public open space area.
 25. Prior to occupation or use of the Sales Office, car parking bays for the exclusive use of visitors and staff of the premises are to be constructed, line marked thereafter maintained for the duration that of the operation of the premises to the satisfaction of the City.
 26. All clothes drying devices and clothes drying areas are to be located and positioned so as not to be visible from the street or public place.
 27. No services, such as air conditioners or water heaters, are to be visible from the street or public place.
 28. Prior to occupation or use of any dwelling, the developer shall implement noise attenuation measures as per the recommendations detailed in the Traffic Noise Assessment by Lloyd George Acoustics dated 3 August 2016 and the noise criteria outlined in State Planning Policy 5.4 – Road and Rail Transport Noise and Freight Considerations to the satisfaction of the City.

Advice Note(s)

- a. Prior to obtaining subdivision or strata title approval for the proposed development the subject of this application, the applicant/owner is to comply with all requirements of the City of Canning Town Planning Scheme No. 24 inclusive of payment of the following costs in accordance with 45 and 49 of the Scheme respectively:
 - i. Local Centre Contribution – 0.8% of the value of the land; and
 - ii. Scheme Costs - \$3,363.20 per hectare (pro rata).
- b. Crossover(s) to interfacing industrial/commercial properties affected by the Woodthorpe Drive road carriageway modifications must be constructed in accordance with the City's "Industrial/Commercial Crossover Standard Details" refer to Plan No. STD 02. The City advises that:
 - iii. Prior to construction of all crossovers and internal driveways, levels at the property boundary must be obtained from the City's Customer Services Officer - Construction on 9231 0654. The City sets a +2% grade from the top of the fully mountable kerb to the property boundary and any variation must be approved by the City's Engineering Services in writing prior to the commencement of any construction. Concrete crossovers must be

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- inspected prior to pouring concrete. All crossovers must be inspected on completion of construction. Contact the Customer Services Officer - Construction on the above number to arrange for an inspection.
- iv. Industrial/Commercial crossover construction material must be bitumen to match existing.
 - v. The City's Engineering Services advise that the minimum and maximum width of industrial/commercial crossovers is four point five (4.5) and eleven (11) metres respectively.
 - vi. Industrial crossover as shown on the plan will require consultation with the impacted owners (including Lot 28 Woodthorpe Drive) with final approval of crossover configuration is to be to the satisfaction of the City Engineering Services department.
- c. In relation to the Construction Management Plan and Traffic Management Plan, the owner/developer is to include information such as: the route that construction vehicles will take to and from the site, the temporary realignment of pedestrian access ways, vehicular access to the site during construction, unloading and loading areas, waste disposal, the location on site of building materials to be stored, safety and security fencing, sanitary facilities, cranes and other details as required by the City.
- d. In relation to works within the Woodthorpe Drive and Portcullis Drive road reserves, the owner/developer is advised to liaise with the City's Engineering Services.
- e. The portion of Woodthorpe Drive Road Carriageway and Drainage which is not used as part of the development must be removed. All kerbs are to be reinstated in accordance with the City's "Extruded Kerbing Standard Details," refer to Plan No C1630, and verges are to be reinstated to match the levels of the existing verge and to be left free of builders rubble.
- f. In relation to the stormwater drainage, the owner/developer is advised that:
- i. Any drainage works within the road reserve must to be approved by the City's Engineering Services.
 - ii. All internal driveways/roadways are to be graded and drained such that the storm water runoff does not flow across the crossover. Where it is not possible to grade the internal driveway as above it are to be necessary to construct suitably designed trench grates across the driveways at the property boundary.
- g. The development is to comply at all times with the Environmental Protection (Noise) Regulations 1997. In this regard, air conditioning systems, carpark ventilation systems, and other plant and equipment that may emit excessive noise are to be designed and located so that noise emissions, when received at neighbouring lots are compliant with the Regulations. To ensure compliance the City recommends the applicant engage the services of an accredited acoustic consultant to provide further advice with regard to the final placement of all plant and equipment and compound construction. The applicant may also refer to the City's guideline titled "An installer's guide to Air Conditioner Noise" to assist in assessing the appropriate air conditioner location. The Guideline is available on the City's website – the link is as follows:

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http://www.canning.wa.gov.au/images/stories/installers_guide_to_air_conditioner_noise.pdf

- h. It is recommended that the developer contact Western Power's Networks Customer Services to register the proposed project, arrange for the removal/relocation of the power pole that conflicts with the eastern access from proposed common property Road 1 to Woodthorpe Drive, and obtain relevant forms. These forms must be completed and submitted at least four weeks prior to work commencing.
- i. With regard to the removal of three street trees, all other street trees located in the verge areas adjoining the subject site are to be retained and protected. Pruning or removal of the tree(s) must be approved in writing by the Manager, Parks and Environment.
- j. This approval does not authorise the commencement of any building works. The applicant is advised that a building permit must be obtained prior to the commencement of any works. To obtain a building permit it will be necessary to submit documentation in compliance with the Building Regulations, including plans incorporating all conditions of this approval, specifications and structural drawings.
- k. If the development the subject of this approval is not substantially commenced within a period of 2 years, the approval will lapse and be of no further effect.
- l. Where an approval has so lapsed, no development must be carried out without the further approval of the local government having first being sought and obtained
- m. If an applicant or owner is aggrieved by this determination there is a right of review by the State Administrative Tribunal in accordance with the Planning and Development Act 2005. An application must be made within 28 days of the determination.

REASON: In accordance with details contained in the Responsible Authority Report.

The Report Recommendation/Primary Motion was put and CARRIED UNANIMOUSLY.

9. Form 2 – Responsible Authority Reports - Amending or cancelling DAP development approval

Nil.

10. Appeals to the State Administrative Tribunal

The following State Administrative Tribunal Application has been received:

- City of Melville - Proposed Shop (Woolworths Supermarket, Pharmacy and Specialty) and Medical Centre - Lots 368 (855) and 369 (857)

Mr Charles Johnson
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Canning Highway, Applecross and Lots 3 (37) and 372 (41) Reynolds Road, Mount Pleasant

As invited by the State Administrative Tribunal under Section 31 of the *State Administrative Act 2004*, the Metro Central JDAP reconsidered Lots 1 (1009), 2 and 3 (1013-1015) Albany Highway, St James on the 27 April 2017.

11. General Business / Meeting Close

The Presiding Member reminded the meeting that in accordance with Standing Order 7.3 only the Presiding Member may publicly comment on the operations or determinations of a DAP and other DAP members should not be approached to make comment.

There being no further business, the presiding member declared the meeting closed at 3.05pm.

A handwritten signature in black ink, appearing to read 'C Johnson'.

Mr Charles Johnson
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Form 1 - Responsible Authority Report
(Regulation 12)

Property Location:	Lot 25 (No. 300) Collier Road, Bassendean
Development Description:	Proposed Convenience Store
DAP Name:	Metropolitan Central JDAP
Applicant:	Planning Solutions
Owner:	HICON (WA) PTY LTD
Value of Development:	\$2.1 million
LG Reference:	DABC/BDVAPPS/2017-033
Responsible Authority:	Town of Bassendean
Authorising Officer:	Dylan Stokes, Planning Officer
Department of Planning File No:	DAP/17/01187
Report Due Date:	18 May 2017
Application Receipt Date:	10 March 2017
Application Process Days:	62 days
Attachment(s):	<ol style="list-style-type: none"> 1. Applicant's (Planning Solutions) submission incorporating a report and plans of the proposed development. 2. Letter provided on 21 February 2017 detailing amendments to plans prior to lodgement. 3. Applicant's correspondence dated 11 April 2017. 4. Correspondence on behalf of the Department of Planning dated 10 April 2017. 5. Town of Bassendean Local Planning policies: <ol style="list-style-type: none"> i. No. 6 – Industrial Zones Development Guidelines; ii. No. 15 – Percent for Art Policy; iii. No. 16 – Control of Advertisements under the Local Planning Scheme No. 10; and iv. No. 18 – Landscaping with Local Plants.

Officer Recommendation:

That the Metropolitan Central JDAP resolves to:

1. **Approve** DAP Application reference DAP/17/01187 and accompanying plans:

Dwg No.	Drawing Name	Rev No.	Dwg Date
1616-TP1	Locality Plan and Survey Plan	C	13.02.17

1616-TP2	Layout Plans	D	13.02.17
1616-TP3	Elevations	B	19.12.16
1616-TP4	Landscape Plan and Signage Details	D	13.02.17
1616-TP5	Truck Path	C	13.02.17
N/A	Overall Site Plan	N/A	21.02.17
1616-TP5 (with recommendations in red)	Figure 17 – Recommended changes to kerbs for fuel tanker access	C	13.02.17

Pursuant to Clause 68 Schedule 2 of the Planning and Development (Local Planning Scheme) Regulations 2015 subject to the following conditions:

1. This approval is for the use of the building as a convenience store only. Any alternative use of the premises will require the submission of an application to Council for a change of use.
2. Lot A as shown on the overall site plan is to be a minimum of 1,828m² and shall not decrease as a result of a re-survey of the subject site associated with the proposed subdivision.
3. Revised drawings shall be submitted in conjunction with the application for a building permit and such drawings shall demonstrate:
 - a. An amended top of retaining wall height of 22.6; and
 - b. A reduced height to S1 – High Main Price Board to a maximum height of 6m.
 - c. Straightening of the kerb on the western side of the site to increase the width of the proposed crossover as shown in red on drawing titled Figure 17. The crossover wing radius must be a minimum of 5.0m in accordance with the Towns crossover specifications.
 - d. Fencing within 11.5m from the Collier Road lot boundary to be shown as either open mesh or palisade fencing.
 - e. The bin area is to be masonry in construction and equipped with a hose cock. The floor must be graded to a floor waste and connected to sewer.
4. A detailed and professionally prepared landscaping plan being submitted prior to or with the application for a Building Permit for the Town's approval which provides full detail of the scope of works to be undertaken in both the private and public realms adjoining the development site, including:
 - a. All the requirements listed under LPP18 including but not limited to: street frontages, contours, reticulation details, details of ground treatment and a plant legend showing:
 - i. quantity of plants,
 - ii. species name
 - iii. pot size of plants at the time of planting;
 - iv. height at full growth

- b. Details of the location and type of proposed trees, shrubs, ground cover and lawn areas to be planted;
 - c. All plants to be of low water use;
 - d. Landscaping of the verge area adjacent to the development site, including the provision of 4 substantial street trees (2 x *Corymbia Ficifolia* to Collier Road and 2 x *Melaleuca Quinquenervia* to Alice Street) of a minimum 90L pot size in accordance with the Town's adopted Street Tree Master Plan;
 - e. The total number of plants to be planted at a minimum rate of 4 per 1m²; and
 - f. Details of the proposed watering system to ensure the establishment of species and their survival during the hot, dry summer months.
5. Landscaping shall be installed and maintained in accordance with a landscaping plan, submitted to and approved by the Town of Bassendean prior to the practical completion of the development.
 6. No vehicle used for the delivery of fuel is permitted on site between 7:00am to 9:00am and 3:00pm to 7:00pm on weekdays.
 7. No products, goods or materials are to be stored outside of the building, unless in a designated area approved by Council for this purpose (Manager of Development Services).
 8. The applicant is responsible for all costs and works associated with extending the kerbed median island within the Collier Road road reserve. The works are to be completed to the satisfaction of the Western Australian Planning Commission and the Town of Bassendean prior to the occupation of the convenience store.
 9. All storm water being contained on site. Details of the method of storm water being submitted for approval in conjunction with the application for a Building Permit.
 10. The car parking spaces and access ways to be designed and constructed in accordance with Local Planning Policy No. 8 and AS 2890.1.
 11. The car parking spaces and access ways being constructed and maintained thereafter to Council's satisfaction.
 12. All building works carried out under this planning approval are required to be contained within the boundaries of the subject lot.
 13. Prior to the issue of a building permit, a development bond for the sum of \$42,000 being lodged with Council to ensure the satisfactory completion of all works associated with landscaping, car parking, access ways, screen walls, and other associated works.
 14. The incorporation of public art into the proposed development or a cash-in-lieu payment of one per cent of the construction cost of the proposed development in accordance with the Town's adopted Local Planning Policy No. 15 'Percent for Art Policy'. Detailed arrangements and agreement with respect to art to be provided on-site with a bond being provided to the Town

for the full cost of the art or alternatively payment of the required fee shall be made prior to or in conjunction with the application for a Building Permit.

15. The building hereby approved shall not be occupied until all of the conditions of development approval have been complied with to the satisfaction of the Manager Development Services, unless the applicant has entered into an agreement with Council to comply with those conditions within a specified period.

Advice Notes

1. The issue of a Building Permit is required prior to the commencement of any on-site works.
2. Separate approval is required from the Town's Asset Services for the proposed crossovers to both Collier Road and Alice Street and these crossovers being constructed in accordance with that approval.
3. The Town of Bassendean encourages the retention of stormwater on-site through various best management practices, as laid out in its Planning Policy. Details of the stormwater containment and disposal method are to be provided with the building licence application.
4. Regarding the proposed modifications to the median island and the crossover to Collier Road, the applicant is to ensure that all necessary approvals or otherwise have been provided by the Western Australian Planning Commission.
5. The applicant is advised that as part of the subdivision clearance of the site, an easement is required to be shown on the Certificate of Title for the purpose of vehicle access as shown on the approved plans.
6. The applicant is advised that they must submit an Application for Approval to Establish or Alter a Food Business, together with the required plans and prescribed fee to the Health Department at the Town of Bassendean. The plans must include details such as :
 - a. Finishes of all walls, floors and ceilings;
 - b. Position and type of fixtures and fittings; and
 - c. Location of sanitary conveniences, ventilating systems, grease trap and bin storage.
7. The applicant is advised to obtain approval from the Department of Mines and Petroleum for the storage of Chemicals/dangerous goods in accordance with the Dangerous Goods Safety Regulations 2007 and Dangerous Goods Safety Act 2004.
8. All fuels, oils and other liquids shall be appropriately stored within a bunded and covered area capable of trapping any discharges. The application is to comply with the requirements of the Environmental Protection (Unauthorised Discharges) Regulations 2004. The applicant shall give consideration to the proximity/location of on-site storm water drains and soak wells to areas where spillage of fuel may occur.

9. Any existing on-site effluent disposal system (septic tanks and leach drains/soak wells on the site shall be decommissioned in accordance with the Health (Treatment of Sewage and Disposal of Effluent and Liquid Waste) Regulations 1974.
10. The applicant is advised that any connection to sewer requires approval from the Water Corporation. Commercial and industrial premises may be required to obtain an industrial waste permit from the Water Corporation, who may also further require, the installation of specific infrastructure such as petrol and oil separators or other devices, or that the liquid waste be treated to a minimum standard prior to discharge.
11. The applicant is advised that noise generated by activities on site shall not exceed the levels as set under the Environmental Protection (Noise) Regulations 1997. All development works are to be carried out in accordance with control of noise practices set out in Section 6 of AS 2436-1981 or the equivalent current Australian Standard. No works shall commence prior to 7am.

Details: outline of development application

Insert Zoning	MRS:	Industrial
	TPS:	General Industrial
Insert Use Class:		Convenience Store
Insert Strategy Policy:		Not applicable
Insert Development Scheme:		Town of Bassendean Local Planning Scheme No. 10 (District Zoning Scheme)
Insert Lot Size:		1828m2 (proposed as part of subdivision)
Insert Existing Land Use:		Factory and Office

The application proposes the development of a 7-Eleven 'Convenience Store' in a general industrial area and includes:

- A 193m² retail building;
- 4 petrol bowsers (8 bowser bays) under a fuel canopy;
- Nine (9) dedicated parking bays for staff and customers;
- Signage associated with the convenience store;
- Landscaping;
- A service bay providing air and water facilities for customers;
- A loading bay for delivery vehicles;
- A proposed bin store;
- A proposed crossover to Collier Road;
- Upgrading of an existing crossover to Alice Street; and
- An extension of an existing kerbed median island in Collier Road to create left in – left out only access from the Collier Road frontage of the development site.

The convenience store will operate 24 hours a day, 7 days a week, will employ 2 staff members on-site at any given time and offer the retail sale of fuel and convenience goods.

Background:

Lot 25 (No. 300) Collier Road is currently occupied by an existing Factory and Office which was originally constructed in 1978. Following the construction of the Factory,

subsequent minor building approvals were issued for additional development as chronicled below:

- 1985 – Proposed Office Extension
- 1985 – Storage Extension
- 1989 – Extensions to the Existing Factory

The applicant currently has a subdivision application that is pending with the Western Australian Planning Commission to subdivide Lot 25 Collier Road. The proposed subdivision is looking to split the existing industrial lot, retaining the existing factory use on the western portion of the site and redeveloping the parking and storage area to the east of the site for the proposed convenience store.

The existing lot is of a size 14,144m² according to the certificate of title, with the two proposed lots comprising 1828m² for the convenience store and 12,316m² for the existing factory use. It should be noted that the lot size differs to the registered lot size at Landgate which identifies the lot as being 14103m². As part of the subdivision process, the lot will be re-surveyed to determine the correct lot size. The Town will request a condition be imposed to ensure that the 1828m² is retained for the subject site.

Lot 25 Collier Road is zoned general industry under the Local Planning Scheme No. 10. The subject lot is surrounded with general industry zoning, with fringe light industry uses located to the north and east approximately 300m to 400m and the closest residential lot located to the northeast approximately 550m away.

The subject site is a corner lot that fronts Collier Road to the north and Alice Street to the east. Collier Road is a dual carriageway that is a Category 2 Blue Road under the Metropolitan Regional Scheme and a District Distributor A Road under the Main Roads Road Hierarchy for Western Australia. Collier Road currently carries a weekday traffic flow of approximately 17,200 vehicles. Alice Street is a single carriageway that terminates at a junction with Collier Road. There are no plans to upgrade the junction of Collier Road and Alice Street as part of the NorthLink upgrades proposed by Main Roads.

Legislation & policy:

Legislation

- a) Planning & Development Act 2005
- b) Planning and Development (Local Planning Scheme) Regulations 2015
- c) Metropolitan Region Scheme

State Government Policies

- a) State Planning Policy 4.1 – State Industrial Buffer Policy
- b) Development Control Policy 5.1 – Regional Roads (Vehicular Access)

Local Policies

- a) Town of Bassendean Local Planning Scheme No. 10 including:
 - Clause 1.6: The Aims of the Scheme;
 - Clause 4.2.4: Objectives of the General Industry Zone;
 - Clause 5.7.2: Car parking

- Clause 9.4: Advertising of Applications
- Clause 10.1: Consultation with Other Authorities
- Clause 10.2: Matters to be Considered by Local Government.

Local Planning Scheme No. 10 can be accessed via the following link:
http://www.bassendean.wa.gov.au/Profiles/bassendean/Assets/ClientData/Document-Centre/Local_Planning_Scheme_10_-_Gazetted/10-Lps-No10-gazetted.pdf

b) Local Planning Policies:

- Local Planning Policy No. 6 – Industrial Zones Development Guidelines
- Local Planning Policy No. 15 – Percent for Art Policy
- Local Planning Policy No. 16 – Control of Advertisements under the Local Planning Scheme No. 10
- Local Planning Policy No. 18 – Landscaping with Local Plants

c) Specification for the Construction of Crossovers

Consultation:

Public Consultation

Under the Zoning Table (Table 1) of the Town's Local Planning Scheme, a convenience store is a 'D' or discretionary use in the General Industry Zone, which did not generate a need for public advertising of the application.

Consultation with other Agencies or Consultants

The application was referred to the following agency for comment:

- Department of Planning – Other Regional Road Reservation (Policy No. DC 5.1)

The proposal for a convenience store incorporates a new crossover to Collier Road. Under Clause 3.3.1 and 3.3.2 of State Planning Policy 5.1, the Commission seeks to minimise the creation of new access arrangements to regional roads and instead aims to seek access from side streets. The Town therefore referred the application to the Commission for consideration.

A response was provided by the Commission that detailed the following:

- Proposed additional access onto Collier Road;
- Analysis of the Transcore traffic report and the increased traffic volume;
- Recommendations relating to road junction upgrades and bicycle parking;
- Analysis of the proposed signage; and
- Analysis of the fuel tanker access and swept paths.

The department indicated support for the application and the proposed additional access onto Collier Road in addition to the proposed signage abutting the lot boundary. Recommendations were made on behalf of the Commission that will be detailed in the Officer Comment section of the report.

Planning assessment:

Local Planning Scheme

- **Zoning, Classification of Use and Permissibility of Use**

The subject land is zoned 'Industrial' under the Metropolitan Region Scheme and 'General Industry' under the Town of Bassendean Local Planning Scheme No. 10 (LPS 10). In accordance with Table 1 under Clause 4.3 of LPS 10, the proposed convenience store is a 'D' use. Clause 4.3.2 of LPS 10 states that a 'D' use is 'not permitted unless the local government has exercised its discretion by granting planning approval'.

Under the LPS 10, there is a separate use class for Service Station that is an 'A' use and would require advertising. The definition of a convenience store is defined below:

"convenience store means premises –

- (a) used for the retail sale of convenience goods commonly sold in supermarkets, delicatessens or newsagents, or the retail sale of petrol and those convenience goods;
- (b) operated during hours which include, but may extend beyond, normal trading hours;
- (c) which provide associated parking; and
- (d) the floor area of which does not exceed 300 square metres net lettable area"

The application is deemed to meet the definition in regards to the following:

- Goods to be sold of a 'retail/convenience nature' in addition to the sale of petrol as detailed in the applicants report;
- The application will operate in addition to normal trading hours, being a 24/7 operation, 7 days a week;
- Parking is provided on site for both customers and staff; and
- Floor area for net lettable area of the convenience store is 193m².

On this basis, the application is considered to meet all aspects of the convenience store definition.

- **Car parking**

Clause 5.7.2.1 of LPS 10 requires that for any use indicated in Table 1 of LPS 10 that sufficient parking bays are provided under Table 2. Despite a convenience store being listed as a use under Table 1, there is no corresponding car parking requirement under Table 2. In the event that no parking requirement is specified, LPS 10 goes on to state that "*the local government shall determine the number of car parking spaces to be provided on the land having regard to the nature of the proposed development, the number of employees likely to be on the site, the prevention of the obstruction of roads and street, and the orderly and proper planning of the locality and the preservation of its amenities.*"

The proposal incorporates the following in regards to parking

- 7 bays (including 1 disabled bay) adjoining the front of the convenience store;
- 2 bays to the rear of the site for staff parking (with 2 employees on site at any given time as specified in the report);
- 8 bays for fuelling purposes;
- 1 bay for "Air and Water" facilities.

The report provided by Planning Solutions referenced a Roads and Traffic Authority (NSW) report titled "*Guide to Traffic Generating Developments*" which references 5 bays being required per 100m². With a net lettable area of 193m², a total of 9.65 (rounded to 10) bays would be required. Combining the designated parking spaces with the staff bays would result in a shortfall of a single bay. If the fuel bays were also included, there would be a surplus of bays.

- **Bicycle Facilities**

Clause 5.7.6 of LPS 10 states that "the local government may require the provision of facilities that provide for and encourage cycling as part of any private developments. Such facilities shall provide for storage and parking of bicycles and change rooms/showers for cyclists." The application does not incorporate any detail relating to bicycle facilities.



Local Planning Policies

- **Local Planning Policy 6 – Industrial Zones Development Design Guidelines**

Item	Requirement	Proposal	Compliance
Building Setbacks	<p>Minimum setback requirements for buildings shall be in accordance with the following:</p> <p>Front: 13.0m Rear: Nil Side: 4.5m Secondary Street: 6.0m</p>	<p>Front: Approximately 8.6m to roof of fuel canopy Approximately 12m to fuel bowser</p> <p>Rear: Nil at the closest point to easement</p> <p>Side: Approximately 3.0m to roof at closest point. Approximately 4.5m to convenience store.</p> <p>Secondary Street: 6.0m to roof at closest point.</p>	<p>Variation to front and side setbacks. Further comment in relation to this matter is provided with the Officer Comment section of the report.</p>
Plot Ratio	Maximum of 0.75	$193\text{m}^2/1828\text{m}^2 = 0.11$	Complies
Site Cover	Maximum of 50%	$492\text{m}^2/1828\text{m}^2 = 27\%$	Complies
Vehicle Parking	<p>Car parking spaces shall be provided, constructed and maintained in accordance with the provisions of Part 5 of the Scheme, Local Planning Policy No. 8 (Parking Specifications) and the approved plan relating thereto.</p> <p>Local Planning Policy 8 Requirements: Width of Bays: 2.5m (Note: Australian Standards 2890.1 calls for 2.6m width bays</p>	<p>Bay Width: 2.6m Depth: 5.4m Aisle Width: Approximately 9m</p> <p>Single Disabled Bay provided in accordance with specifications under the Australian Standards 2890.6:2009.</p> <p>No detail provided of materials for paving. Condition recommended to ensure paving</p>	Complies (with condition discussed in officer comments)

	for this type of development) Depth of Bay: 5.4m Aisle Width: 6.2m	is consistent with policy material type.	
Landscaping	All development within these zones shall be landscaped in accordance with the following requirements: a) Minimum width of landscaping to front setback of 2m. b) Minimum width of landscaping to secondary street of 1m. c) Landscaping to be provided in accordance with Local Planning Policy 18 d) Shade trees provided in parking areas at a ratio of one per six car parking bays.	a) Landscaping in front setback of 1.65m b) Landscaping in secondary street of 1.3m c) Landscaping plan requested by way of condition to meet this policy. d) Two shade trees provided for visitor/staff parking.	Variation proposed to landscaping in front street setback and condition required for a finalised landscaping plan discussed in officer comments.
Security Fencing	Where a security fence is proposed on a street lot boundary, it shall be of an open style either mesh fencing or palisade style fencing and setback 2-metres behind the street alignment. Solid screen fences on industrial sites are only acceptable where located at or behind the 13-metre building line to the front setback area. In the case of corner lots solid screen fencing must be setback 6-metres from the secondary street.	No fencing proposed adjoining the primary or secondary street frontages. Solid boundary fencing has been proposed between the subject site and the lot to west within the 13m front setback area.	Variation proposed to solid fencing in the front setback area. Further comment in relation to this matter is provided with the Officer Comment section of the report.
Service Access	Provision shall be made for service access in the following manner:	a) Vehicle access and loading bay provided on the western side of the	Complies (conditions recommended under

	<p>a) a paved accessway shall be provided for vehicles from the street to the rear of and to any other part of the building where provision is made in the external walls of the building for the entry of or the loading or unloading of vehicles;</p> <p>b) the access way shall be so constructed that all vehicles using it can enter from and return to a street in forward gear without reversing any part of the vehicle onto a street;</p> <p>c) except as hereinafter mentioned, the access way shall not be less than 4.5 metres in width, unless the size of the lot makes the provision of such an access way impracticable or unreasonable the Council may permit an access way of a narrower width but in no case less than 3.0 metres in width;</p> <p>d) the provision of on-site delivery manoeuvring areas where large single axle commercial vehicles can enter and leave the site in forward gear. The manoeuvring area shall be exclusive of car parking bays and the interior of the building.</p>	<p>lot. Delivery vehicles can pass alongside filling bay No. 8 but would be obstructed if the air/water bay were in use.</p> <p>b) Delivery vehicle can enter from Collier Road and exit from Alice Street.</p> <p>c) Access way of 3.5m in width. The service bay itself has a width of 3.5m.</p> <p>d) Manoeuvring of a 19m fuelling truck is shown on plans entering from Alice and exiting from Collier Road. Fuelling location is located under one of the proposed fuelling bays.</p>	<p>officers comment)</p>
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Storage and Refuse Areas	<p>The Council may require the provision of one or more areas for the storage of refuse in a development. This area shall be:</p> <ul style="list-style-type: none"> a) screened from view from any public street, and enclosed by a wall of masonry or other approved building material, and being of not less than 1.8 metres in height; and b) accessible to service vehicles. <p>Irrespective of whether a storage area is required for a development, no land shall be used for open storage purposes unless it is screened from view of any public street by a fence or wall built to the specifications and satisfaction of the Council.</p>	<ul style="list-style-type: none"> a) A refuse area has been provided of approximately 3mx5m for bin storage enclosed to a height of 2.2m as stated within the report. No details provided of the material that the storage area will be constructed from. b) Access from a delivery and service vehicle to a length of 8.8m has been shown on the plans. Access 	Complies (conditions recommended under officers comment)
Building Materials	<p>The facades of all buildings facing public streets shall be constructed of brick, glass, stone, masonry or concrete in respect of the ground floor level. The second floor level, or its equivalent, may be constructed of other material in accordance with the Building Code of Australia.</p> <p>The use of zincalume sheeting for construction of Industrial buildings is prohibited, unless such buildings or parts of buildings are not visible from any public street, or Council approval is granted to vary this</p>	The report states that concrete panels will be used as part of construction and the plans show glass to the front of the convenience store building façade.	Complies

requirement.		
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- **Local Planning Policy 16 – Control of Advertisements under the Local Planning Scheme No. 10**

Item	Requirement	Proposal	Compliance
Pylon Signs	Max height of device: 6m Max width of device: 2m Max area: 4m ² Minimum headroom: 2.4m Max height above ground: 6m Projection maximum: 900mm Setbacks front: 1m Setbacks side: 2m Other setbacks: 6m Location: Minimum 6m clearance of another sign.	Sign 1 (S1) – Pylon Sign (High Main Price Board) Max height of device: 8m Max width of device: 2m Max area: 16m ² Minimum headroom: N/A Max height above ground: 8m Projection maximum: 0.55m Setbacks front: Approximately 200mm Setbacks side: Approximately 200mm Other setbacks: >6m Location: >6m separation distance Sign 2 (S2) – 3 x Pylon Sign (Directional Signage) Max height of device: 1.55m Max width of device: 0.55m Max area: 0.9m ² Minimum headroom: N/A Max height above ground: 1.55m Projection maximum: N/A Setbacks front: Approximately 200mm for northernmost signs	Variations to S1: - Max height - Max area - Max height above ground - Front setback - Side setback Variation to S2: - Front setback - Side setback Variation to S3: - Side setback Variation to S8: - Signs located within 6m.

		<p>Setbacks side: Approximately 200mm for easternmost and westernmost sign. Other setbacks: >6m Location: >6m separation distance</p> <p>Sign 3 (S3) – 1 x Pylon Sign (Air and Water Sign)</p> <p>Max height of device: 0.45m Max width of device: 0.3m Max area: 0.14m² Minimum headroom: N/A Max height above ground: <6m Projection maximum: N/A Setbacks front: >2m Setbacks side: Approximately 500mm Other setbacks: >6m Location: >6m separation distance</p> <p>Sign 8 (S8) – 2 x Pylon Sign (Advertising Sign)</p> <p>Max height of device: 2.8m Max width of device: 0.8m Max area: 1.9m² Minimum headroom: N/A Max height above ground: 2.4m Projection maximum: N/A Setbacks front: >2m Setbacks side: >1m Other setbacks: >6m Location: Separated by approximately</p>	
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		2.2m	
Roof Sign	Height of main building above ground level: <ul style="list-style-type: none"> - 3.7m to 4.5m requires a maximum height of 0.9m for the sign. - 4.5m to 6.0m requires a maximum height of 1.2m for the sign. 	Sign 4 (S4) – Roof Sign (On Convenience Store) Height of Roof: 3.7m to 4.5m Height of Sign: 1.4m Sign 5 (S5) – 4 x Roof Sign (On Fuel Canopy) Height of Roof: 5.5m Height of Sign: 0.9m	Variation to S4 <ul style="list-style-type: none"> - Height of sign Sign S5 complies.
Tower Sign	Max Height of Sign: 20% of mast, tower of chimney Max length/width of sign: Width of mast, tower, chimney, or structure where sign affixed.	Sign 6 (S6) – 2 x Tower Sign Size of tower: 24m ² Size of sign: 3.3m ² (13.5%)	Sign S6 complies.
Animated Sign	Max height of device: 1.5m Max width of device: 1m Max area: 3m ² Minimum headroom: 2.4m Max height above ground: <6m Setbacks front: >2m Setbacks side: >1m Other setbacks: >6m	Sign 9 (S9) – Animated Sign Max height of device: 1.7m Max width of device: 1.2m Max area: 2m ² Minimum headroom: N/A Max height above ground: 2.4m Setbacks front: >2m Setbacks side: >1m Other setbacks: >6m Location: Separated by approximately 2.2m	Variation to S9 <ul style="list-style-type: none"> - Height of sign.



- **Local Planning Policy 15 – Percent for Art Policy**

LPP 15 details requirements relating to contribution towards the Town for public art either on the subject site or on crown land. The policy applies throughout the entirety of Bassendean for *“all development proposals for multiple dwellings, mixed use, commercial, civic, institutional, educational projects or public works with a value greater than \$1,000,000 shall be regarded as eligible proposals under this Policy.”*

The value of the contribution must be provided at 1% the value of the eligible proposal. For the proposed \$2,100,000 convenience store this would equate to a contribution of \$21,000. The applicant can elect to either pay the contribution directly to the Town or instead provide the public art on site at the applicant’s expense.

- **Specification for the Construction of Crossovers**

The Town of Bassendean’s Specification for the Construction of Crossovers details requirements to ensure the adequate construction of Crossovers and access throughout the Town of Bassendean. The document incorporates a table that specifies maximum crossover sizes for industrial properties. The table specifies a maximum width of the crossover at the property boundary as being 10.7m. Due to the easement to the south of the subject site providing shared vehicular access and additional turning space for the large trucks entering the site, the width of the combined crossover is 12.5m at the property line.

State Planning Legislation

- **State Planning Policy 4.1 – State Industrial Buffer Policy**

State Planning Policy 4.1 – State Industrial Buffer Policy (SPP 4.1) identifies the need for land uses commonly associated with off-site amenity impacts (noise, dust, odour, risk and particulate emissions) to be separated from sensitive land uses to ensure acceptable environmental criteria can be achieved at nearby sensitive receivers.

The Environmental Protection Authority’s - Guidance for the Assessment of Environment Factors - Separation Distances between Industrial and Sensitive Land Uses compliments, and provides guidance on, the implementation of SPP 4.1. As outlined in this framework, there is no direct guidance specifically regarding convenience stores and required separation distances. Regarding 24-hour service stations it is recommended that a minimum separation distance of 200m be provided from sensitive land uses in order to mitigate impacts of gas emissions, noise, odour and risk. The subject site is surrounded by general industrial and light industrial land uses, with the nearest residential lot approximately 550m from the subject site. On this basis, the application is considered a sufficient distance from adjoining sensitive land uses.

- **Development Control Policy 5.1 – Regional Roads (Vehicular Access)**

Development Control Policy 5.1 – Regional Roads (Vehicular Access) (DCP 5.1) addresses matters relating to the control of development adjacent to regional roads. Clause 3.3.1 of DCP 5.1 requires states that the Commission will seek to minimise the creation of new driveways on regional roads. Clause 3.3.2 states that *“on regional roads not constructed or planned to freeway standards, there is a general*

presumption on traffic and safety ground against the creation of new driveways or increased use of existing access to these roads. Where alternative access is or could be made available from side or rear street or from rights of way, no access shall be permitted to the regional road unless special circumstances apply.”

Due to the confined nature of the site, access is required from Collier Road to allow the 19.0m fuel trucks to adequately access the site and exit in forward gear. The Department of Planning have indicated in their correspondence that they support the proposed access arrangement with a proposed crossover to Collier Road.

Officer Comments

Assessment Under Local Planning Scheme:

- Car Parking

The Scheme permits discretion in regards to applying an acceptable amount of bays on site. The fuelling bays will likely be the most used bays on the site, with cars waiting in the manoeuvring areas to access the fuelling bays during peak times. In this regard, enforcing more dedicated parking bays on site would be considered unnecessary, as the primary demand on site would be for the fuel bays. There is sufficient manoeuvring space for cars to queue on the northern and southern sides of the fuel bays, noting that there is the potential for vehicle conflict between cars queueing on the southern side of the fuel bays and cars reversing from the dedicated visitor parking.

The 7 dedicated customer bays and 2 dedicated staff bays would be considered sufficient to provide for the retail needs of the convenience store. Furthermore, in the unusual circumstance that all 7 of the customer bays are occupied, the fuel bays can cater for overflow parking if they are not otherwise occupied.

- Bicycle Parking

Comments were provided from the Department of Planning recommending that bicycle parking be provided on site. The application for a proposed convenience store is located in an industrial area where the primary form of transportation would be by vehicle. On this basis, it is not deemed necessary that the site provide bicycle bays.

- Local Planning Policy 6 – Industrial Zones Development Design Guidelines (LPP 6) Variations and Recommended Conditions

Setbacks of Buildings:

A variation to the front setback and to the side setback if measuring to the roof canopy. Despite the large size of the structure, its open nature is not imposing upon the streetscape and forcing the proposal to meet the setback requirements would not result in a notable difference from the streetscape. On this basis, the variation should be supported.

Vehicle Parking:

LPP 6 refers to compliance with Local Planning Policy No. 8 – Parking Specifications (LPP 8) relating to minimum construction requirements for parking bays. As no detail has been provided as to the finished construction requirements, the Town

recommends implementing a condition that requires all parking bays and manoeuvring areas on site to be constructed in accordance with LPP8.

Landscaping:

As discussed in the applicants report, the provided plan is a landscaping concept plan and not a finalised landscaping plan as required under the LPP6 and Local Planning Policy 18 – Landscaping with Local Plants (LPP 18). The Town requests that a condition be applied to the approval that requires a professionally prepared landscaping plan to be provided at the building license stage that details the following as a minimum:

- a. All the requirements listed under LPP18 including but not limited to: street frontages, contours, reticulation details, details of ground treatment and plant legend showing:
 - i. quantity of plants,
 - ii. species name
 - iii. pot size of plants at the time of planting;
 - iv. height at full growth
- b. Details of the location and type of proposed trees, shrubs, ground cover and lawn areas to be planted;
- c. All plants to be of low water use;
- d. Landscaping of the verge area adjacent to the development site, including the provision of 4 substantial street trees (2 x *Corymbia Ficifolia* to Collier Road and 2 x *Melaleuca Quinquenervia* to Alice Street) of a minimum 90L pot size in accordance with the Town's adopted Street Tree Master Plan;
- e. The total number of plants to be planted at a minimum rate of 4 per 1m²; and
- f. Details of the proposed watering system to ensure the establishment of species and their survival during the hot, dry summer months.

A variation has been proposed to landscaping in the front street setback adjoining Collier Road, with 2m of landscaping being required and 1.65m of landscaping being provided to allow sufficient truck manoeuvring on site. The opportunity exists for landscaping to be provided in the verge to cater for the shortfall of landscaping. The Town considers the additional landscaping provided to the rear of the site to be sufficient in catering to the landscaping requirements of the site. The variation to landscaping is minor in nature and is occurring as a result of vehicle manoeuvring. On balance, the Town considers a variation to landscaping to permit adequate manoeuvring to be an acceptable.

Security Fencing:

Boundary fencing to a height of 1.8m has been proposed on the western lot boundary that protrudes into the 13m front street setback area. As the convenience store does not incorporate fencing to the front of the lot, the fencing is to separate the two lots and prevent vehicle access to the two sites. An application has been submitted for the adjoining site that seeks to retain existing solid fencing at a distance of 11.5m from the street but that doesn't incorporate fencing forward of 11.5m. The Town recognises that the applicant wishes to separate the two lots through fencing, but considers a solid fence of 1.8m forward of 11.5m to be unnecessary. The Town therefore recommends a condition to make all fencing forward of the 11.5m setback line from the Collier Road lot boundary to be open style palisade fencing.

Storage and Refuse:

The policy requires that the storage and refuse area be constructed from masonry or other approved building material. Despite the location of the of the bin storage area,

the Town recommends a condition to ensure that the bin area will be constructed of masonry construction. The Town also recommends through a condition that the bin store is fitted with a hose cock and that the floor shall be graded to a floor waste and connected to sewer.

- **Local Planning Policy 16 – Control of Advertisements under the Local Planning Scheme No. 10 (LPP 16) Variations and Recommended Conditions**

There are multiple variations to Local Planning Policy 16 in regards to the proposed signage incorporated as part of application for the convenience store. The Town recognises the minimal impact of many of these signs considering the surrounding general industrial land uses and also considering proposed signage would be standard as part of a 7-Eleven convenience store.

However, the proposed S1 “High Main Price Board” is considered excessive in regards to its height within a close proximity to the front boundary. A similar DAP application for a 7-Eleven convenience store was considered at Caversham in the City of Swan that incorporated signage to a height of 6m. Furthermore, the existing Puma convenience store on the intersection of Guildford Road and Katanning Street in Bayswater incorporates signage to an approximate height of 6m. A height of 6m is considered sufficient to cater to the commercial needs of the applicant and therefore the Town recommends a reduction in size to 6m through the way of a condition in accordance with the Town’s policy.

- **Access Arrangements onto Collier Road, Vehicle Manoeuvring, Service Access and Crossover Width**

The application incorporates a large crossover (12.7m at lot boundary) to Alice Street that provides access to both the subject site in addition to an easement for staff parking and the access to adjoining site to the west. The use of the easement and the extended crossover to Alice Street provides the added benefit of additional vehicle manoeuvring area for the 19.0m fuel trucks and allow the vehicles to adequately enter the site in conjunction with the provided vehicle templates. However, the resulting crossovers to both Alice Street and Collier Road (approximately 12.4m at lot boundary) exceeds the Towns standards in regards to width, which permits a maximum width of 10.7m. An argument could be presented that the site should be increased to a sufficient size as part of the subdivision process that would allow adequate vehicle manoeuvring to occur within the site whilst also adhering to the crossover standards, but on balance this would be considered excessive. On this basis, the Town is supportive of the extended crossovers to support vehicle manoeuvring.

In addition, the application incorporates a crossover to Collier Road to enable the fuel trucks to leave the site in forward gear. As a result of the vehicle leaving the site there is the potential for vehicle conflict for cars turning into the site from Collier Road, holding up traffic in the left land as the vehicle exits. Furthermore, the turning movements shown indicate that the truck will cross into the right hand lane to adequately exit the site. The provided Transcore Transport Impact Assessment states that the average weekday traffic flow is approximately 17,210 vehicles per day with peak periods between 8:00am to 9:00 am and 3:00pm to 4:00pm. The report also states that as a result of the proposed convenience store, there is estimated to be 13 vehicles per hour entering the site from Collier during peak AM hours, and 19 vehicles per hour entering during peak PM hours.

Section 9.0 of the report states that “Fuel tanks and other service vehicles are expected to access the site no more than 3-4 times a week and generally outside the peak road network and peak site activity periods.” To ensure that the fuel trucks are exiting the site during off peak periods, the Town recommends a condition on the approval limiting the periods that fuel trucks can enter and exit the subject site. Comments provided by the Department of Planning also recommends limiting fuel tanker access outside of peak times. In accordance with Figure 17 under 7.1 of the traffic report, the Town recommends preventing access to the site from fuel trucks between 7:00am and 9:00am and 3:00pm to 7:00pm on weekdays.

The application also incorporates a left in-left out modification to the median island that prevents right hand turns into and out of the proposed crossover to Collier Road. This extension is supported on the basis that it prevents dangerous vehicle movements into and out of the site. However, it is the expectation of the Town that the applicant is responsible for the entire cost associated with the upgrading works. It is also the expectation that the applicant will liaise with the both the Department of Planning and Town of Bassendean as necessary to organise the works and that all works shall be completed prior to occupation of the fuel station. The Town therefore recommends a condition relating to the extension of the median.

Comments made on behalf of the Department of Planning recommend consideration of upgrades to the intersection of Alice Street and Collier Road. As part of the analysis in the Transcore report, it has been identified that there is a potential increase in delay during PM peak hours from 64.6 seconds to 94.6 seconds for vehicles turning right from Alice onto Collier Road. As the road is already considered to have a poor level of service, upgrading the intersection is not deemed necessary as part of this application.

The Department of Planning also made recommendations regarding changes to the kerbing on site to allow additional access throughout the site. Whilst the Town looks to enforce its crossover standards wherever possible, the Town is agreeable to a proposed modification on the west side of the site, straightening the kerb inside the lot to allow improved vehicle access with a reduction to landscaping. The Town would still recommend maintaining a wing radius of 5m in accordance with the Town’s standards. The Town recommends adding a condition requiring the plans to be updated prior to the submission of the building permit.

- **Percentage for Art Policy**

The policy states that all development proposals that are commercial in nature with a value that exceeds \$1,000,000 requires a contribution. Whilst the application falls within the industrial zone, the use is considered commercial in nature. On this basis, the Town recommends implementing a condition for the public art contribution prior to submission of a building permit.

- **Retaining Wall Height**

Retaining walls are proposed to a height of 0.9m to 0.75m on the eastern side of the site on the Alice Street lot boundary. The proposed top of wall height is 23.0 with the proposed levels on site being 22.6 at the highest point. Retaining walls are therefore only considered necessary to a height of 22.6. The Town recommends implanting a condition which limits the heights of the retaining walls to not exceed a top of wall height of 22.6.

Council Recommendation:

To be provided following Council's consideration of the application for development approval.

Alternate Recommendation:

Not applicable.

Conclusion:

The application proposes a 7-Eleven convenience store at Lot 25 (No. 300) Collier Road, Bassendean. As identified within this report, the proposed development satisfactorily addresses requirements contained within the Town's policies and LPS10 with the exception of the variations identified. Where Council Officers are of the view that a variation should not be accepted or have concerns regarding the proposal, conditions have been recommended to address the identified issues. The Town recommends approving the application subject to the conditions recommended.

Development Application Report Proposed Convenience Store

Lot 25 (300) Collier Road,
Bassendean

PLANNING SOLUTIONS
URBAN & REGIONAL PLANNING

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Figure 2: Zoning Map

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- Appendix 1: Certificate of Title and Diagram
Appendix 2: Easement Document H523108
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-

1 Preliminary

1.1 Introduction

Planning Solutions acts on behalf of Collier Road WA Pty Ltd, the proponent of the proposed convenience store development at the easternmost portion of Lot 25 (300) Collier Road, Bassendean (**subject site**). Planning Solutions has prepared the following report in support of an Application for Development Approval for a fuel retailing convenience store on the subject site.

This report will discuss various issues pertinent to the proposal, including:

- Site details.
- Proposed development.
- Town planning considerations.

The application seeks approval to develop a convenience store on a 1,828m² portion of the subject site, which will provide for the retail sale of fuel and goods of a convenience/retail nature. The proposed convenience store is suitably located to provide key services to the surrounding industrial area and patrons travelling along Collier Road.

We respectfully request the Metro Central Joint Development Assessment Panel (**JDAP**) grant approval to the proposed convenience store.

1.2 Background

On 17 July 2016, the project traffic consultants attended a meeting with the Town of Bassendean's (**Town**) senior officers to discuss the proposed convenience store's access arrangements, including the proposed crossover to Collier Road. The Town's officers provided their 'in-principle support' for the site's access arrangements.

In December 2016, various discussions were undertaken with the Town's senior staff, who advised the proposed convenience store development would need to demonstrate it would not impact on the existing warehouse facility on the subject site, in addition to meeting the requirements of the relevant planning framework.

2 Site details

2.1 Land description

Refer to **Table 1** below for a description of the land subject to this development application.

Table 1 – Lot details

Lot	Diagram	Volume	Folio	Area (ha)
25	60011	2209	97	1.4144

A 'right of support' easement (document H523108) runs along the site's southern boundary, benefitting Lot 24 to the south. The proposed development does not affect this easement.

A separate subdivision application has been lodged with the WAPC, seeking to subdivide the subject site in the following configuration:

- Lot A – 1,828m² at the corner of Collier Road and Alice Street.
- Lot B – 12,316m² fronting Collier Road, with access leg connecting to Alice Street.

For the purpose of this development application, the land on which the convenience store is proposed is future 'Lot A' only.

Refer **Appendix 1** for a copy of the Certificate of Title and Deposited Plan, and **Appendix 2** for a copy of easement document H523108.

2.2 Location

2.2.1 Regional context

The subject site is located approximately 8.5km north-east of the Perth CBD, 3km south-east of the Morley strategic metropolitan centre, and 5km north-west of the Perth Airport.

The subject site fronts Collier Road, an 'Other Regional Road' under the MRS. Collier Road links the site to Tonkin Highway, a major north-south regional road link through the Metropolitan Region. Collier Road also links to Guildford Road, which provides access to both the Perth CBD and Midland.

The subject site is situated within the municipality of the Town of Bassendean (**Town**).

2.2.2 Local context, land use and topography

The subject site is bounded by Collier Road to the north and Alice Street to the east. The subject site adjoins existing industrial land uses to the south and west.

The subject site is surrounded by industrial land uses on relatively large lots generally comprising:

- Transport depots.
- Factories and warehouses.

- Fast food and lunch bars.
- Light industry.

Collier Road is dual carriageway and carries weekday traffic flows in order of approximately 17,210 vehicles in the vicinity of the subject site. The western portion of Lot 25 currently contains an existing warehouse building and large hardstand/parking area. The property slopes slightly from west to east and contains no vegetation.

Refer **Figure 1**, aerial photograph.

Photographs 1 to 5 depict the subject site and surrounds.



Photograph 1 – the subject site, viewed from Collier Road.



Photograph 2 – Alice Street, viewed from Collier Road.

LEGEND

Subject Site



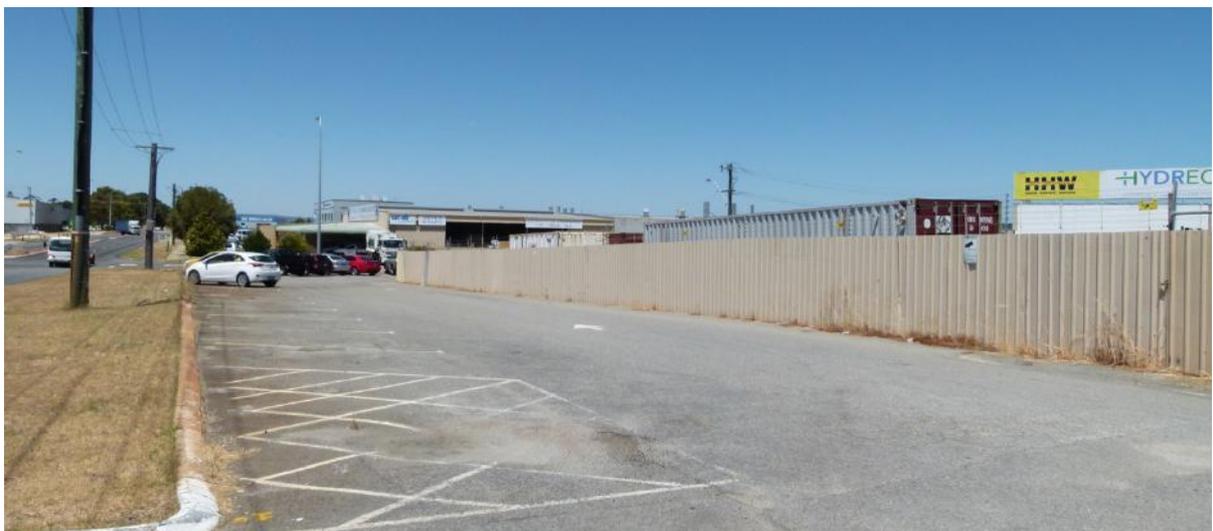
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Photograph 3 – Existing crossover to Alice Street.



Photograph 4 – the development site, viewed from Alice Street.



Photograph 5 – existing hardstand and fenced areas on the subject site.

3 Proposed development

The proposal seeks to develop a convenience store on a 1,828m² portion of the subject site (**development site**), with associated parking areas and signage. The 1,828m² of land encompassing the 'development site' is the subject of a current subdivision application and accordingly, is identified as 'Lot A' on the development plans. The balance portion of the subject site is identified as 'Lot B'.

The proposed convenience store is suitably located to provide key services to the surrounding industrial area and patrons travelling along Collier Road, and warrants approval accordingly.

3.1 Convenience store

The proposed convenience store will provide the retail sale of fuel and other goods of a retail/convenience nature. Specifically, the proposed development comprises:

- A retail building of 193m² gross floor area (**GFA**) fronting Collier Road.
- Plant and servicing areas at the western side of the retail building with an associated loading zone for service vehicles.
- A fuel canopy comprising a height of 5.5m.
- Four fuel bowsers, with eight refuelling spaces (two per bower).
- Two underground fuel storage tanks, with the filling point located at the eastern side of the fuel canopy.
- Nine additional (non-refuelling) car parking bays for customers and staff plus one air/water bay.
- Landscaping along street frontages and within the site.
- Various signage associated with the convenience store.
- A 1.8m fence along the development site's future western boundary.

The retail building is set back approximately 11.9m from Alice Street and 4.5m from the future property boundary to the west. The retail building is separated from the site's southern boundary by a 6m wide accessway, which will provide communal access for the development site (future Lot A) and western adjoining site (future Lot B) to Alice Street.

The plant/servicing area is located at the western side of the retail building, enclosed with a 1.8m Colorbond fence, accessed by a loading zone which allows for the access and temporary stopping of service vehicles. The plant/servicing area is enclosed with screen fencing.

The proposed fuel canopy has a Collier Road setback of 8.8m-11.5m, and an Alice Street setback of 6.2m-6.8m. The fuel canopy is a visually permeable structure, supported by structural beams integrated into the bowsers (located centrally within the canopy itself).

The proposed convenience store is orientated northwards (fronting Collier Road), with an ID sign at the site's north-eastern boundary. This arrangement allows sufficient exposure for east and westbound vehicles travelling along Collier Road to enter the site in a safe and coordinated manner. The proposed retail building shop front incorporates substantial glazing and high quality materials which result in a visually permeable and well-presented built form.

The proposed convenience store provides 9 car parking bays for the use of customers and staff, 8 refuelling bays adjacent to petrol bowsers (two each side) and 1 air/water bay.

The proposed convenience store will operate 24 hours per day, 7 days per week and accommodate two staff on site at any one time.

Refer to **Appendix 3** for the development plans.

3.2 Balance of subject site (future Lot B)

An 'overall' site plan is included within the development plan package depicting the indicative use, layout and reconfiguration of 'Lot B' in relation to this convenience store development.

The 'overall' site plan demonstrates that existing parking areas, fencing and storage areas are capable of being reconfigured to accommodate the proposed convenience store and allow Lot B to continue being used in a compliant manner. Importantly, the 'overall' site plan also demonstrates that both of the developments on future 'Lot A' and future 'Lot B' will be able to operate as separate entities on separate lots.

This development application relates only to the proposed convenience store on the 1,828m² 'development site' (Lot A). The reconfiguration of future 'Lot B' is subject to a separate development application being progressed by the landowner.

3.3 Access

The proposed convenience store will gain access via the site's existing crossover to Alice Street and a proposed crossover to Collier Road. The proposed access arrangements are supported by a Transport Impact Assessment (TIA) prepared by Transcore (refer **Appendix 4**).

The site's Alice Street crossover will operate as full movement, while the proposed Collier Road crossover will operate as left-in/left-out, formalised through an extended median strip. The convenience store is designed such that vehicles can access the convenience store via either crossover and safely navigate/egress the site in a coordinated manner.

The proposed access/crossover arrangements were discussed at a meeting between Transcore and the Town's senior officers on 17 July 2016, where the Town's officers granted their 'in-principle' support.

A subdivision application is currently being progressed that seeks to excise the development site containing the proposed convenience store (future Lot A) from the balance of the subject site to the west (future Lot B). To the south of the development site (future Lot A) is a 6m wide access leg, which forms part of future Lot B. An access easement will be applied to this access leg to benefit Lot A through the subdivision process.

3.3.1 Servicing

The proposed convenience store has been designed to facilitate the safe and efficient movements of fuel tankers and service vehicles. A plan depicting the swept path movements of a 19.0m fuel tanker and 8.8m service vehicle is contained in **Appendix 3**.

Fuel tankers will access the site in the following manner:

- Ingress via the Alice Street crossover.
- Navigate along eastern aspect of site towards the underground fuel tank filling point.
- Continue along the northern aspect of the site and safely egress via the Collier Road crossover.

Fuel tankers are anticipated to make up to 3-4 deliveries per week, depending on retail fuel consumption and general demand. Fuel deliveries will generally take place outside of peak traffic periods to ensure minimal disturbance to the site's operations and external traffic.

General stock deliveries and bin servicing will take place 1-2 times per week, although this may fluctuate depending on the time of year and demand for certain products. As depicted on the site plan, the loading area is situated at the western side of the retail building, away from the traffic flow of patrons and fuel tankers.

Refer **Appendix 3** for the development plans and **Appendix 4** for the transport impact assessment.

3.4 Signage

The proposal incorporates various advertising signage as part of the overall development. The proposed signage comprises:

- An 8m high illuminated ID sign fronting Collier Road.
- Three 2.2m high illuminated directional signs (two at Collier Road frontage, one at Alice Street frontage).
- Wall/pole mountable air and water sign identifying the air/water bay to patrons.
- Illuminated 7-Eleven building fascia signage, including changeable shop front promo sign and wall mounted scrolling sign.
- Illuminated 7-Eleven fuel canopy fascia signage.
- A square 1.8m x 1.8m tower logo sign, which will be affixed to a feature tower on the north west elevation of the premises.

The proposed ID sign comprises an LED digital price board and illuminated graphic panels.

The proposed convenience store incorporates high quality advertising sign panels that complement the architectural style and design of the retail building. The proposed signage is consistent with the 7-Eleven corporate branding implemented on all new and refurbished sites across Australia.

Refer to **Appendix 3** for the development plans, which contain signage details.

4 Statutory planning framework

4.1 Metropolitan Region Scheme

The subject site is zoned 'Industrial' under the Metropolitan Region Scheme (**MRS**). The subject site fronts Collier Road, which is reserved 'Other Regional Roads' under the MRS. A search under Clause 42 of the MRS has confirmed the site is not affected by regional reserves.

The proposal is consistent with the provisions of the MRS and may be approved accordingly.

Refer **Appendix 5** for a copy of the Clause 42 certificate.

4.2 WAPC Development Control Policy 5.1 – Regional Roads (Vehicular Access)

The WAPC Development Control Policy 5.1 – Regional Roads (Vehicular Access) (**DCP 5.1**) sets out the planning framework controlling access to regional roads. Section 3.3 of DCP 5.1 stipulates the development requirements to control access to regional roads. **Table 2** below provides an assessment against the relevant requirements of Section 3.3 of DCP 5.1.

Table 2 – assessment of DCP 5.1 requirements.

Requirement	Comment
<p>3.3.1</p> <p><i>In considering applications for access on regional roads, the effects of the proposals on traffic flow and road safety will be the primary consideration. The more important the regional road, the greater the importance attached to these factors. In general, the Commission will seek to minimise the creation of new driveways on regional roads and rationalise existing access arrangements.</i></p>	<p>The proposal seeks to construct a crossover to Collier Road, which is reserved as 'Other Regional Roads' under the MRS.</p> <p>The proposed crossover to Collier Road will operate as 'left-in/left-out', to maximise safety and ensure minimal disturbances to the existing traffic flow of Collier Road. The operation of this crossover as 'left-in/left-out' is formalised through an extended median strip along Collier Road.</p> <p>A transport impact assessment (Appendix 4) has been prepared for the proposed development. The transport impact assessment demonstrates the proposal is satisfactory from a traffic engineering perspective, and that the proposed access arrangements are safe and acceptable.</p>
<p>3.3.2</p> <p><i>Where regional roads are constructed or planned to freeway standards, no access to frontage development is permitted. On regional roads not constructed or planned to freeway standards, there is a general presumption on traffic and safety grounds against the creation of new driveways or increased use of existing accesses to these roads. Where alternative access is or could be made available from side or rear streets or from rights of way, no access shall be permitted to the regional road unless special circumstances apply.</i></p>	<p>The development site is located at the corner of Collier Road and Alice Street. Collier Road is reserved as 'Other Regional Roads' under the MRS and not planned to be upgraded to freeway standard.</p> <p>The proposed convenience store requires a crossover to both of its frontages to function effectively. Transcore's transport impact assessment (Appendix 4) undertakes a detailed analysis of the proposed access and demonstrates the suitability of this arrangement.</p> <p>The development site (which will be excised from the balance of the subject site through a subdivision application) has been specifically designed to accommodate a convenience store. 19.0m fuel tankers can comfortably and</p>

Requirement	Comment
	safely access via Alice Street, navigate and egress the site via the site's Collier Road, as demonstrated by swept path modelling.
<p>3.3.3</p> <p><i>An arrangement whereby adjoining owners enter into cross-easement agreements to provide reciprocal rights of access across adjacent lots may be required as a means of rationalising access to the regional road.</i></p>	<p>An additional left-in-in/left-out crossover is proposed to service the development site, as utilising the existing access point to Collier Road is not practical or achievable.</p> <p>Transcore has undertaken a detailed analysis of the proposed access arrangements and confirmed they are acceptable (and supported 'in-principle' by the Town).</p> <p>Notwithstanding, the site's crossover to Alice Street will remain full movement, and a common accessway for the use of both future Lot A and Lot B will be provided.</p> <p>This provides an access arrangement whereby vehicles can access Lot B via Alice Street (reducing dependency on access points to Collier Road).</p>
<p>3.3.4</p> <p><i>The types of development that would be allowed access to a regional road include large traffic generators such as major shopping, recreation or community centres. These would justify either a local distributor or access road, leading from a junction with the regional road to car parks servicing the centre.</i></p>	<p>A fuel retailing convenience store in this location requires direct access to the regional road to achieve safe and efficient movement of all vehicles using the site (including fuel tankers).</p>
<p>3.3.5</p> <p><i>In determining applications for development involving the formation, laying out or alteration of a means of access to regional roads, the following must be considered:</i></p> <p><i>i) the effects of the development on traffic flow and safety, the character and function of the road, the volume and speed of traffic, the width of the carriageway and visibility; and</i></p> <p><i>ii) the volume and type of traffic generated by the development.</i></p>	<p>The proposed convenience store is appropriately located to provide key services to the relatively high number of daily vehicles utilising Collier Road, which has a speed limit of 70km/h.</p> <p>The proposed crossover will operate as left-in/left-out, which will ensure the flow of traffic along Collier Road can continue to operate with minimal disturbances. The proposed left-in/left-out crossover is supported by a transport impact assessment (Appendix 4) which confirms there will be minimal adverse impacts on the surrounding road network from the proposed convenience store.</p> <p>Furthermore, the surrounding area is zoned for industrial purposes and contains a variety of operational industrial activities. The transport impact assessment confirms that the proposed access to Collier Road will function effectively and will not have any negative effects on the character or function of Collier Road.</p>
<p>3.3.6 <i>Where access is permitted, conditions may be imposed prescribing the location and width of the junction or driveway to ensure adequate visibility and to provide for the safe and convenient movement of vehicles both entering and leaving the traffic stream. In determining design requirements for junctions and driveways, the Commission has regard to standards recommended by both the National Association of State Road Authorities and Main Roads Western Australia.</i></p>	<p>The proposed access point to Collier Road has been designed to comply with relevant standards, including Australian Standards.</p>

Having regard to **Table 2** the proposal is consistent with DCP 5.1 and the proposed access point to Collier Road warrants approval.

4.3 Town of Bassendean Local Planning Scheme No. 10

4.3.1 Zoning

The subject site is zoned 'General Industry' under the Town's Local Planning Scheme No. 10 (**LPS10**). Clause 4.2.4 of LPS10 sets out the objectives of the 'General Industry' zone. The relevant objectives are as follows:

- (c) *To accommodate a range of manufacturing and associated service activities which will not, by the nature of their operations, detrimentally affect the amenity of the adjoining or nearby land.*
- (d) *To achieve safety and efficiency in traffic circulation, and also recognise the function of Collier Road as a regional road.*
- (e) *To provide car parking and landscaping appropriate to the scale of development.*

The proposal seeks to develop a convenience store on the development site (including parking and landscaping), which will provide the retail sale of fuel and goods of a retail/convenience nature. The convenience store will complement the surrounding industrial businesses and service the relatively high number of daily vehicles travelling along Collier Road. The proposed convenience store is supported by a transport impact assessment.

The proposed convenience store is suitably located to provide key services to the surrounding area and is consistent with the objectives of the 'General Industry' zone.

Refer **Figure 2**, zoning map.

4.3.2 Land use

The proposal involves the development of a 'convenience store' on the development site. A 'convenience store' is defined under *Schedule 1 – Dictionary of Defined Words and Expressions* of LPS10 as:

"convenience store" means premises -

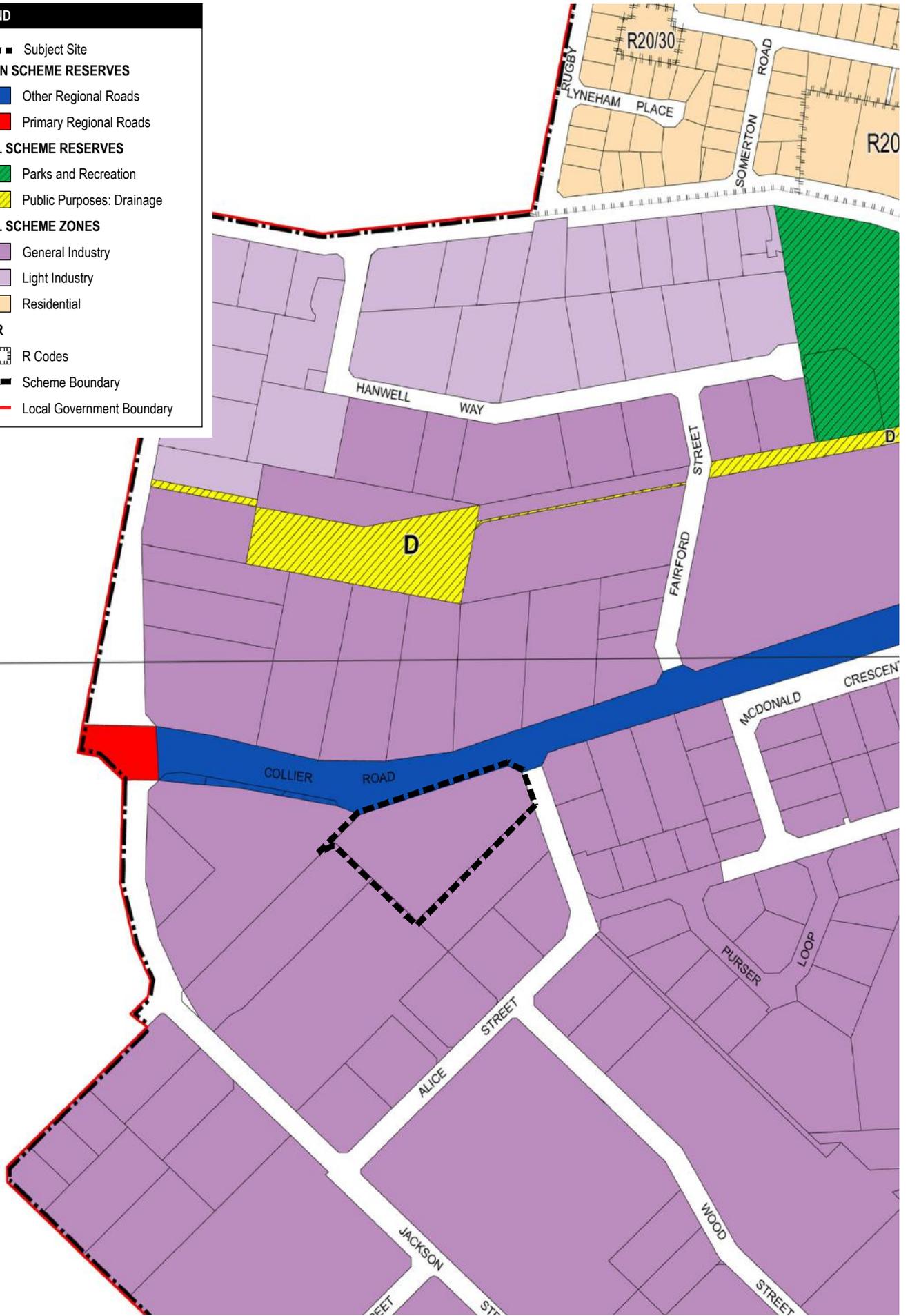
- (a) used for the retail sale of convenience goods commonly sold in supermarkets, delicatessens or newsagents, or the retail sale of petrol and those convenience goods;*
- (b) operated during hours which include, but may extend beyond, normal trading hours;*
- (c) which provide associated parking; and*
- (d) the floor area of which does not exceed 300 square metres net lettable area;*

As described in section 3.1 of this report, the proposed convenience store will:

- Provide the retail sale of fuel, in addition to other goods of a retail/convenience nature.
- Operate 24 hours per day, 7 days per week.
- Provide parking for customers and staff.
- Provide a retail building of 193m² GFA.

LEGEND

- Subject Site
- REGION SCHEME RESERVES**
- Other Regional Roads
- Primary Regional Roads
- LOCAL SCHEME RESERVES**
- Parks and Recreation
- D Public Purposes: Drainage
- LOCAL SCHEME ZONES**
- General Industry
- Light Industry
- Residential
- OTHER**
- R Codes
- Scheme Boundary
- Local Government Boundary



The proposed development, by nature of its use, function and layout, is properly classified as 'convenience store' under LPS10. A convenience store is a 'D' discretionary use in the 'General Industry' zone and is therefore capable of approval, subject to discretion of the decision-maker.

4.3.3 Development standards

Section 5.7 of LPS10 sets out general development requirements applicable to all development applications within the scheme area. An assessment against the relevant requirements is provided in **Table 3** below.

Table 3 – general development requirements.

Development requirement	Provided/Comment
5.7.2 Car parking	
<p><i>5.7.2.1 General</i></p> <p><i>A person shall not develop or use land or erect, use or adapt any building for use for the purpose indicated in Table 1 of the Scheme, unless car parking spaces of the numbers specified in Table 2 are provided and such spaces are constructed, marked and maintained in accordance with the provisions of the Scheme.</i></p> <p><i>Where an application is made for planning approval and the purpose for which the land or building is to be used is not specified in Table 2, the local government shall determine the number of car parking spaces to be provided on the land having regard to the nature of the proposed development, the number of employees likely to be on the site, the prevention of the obstruction of roads and streets, and the orderly and proper planning of the locality and the preservation of its amenities.</i></p>	<p>There is no specified parking rate for a convenience store (or service station, in the absence of convenience store) under LPS10.</p> <p>As a guide, an assessment is provided against the recommended parking rates under the RTA <i>Guide to Traffic Generating Developments</i>.</p> <p>Rate: <i>Convenience store: 5 bays/100m² GFA</i></p> <p>The convenience store comprises a GFA of 193m², requiring approximately 10 parking bays under the RTA's guidelines.</p> <p>The proposed development provides 9 bays for customers and staff, 1 air/water bay, 8 refuelling spaces and 1 loading bay. A minimum of 19 vehicles can therefore be comfortably accommodated on site.</p> <p>Having regard to the above, proposed parking provisions are acceptable.</p>
<p><i>5.7.2.6 Landscaping Construction and Maintenance</i></p> <p><i>The owner and occupier of premises on which car parking spaces are provided shall ensure that:</i></p> <p><i>(a) the car parking area is landscaped with shade trees, laid out, constructed, drained and maintained in accordance with the approved plan;</i></p> <p><i>(b) the car parking spaces are sealed and clearly marked out at all times to the satisfaction of the local government; and</i></p> <p><i>(c) all trafficable areas to be sealed to the satisfaction of the local government.</i></p>	<p>Various landscaping is provided throughout the site, including around car parking areas.</p> <p>Landscape planting will be provided in accordance with Council requirements, as annotated on the development plans.</p>
<p><i>5.7.2.7 Landscaping for Off-Street Parking</i></p> <p><i>Boundary landscaping shall be provided for parking areas with more than 5 parking spaces and interior landscaping shall be provided for open parking areas with 21 or more parking spaces. Landscaping shall comply with the following requirements:</i></p>	<p>A landscaping strip is provided along all street frontages of the development site (future Lot A), which are largely compliant. However, the landscaping strip fronting Collier Road has been tapered and slightly reduced to facilitate tanker movements. This reduction will be offset through the provision of sufficient landscaping comprising endemic species, as specified in the landscaping concept plan.</p>

<p>(a) all areas between parking areas and adjoining streets shall have a minimum of 2.0 metres wide permanent landscape area, except in the instance of corner lots, where minimum width of 1.0 m shall apply. In addition, the local government may also require permanent landscaping between the parking area and all other side and rear property lines; and</p> <p>(b) for open parking areas, with 21 or more parking spaces, there shall be provided a minimum of 1 square metre of permanent landscaping for every 10 Square metres of parking bay area. Such landscaping shall not be in addition to any other landscaping required by this Scheme.</p>	<p>No parking areas for the convenience store about any street frontage.</p>
---	--

Having regard to **Table 3** the proposal is consistent with the general development requirements of LPS10.

Section 5.9 of LPS10 sets out further general development requirements for the Town’s industrial zones. **Table 4** provides an assessment against the relevant requirements.

Table 4 – general development requirements for industrial zones.

Development requirement	Provided/Comment
5.9.3 Use of Setback Areas	
<p>The land between the street alignment and the building setback shall not be used for any purpose except one or more of the following:</p> <p>(a) a means of access and egress;</p> <p>(b) the parking of vehicles used by customers and employees;</p> <p>(c) the loading and unloading of vehicles (refer to 9.4)</p> <p>(d) open air display of goods, provided such area does not cover more than 20% of the setback area, is not within 3 metres of the street alignment and does not reduce the area set aside for landscaping;</p> <p>(e) landscaping; and</p> <p>(f) the display and sale of motor vehicles where the local government's approval has been granted.</p>	<p>Primary and secondary street setback areas of the proposed convenience store are used for the following purposes:</p> <ul style="list-style-type: none"> - Access and egress. - Parking of vehicles. - Landscaping.

Having regard to **Table 4**, the proposal is consistent with the relevant development requirements for industrial zones of LPS10.

The proposal is consistent with the specific requirements set out by LPS10 and warrants approval.

4.4 Local Planning Policy No. 6 – Industrial Zones Development Design Guidelines

The Town’s Local Planning Policy No. 6 – Industrial Zones Development Design Guidelines (**LPP6**) sets out various development requirements specific to the Town’s industrial zones.

Table 5 below provides an assessment against the requirements of LPP6.

Table 5 – assessment against LPP6.

Development requirement	Provided/Comment								
Building Setbacks									
<p><i>The minimum setback requirements for all buildings within these zones shall be in accordance with those prescribed as follows:</i></p> <table border="0"> <tr> <td><i>Front</i></td> <td><i>13.00m</i></td> </tr> <tr> <td><i>Rear</i></td> <td><i>Nil</i></td> </tr> <tr> <td><i>Side</i></td> <td><i>4.50m one side</i></td> </tr> <tr> <td><i>Secondary Street</i></td> <td><i>6.00m</i></td> </tr> </table>	<i>Front</i>	<i>13.00m</i>	<i>Rear</i>	<i>Nil</i>	<i>Side</i>	<i>4.50m one side</i>	<i>Secondary Street</i>	<i>6.00m</i>	<p>The proposal's buildings and structures are set back as follows:</p> <p><u>Retail building</u> Front (Collier Road): 34.1m. Secondary (Alice Street): 11.7m-12.22m. Rear: 6.3m-15.4m. Side: 4.5m.</p> <p><u>Fuel canopy</u> Front (Collier Road): 8.9m-11.5m. Secondary (Alice Street): 6.25m-6.85m. Side: 3m.</p> <p>The proposed retail building complies with LPP6 setback requirements. However, the proposed fuel canopy varies front and side setback requirements. The variation warrants approval for the following reasons:</p> <ul style="list-style-type: none"> - The fuel canopy is an integral component of the convenience store development. The fuel canopy is suitably positioned to meet functional requirements of a fuel station. - The fuel canopy is a visually permeable structure (with no walls) and does not present to the street in the same way as a conventional building. - The fuel canopy is constructed of high quality materials and will complement the overall design of the convenience store.
<i>Front</i>	<i>13.00m</i>								
<i>Rear</i>	<i>Nil</i>								
<i>Side</i>	<i>4.50m one side</i>								
<i>Secondary Street</i>	<i>6.00m</i>								
Plot Ratio Limit and Definition									
<p><i>The maximum plot ratio for a site is 0.75.</i></p> <p><i>Plot ratio is defined as the ratio of the gross total of the areas of all the floor to the land within the site boundaries. In calculating the gross total of the areas of all floors the areas shall be measured over any walls provided that lift shafts, stairs, toilets and amenities, external wall thicknesses, plant rooms and the gross floor area of any floor space used for the parking of wheeled vehicles, including access to and from that space within the building, shall not be included.</i></p>	<p>The proposed convenience store retail building has an area of 193m², and the fuel canopy has an area of 304m². This equates to a total plot ratio of 0.27 when measured against the development site area of 1,828m².</p> <p>The existing buildings have a plot ratio of 0.38 over the total 1.4144ha of site area. This increases to 0.41 when the convenience store is added.</p>								
Site Cover									
<p><i>The maximum site coverage permitted to any development excluding loading bay awnings is 50%.</i></p>	<p>Site coverage equates to 497m² (27%) when measured against the development site area of 1,828m². Site coverage equates to 41% when measured with existing buildings over the total subject site area of 1.4144ha.</p>								

Vehicle Parking	
<i>Car parking spaces shall be provided, constructed and maintained in accordance with the provisions of Part 5 of the Scheme, Local Planning Policy No. 8 (Parking Specifications) and the approved plan relating thereto.</i>	A parking assessment is provided in section 4.3.3 of this report, which demonstrates the proposed parking provisions are acceptable and can sufficiently cater for the proposed convenience store development.
Landscaping	
<i>All development within these zones shall be landscaped in accordance with the following requirements</i>	A landscaping strip is provided along the site's primary and secondary street boundaries, which varies from 1.3m to 1.65m in width. Grass/turf is also provided on the verge fronting the development site.
<i>a) the minimum width of front boundary landscaping shall be 2 metres, except in the case of a corner lot, in which case the minimum shall be 1.0 metres on the secondary street as nominated by Council;</i>	To facilitate safe and efficient fuel tanker movements, a 2m wide landscaping strip along the site's Collier Road frontage cannot be provided. This variation is offset by providing landscaping throughout other parts of the site, which significantly contributes to the overall aesthetic value of the proposed development.
<i>b) the minimum width of side boundary landscaping (excluding side street boundaries) shall be 1 metre, to be provided from the front boundary to the setback line;</i>	No shade trees can be provided along the site's shopfront parking bays, as this would detrimentally affect visibility of the retail building and reduce opportunities for passive surveillance. Notwithstanding, shade is provided for vehicles by the proposed fuel canopy.
<i>c) landscaping is to be provided in accordance with Councils landscaping policy as amended from time to time, and shall be maintained by the owner of the lot thereafter.</i>	Potential locations of trees are depicted on the landscaping concept contained in the development plans. All landscape planting will be provided in accordance with Council requirements at the detailed design stage, as per standard practice.
<i>d) Shade trees in car parking areas shall be provided at a ratio of 1 per six car parking bays recommended in the Councils' landscaping policy.</i>	
Security Fencing	
<i>Where a security fence is proposed on a street lot boundary, it shall be of an open style either mesh fencing or palisade style fencing and setback 2-metres behind the street alignment.</i>	No security fencing is proposed along any street lot boundary within the development site.
<i>Solid screen fences on industrial sites are only acceptable where located at or behind the 13-metre building line to the front setback area. In the case of corner lots solid screen fencing must be setback 6-metres from the secondary street.</i>	
Service Access	
<i>Provision shall be made for service access in the following manner:</i>	A loading bay is provided at the western side of the retail building, which provides access to the site's bin area and delivery area. The loading bay is segregated from areas used by patrons and fuel tankers.
<i>a) a paved accessway shall be provided for vehicles from the street to the rear of and to any other part of the building where provision is made in the external walls of the building for the entry of or the loading or unloading of vehicles;</i>	The loading bay is 3.5m in width, which allows service vehicles up to 8.8m to access the bay and return to the street in forward gear, as demonstrated in the swept path modelling contained within the development plans (Appendix 3).
<i>b) the access way shall be so constructed that all vehicles using it can enter from and return to a street in forward gear without reversing any part of the vehicle onto a street; and</i>	
<i>c) except as hereinafter mentioned, the access way shall not be less than 4.5 metres in width, unless the size of the</i>	

<p><i>lot makes the provision of such an access way impracticable or unreasonable the Council may permit an access way of a narrower width but in no case less than 3.0 metres in width.</i></p> <p><i>d) the provision of on-site delivery manoeuvring areas where large single axle commercial vehicles can enter and leave the site in forward gear. The manoeuvring area shall be exclusive of car parking bays and the interior of the building.</i></p>	<p>An accessway of 3.5m in width is appropriate for the nature and scale of the development proposed, given it will only be used for the collection of waste and the delivery of stock.</p>
<p>Storage and Refuse</p>	
<p><i>The Council may require the provision of one or more areas for the storage of refuse in a development. This area shall be:</i></p> <p><i>a) screened from view from any public street, and enclosed by a wall of masonry or other approved building material, and being of not less than 1.8 metres in height; and</i></p> <p><i>b) accessible to service vehicles.</i></p> <p><i>Irrespective of whether a storage area is required for a development, no land shall be used for open storage purposes unless it is screened from view of any public street by a fence or wall built to the specifications and satisfaction of the Council.</i></p>	<p>The proposed bin area is located at the western side of the retail building. The bin area is positioned such that it is screened to the east, south and west by the retail building itself, and the proposed 1.8m Colorbond fence.</p> <p>A 2.2m high screen fence will screen the bin store from view to Collier Road. As noted above, the bin area is accessible to service vehicles.</p>
<p>Building Materials</p>	
<p><i>The facades of all buildings facing public streets shall be constructed of brick, lass, stone, masonry or concrete in respect of the ground floor level. The second floor level, or its equivalent, may be constructed of other material in accordance with the Building Code of Australia.</i></p> <p><i>The use of zincalume sheeting for construction of Industrial buildings is prohibited, unless such buildings or parts of buildings are not visible from any public street, or Council approval is granted to vary this requirement.</i></p>	<p>The retail building will be constructed of concrete panels, with other materials incorporated into the design to achieve a high quality built form.</p> <p>The fuel canopy will be constructed of metal and other high quality materials (in accordance with standard practice).</p> <p>All materials use for the proposed development will be in accordance with the BCA.</p>
<p>Floor Area</p>	
<p><i>A factory or factory unit shall have a floor area of not less than 100m² and each the length and width shall not be less than 6 metres clear between the internal wall surfaces.</i></p>	<p>N/A – no factory or factory unit is proposed.</p>
<p>Factory Units</p>	
<p><i>Multiple factory units on one lot shall not be permitted unless the following requirements are complied with:</i></p> <p><i>a) each factory unit shall have a visually screened service yard of not less than 16m² with a minimum depth of 2m adjoining and having connecting access to each unit; and</i></p> <p><i>b) the internal partition walls between factory units shall be constructed of brick, stone or concrete or other material approved by Council.</i></p>	<p>N/A – no factory or factory unit is proposed.</p>

Having regard to **Table 5**, the proposed convenience store is consistent with the provisions of LPP6 and is compatible with the existing industrial amenity of the locality.

4.5 Local Planning Policy No. 16 – Control of Advertisements Under the Local Planning Scheme No. 10

The Town’s Local Planning Policy No. 16 – Control of Advertisements under the Local Planning Scheme No. 10 (**LPP16**) sets out various standards for signage proposed within the scheme area. **Table 6** provides an assessment against the relevant requirements of LPP16.

Table 6 – assessment against LPP16.

LPP16 requirement	Provided/Comment
Wall signs	
Max height – 3m. Max length/width – 8m. Max area – 12m ² . Min. headroom – 1m. Max height above ground – 5m.	Various wall signs are proposed on the retail building fascia and fuel canopy fascia. The signage is simply a square containing the 7-Eleven corporate logo, which is 1m x 1m. Vinyl panels surround the wall signs which display the corporate colours of 7-Eleven. The dimensions of the proposed 7-Eleven wall signs are compliant with the specifications set out by LPP16 with respect to height, width and area.
Roof sign	
Max height – 3m.	A roof sign is proposed, which will be affixed to the feature tower that forms part of the retail building. The tower roof sign is 1.8m x 1.8m and therefore compliant with the roof sign specification of LPP16.
Pylon sign	
Max height – 6m. Max length/width – 2m. Max area – 4m ² . Max headroom – 2.4m. Max height above ground – 6m.	Proposed height – 8m. Proposed width – 2m. Proposed area (of advertising panels) – 13m ² The proposed pylon sign exceeds the prescribed height and area requirements set out by LPP16. The proposed pylon sign warrants approval for the following reasons: <ul style="list-style-type: none"> - The subject site is situated within an established industrial area, which is comprised of industrial uses situated on large lots of land. The size and scale of the sign is consistent with the existing context of the locality, and would not detrimentally affect the established industrial amenity. - The pylon sign is required to appropriately identify the site to passing motorists. The proposed Collier Road crossover is left-in/left-out only – motorists accessing the site via Alice Street will therefore require sufficient exposure/notice to safely ingress the site. - The proposed pylon sign is constructed of high quality materials and designed to a high standard. The sign will not detrimentally affect the visual amenity of the area.

Having regard to **Table 6**, the proposed signage warrants approval accordingly.

4.6 Local Planning Policy No. 15 – Percent for Art Policy

The Town's Local Planning Policy No. 15 – Percent for Art Policy (**LPP15**) stipulates requirements relating to the provision of public art for developments with an estimated construction cost of over \$1 million.

As the proposed convenience store has an estimated cost of development in the order of \$2.1 million, it may be considered an eligible proposal.

It is considered, however, that a public art contribution should not be required for this particular proposal, which seeks approval for a commercial use on a general industrial site, and is likely to fall within the scope of public art provision.

Regardless, any valid public art contribution should be addressed as a condition of planning approval in accordance with standard practice, noting there is no specific requirement under this policy for public art to be addressed during the development application stage. It is not considered appropriate that public art be provided onsite, nor on any adjacent crown land.

4.7 Local Planning Policy No. 18 – Landscaping with Local Plants

The Town's Local Planning Policy No. 18 – Landscaping with Local Plants (**LPP18**) provides guidance on matters to be considered for the preparation of detailed landscaping plans. Section 1.6 of LPP18 states the following regarding the Town's LPS10, with respect to landscaping:

Under the Local Planning Scheme No. 10, each application for planning approval is to be accompanied by:

- 1. The existing and proposed ground levels over the whole of the land the subject of the application and the location, height and type of all existing structures, and both the structures and vegetation proposed to be removed; and*
- 2. The nature and extent of any open space and landscaping proposed for the site.*

In accordance with section 1.6 of LPP18, a landscaping concept is provided which depicts existing/proposed levels and associated heights of structures, and the nature/extent of open space and landscaping proposed (including areas to be landscaped and likely species).

The following details set out by LPP18 are reasonably required at the detailed design stage as a condition of planning approval:

- street frontages, neighbouring buildings and fence lines;
- contours – both within the site and for the adjoining verge;
- reticulation details (type and method of operation);
- details of ground treatment for all common areas (for example; grass, paving, ground covers, mulch);
- plant legend, including the number of plants and species name including pot-size of plants at the time of planting; and
- accurate details of existing tree positions, with further detail for trees over 2m in height (species, trunk diameter, drip line and crown height).

5 Conclusion

It is proposed to develop a convenience store on a 1,828m² portion of the subject site, including associated signage. The proposed convenience store will comprise modern, attractive buildings and will conveniently provide key services to the surrounding area.

In summary, the proposed development responds to the planning framework and warrants the Town's support for the following reasons:

- The proposal seeks to use an underdeveloped portion of a large industrial lot for service commercial purposes, and is suitably located to conveniently service the surrounding locality.
- The proposed convenience store will have no detrimental impacts on the existing industrial amenity of the area.
- The proposal is supported by a Transport Impact Assessment, which demonstrates its acceptability from a traffic engineering perspective.
- The proposed convenience store will result in an attractive built form outcome for the site.
- It has been demonstrated that the proposed convenience store on future 'Lot A' and the existing warehouse facility on future 'Lot B' can operate as two separate entities and be consistent with the Town's planning framework.

Having regard to the above, the proposal clearly demonstrates to suitability of the proposed convenience store for the development site. Accordingly, we respectfully request the Application for Development Approval is approved by the Metro Central Joint Development Assessment Panel.

Appendix 1 Certificate of Title and Diagram

WESTERN



AUSTRALIA

REGISTER NUMBER

25/D60011

DUPLICATE EDITION

3

DATE DUPLICATE ISSUED

8/2/2005

VOLUME

2209

FOLIO

97

**RECORD OF CERTIFICATE OF TITLE
UNDER THE TRANSFER OF LAND ACT 1893**

The person described in the first schedule is the registered proprietor of an estate in fee simple in the land described below subject to the reservations, conditions and depth limit contained in the original grant (if a grant issued) and to the limitations, interests, encumbrances and notifications shown in the second schedule.



REGISTRAR OF TITLES

LAND DESCRIPTION:

LOT 25 ON DIAGRAM 60011

**REGISTERED PROPRIETOR:
(FIRST SCHEDULE)**

HICON (WA) PTY LTD OF PO BOX 224 CLAREMONT

(T N237991) REGISTERED 29/1/2016

**LIMITATIONS, INTERESTS, ENCUMBRANCES AND NOTIFICATIONS:
(SECOND SCHEDULE)**

1. THE LAND THE SUBJECT OF THIS CERTIFICATE OF TITLE EXCLUDES ALL PORTIONS OF THE LOT DESCRIBED ABOVE EXCEPT THAT PORTION SHOWN IN THE SKETCH OF THE SUPERSEDED PAPER VERSION OF THIS TITLE. VOL 2209 FOL 97.
2. T7506/1948 EASEMENT BENEFIT AS TO PORTION ONLY SEE TRANSFER 7506/1948 AND VOL 2209 FOL 97. REGISTERED 16/6/1948.
H562135 EASEMENT T7506/1948 MODIFIED REGISTERED 28/9/2000.
3. T7507/1948 EASEMENT BENEFIT AS TO PORTION ONLY SEE TRANSFER 7507/1948 AND VOL 2209 FOL 97. REGISTERED 16/6/1948.
H562136 EASEMENT T7507/1948 MODIFIED REGISTERED 28/9/2000.
4. H523108 EASEMENT BURDEN SEE SKETCH ON VOL 2209 FOL 97. REGISTERED 11/8/2000.
5. *N237992 MORTGAGE TO NATIONAL AUSTRALIA BANK LTD REGISTERED 29/1/2016.

Warning: A current search of the sketch of the land should be obtained where detail of position, dimensions or area of the lot is required.
* Any entries preceded by an asterisk may not appear on the current edition of the duplicate certificate of title.
Lot as described in the land description may be a lot or location.

-----END OF CERTIFICATE OF TITLE-----

STATEMENTS:

The statements set out below are not intended to be nor should they be relied on as substitutes for inspection of the land and the relevant documents of for local government, legal, surveying or other professional advice.

SKETCH OF LAND: 2209-97 (25/D60011)
 PREVIOUS TITLE: 1596-392
 PROPERTY STREET ADDRESS: 300 COLLIER RD, BASSENDEAN.
 LOCAL GOVERNMENT AUTHORITY: TOWN OF BASSENDEAN

END OF PAGE 1 - CONTINUED OVER

RECORD OF CERTIFICATE OF TITLE

REGISTER NUMBER: 25/D60011

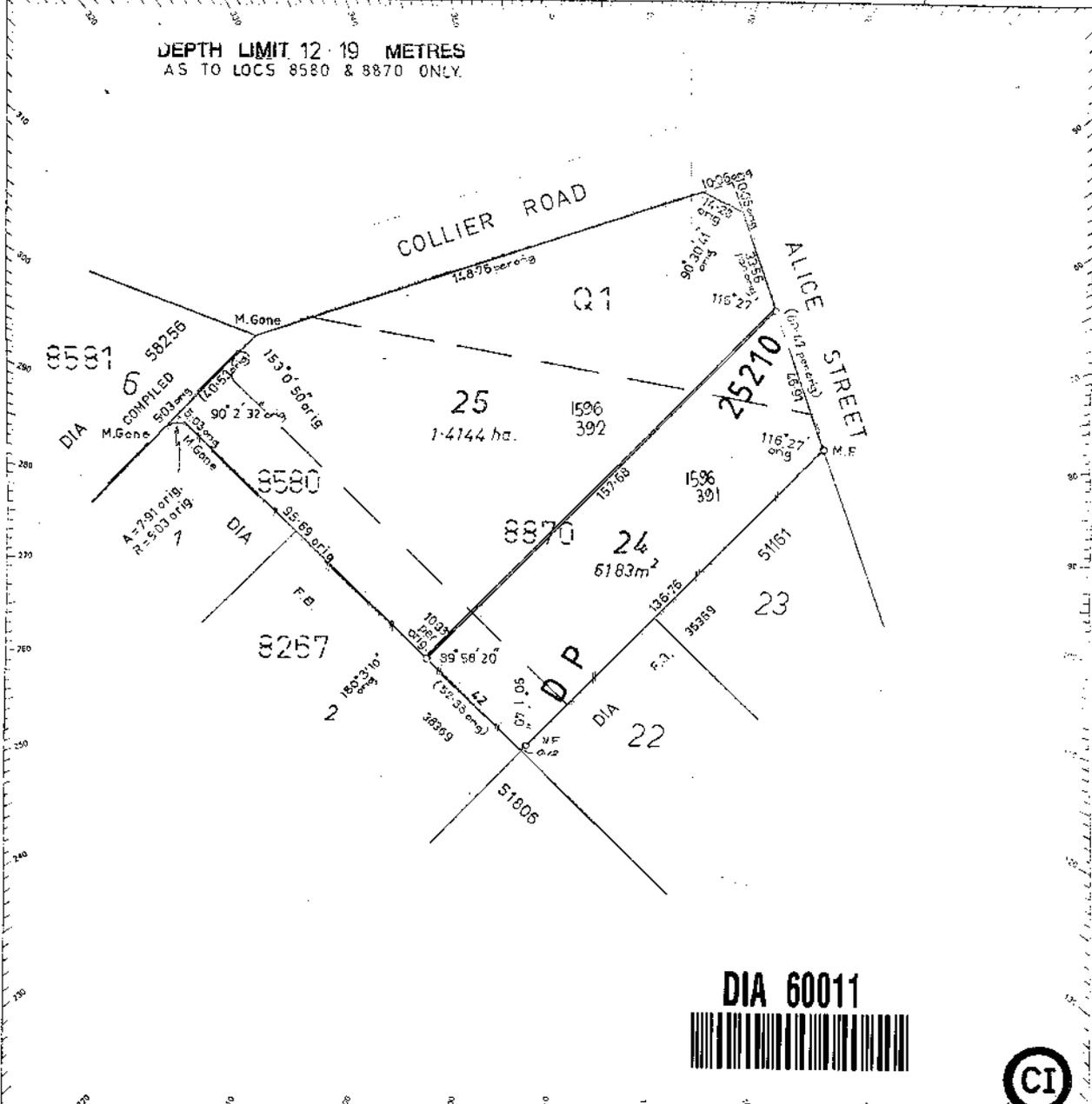
VOLUME/FOLIO: 2209-97

PAGE 2

NOTE 1: DUPLICATE CERTIFICATE OF TITLE NOT ISSUED AS REQUESTED BY DEALING
 N237992

Town or District.	Number of Lot or Location.	Field Book.	Scale.	Certificate in which Land is Vested.	Area
SWAN	LOT 21 OF LOC's 8580, 8870, Q1	44706	1:1500	Vol. 1473 Fol. 281	2.0327 ha

DEPTH LIMIT 12.19 METRES
AS TO LOC'S 8580 & 8870 ONLY.



DIA 60011



<p>CERTIFICATE</p> <p>I hereby certify that this survey was performed by me personally (or under my own personal supervision, inspection and field check) in strict accordance with the Licensed Surveyors (Guidance of Surveyors) Regulations, 1981.</p> <p>Date: <u>26.9.80</u> <u>G. W. Gordon</u> Licensed Surveyor</p>		<p>Approved by Town Planning Board</p> <p><u>55575</u> Date: <u>23rd September 1980</u> Chairman</p>	
<p>Approved</p> <p><u>G. Singleton</u> 13.10.80</p>	<p>On</p> <p>Plan: <u>51151</u> Diagram: <u>42479</u> Index Plan: <u>PERTH 2000 37.30</u></p>	<p>Registered</p> <p><u>824-</u> <u>42479</u> <u>29.9.80</u></p>	<p>Diagram No</p> <p><u>600</u></p>

Appendix 2 Easement Document

INSTRUCTIONS

1. This form may be used only when a "Box Type" form is not provided or is unsuitable. It may be completed in narrative style.
2. If insufficient space hereon Additional Sheet Form B1 should be used.
3. Additional Sheets shall be numbered consecutively and bound to this document by staples along the left margin prior to execution by the parties.
4. No alteration should be made by erasure. The words rejected should be scored through and those substituted typed or written above them, the alteration being initialled by the persons signing this document and their witnesses.

NOTES

1. Insert document type.
2. A separate attestation is required for every person signing this document. Each signature should be separately witnessed by an Adult Person. The address and occupation of the witness must be stated.

H 523108 E

11 Aug, 2000 08:20:33 Perth



REG. \$ 70.00

LODGED BY *ANZ*

ADDRESS

PHONE No.

FAX No.

REFERENCE No.

ISSUING BOX No. *25*

PREPARED BY **Richard Payne & Associates**
Commercial Solicitors

ADDRESS P.O. Box 1275, West Perth, WA, 6872
Tel - 08 9481 0844 Fax - 08 9481 2434

PHONE No.

FAX No.

INSTRUCT IF ANY DOCUMENTS ARE TO ISSUE TO OTHER THAN LODGING PARTY

*CT to
ANZ*

1/2

TITLES, LEASES, DECLARATIONS ETC LODGED HERewith

1. *CT 1596/392*
2. *1596/391*
3. _____
4. _____
5. _____
6. _____

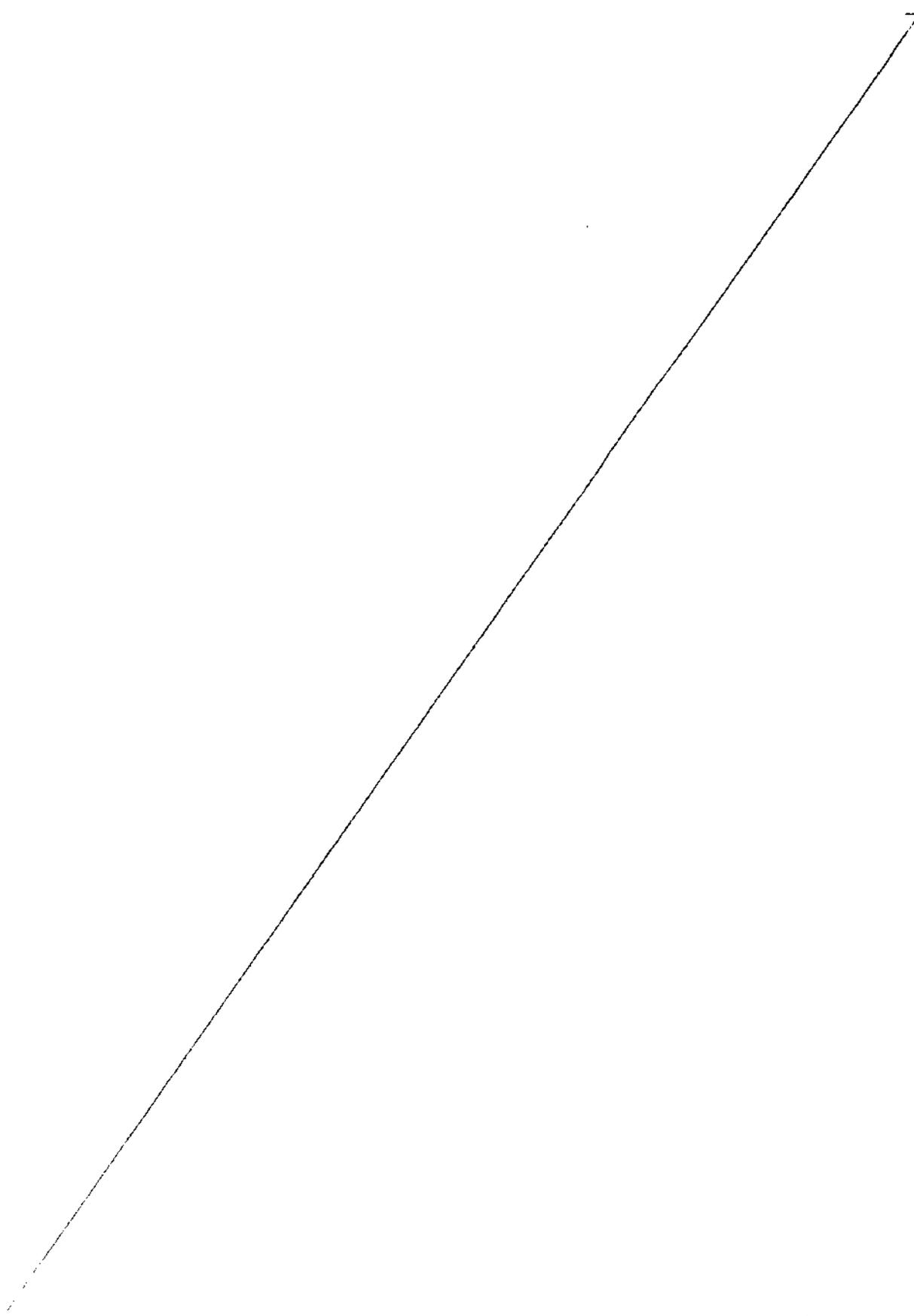
Received Items
Nos. *1/2*

Receiving Clerk *M*

Registered pursuant to the provisions of the TRANSFER OF LAND ACT 1893 as amended on the day and time shown above and particulars entered in the Register.

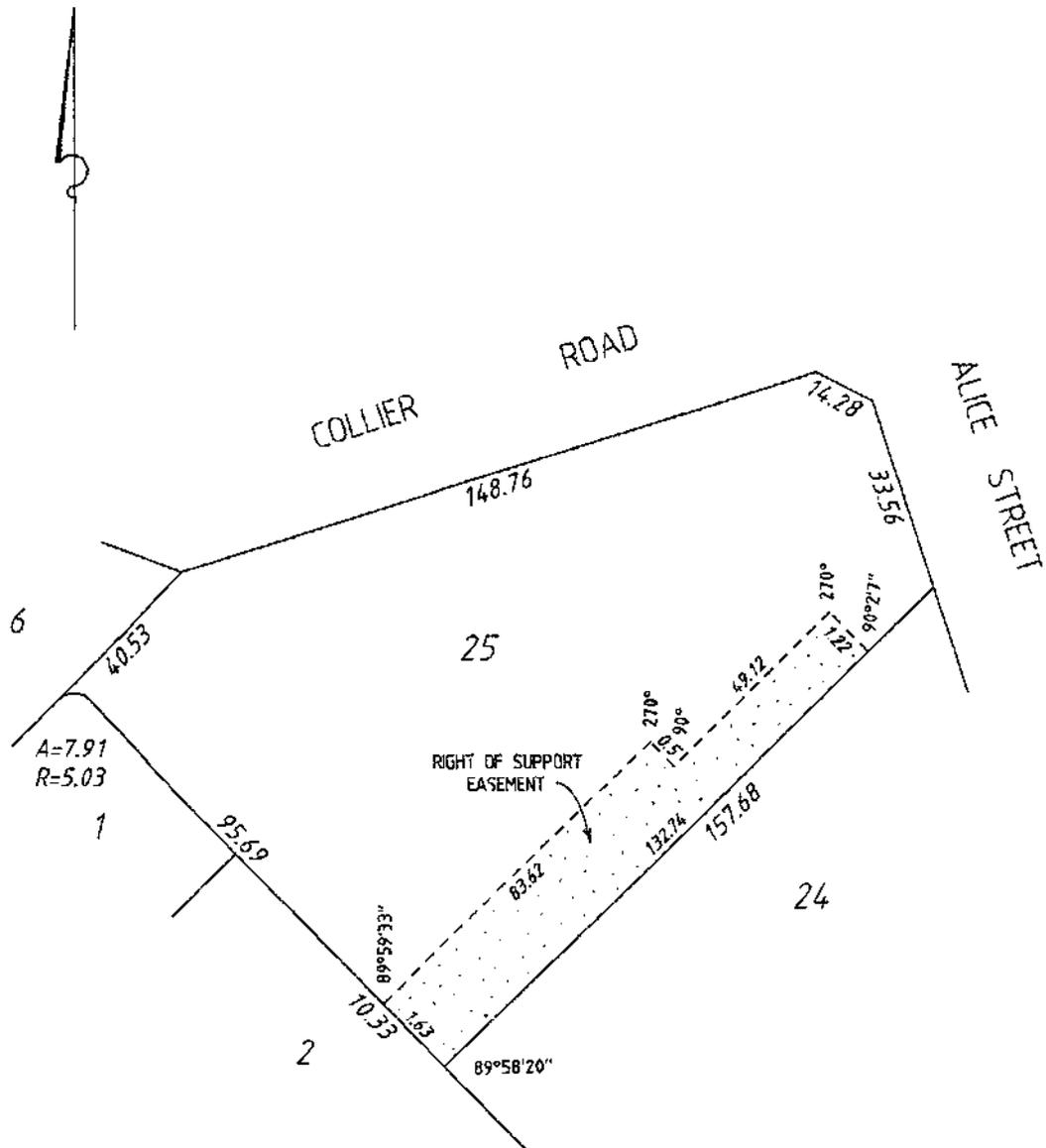
EXAMINED





Handwritten signature or initials, possibly 'AS' followed by a name.

CARLTON SURVEYS
 Licensed Surveyors
 SUITE 4, 180 BURSWOOD ROAD,
 BURSWOOD 5100.
 TELEPHONE 9361 5358 FAX 9351 3457



RIGHT OF SUPPORT EASEMENT IN FAVOUR OF
 LOT 24 OVER LOT 25 ON DIAGRAM 60011 CONTAINED
 IN CERTIFICATE OF TITLE VOLUME:1596 FOLIO:392

JOB : 3644

From Title
[Signature]

NOT TO SCALE
 DATE : 6-7-2000

Consent of mortgagee

Australian & New Zealand Banking Group Ltd ACN 11 005 357 522 as the mortgagee under mortgage no. F934989 over the Dominant Tenement hereby consents to the within easement being created and agrees that the within easement shall rank in priority to the said mortgage.

Dated the 8th day of August 2000

Signed for and on behalf of the)
Australian & New Zealand Banking)
Group Ltd ACN 11 005 357 522 by)
MARGARET DENISE KENSIT)
the attorney of the said Bank)
in the presence of:)
ACTING MANAGER-SECURITIES)

M. D. Kensit
Signature

[Signature]
An Officer of the said Bank

8. Waiver and variation

A provision of or a right created under this deed may not be waived or varied except in writing by the party to be bound.

9. Governing law

This deed shall be governed and construed by the laws of Western Australia and the parties hereto agree to submit to the jurisdiction of the Courts of Western Australia and any court hearing appeals from those courts in all matters arising out of this deed.

10. Consent of encumbrance holders

The parties shall obtain any necessary consent of the holder of any mortgage or other encumbrance over the Dominant Tenement or the Servient Tenement to the creation of the easement in accordance with this deed.

Executed as a deed

The common seal of Lina Pty Ltd)
ACN 009 346 963 was hereunto)
affixed by authority of the Directors)
in the presence of :)



TERENCE L. O'NEILL
Director's name

[Handwritten Signature]
Director's signature

Janet B. O'NEILL
Director/Secretary's name

[Handwritten Signature]
Director/Secretary's signature

The common seal of Trilon)
Nominees Pty Ltd ACN 008 872 133)
was hereunto affixed by authority)
of the Directors in the presence of :)



LOUIS R SAUZIER
Director's name

[Handwritten Signature]
Director's signature

BARBARA J SAUZIER
Director/Secretary's name

[Handwritten Signature]
Director/Secretary's signature

Encumbrance: Caveat 6247223

C:\mp\ltd\2014\02\24\N700
See letter

3.2 Grantor's power

The Grantor covenants with the Grantee that the Grantor has full power to make the grant set out herein and assures the Grantee such grant shall remain to and be quietly held and enjoyed by the Grantee and the benefit thereof shall be received and taken accordingly without interruption or disturbance by the Grantor or any person claiming by, through, under or in trust for or in any way against the Grantor.

3.3 Registration

The Grantor covenants and agrees to sign all applications and other documents and procure all necessary consents and doing all things necessary to procure the registration of this deed at the Office of Titles, pursuant to the provisions of the Transfer of Land Act 1893.

3.4 Further assurance

The Grantor and every other person having or rightfully claiming any estate or interest in the Servient Tenement will from time to time and at all times hereafter at the request of the Grantee do all such lawful assurances and things for more perfectly assuring the grant set out herein as the Grantee reasonably requires.

4. Grantee's covenants

The Grantee covenants with the Grantor:

- (a) to exercise the easement hereby granted in such manner as to cause as little inconvenience as is practicable to the Servient Tenement.
- (b) forthwith to remedy and make good any damage to the Servient Tenement which may result from the exercise of this easement and any rights granted hereunder by the Grantee.

5. Costs and stamp duty

The Grantee shall pay the costs of and incidental to the negotiation, preparation, execution, stamping and registration of this deed and all stamp duties and registration fees payable hereon.

6. Notices

Any notice or other communication in connection with this deed:

- (a) must be in writing in order to be valid;
- (b) may be signed by the party giving it or his solicitor or agent;
- (c) must be served, given or made by leaving it at the address of the addressee, or by sending it by prepaid ordinary post to the address of the addressee, or any other address which is notified by one party to the other; and
- (d) will be taken to be served, given or made in the case of delivery by hand, on delivery and in the case of prepaid ordinary post on the third day after posting.

7. Severance

The parties agree that a construction of this deed that results in all provisions being enforceable is to be preferred to a construction that does not so result. However, if despite this, any provision of this deed or the application of that provision to any person or circumstance is or becomes invalid or unenforceable, then the remaining provisions of this deed are not affected and are valid and enforceable to the fullest extent permitted by law.



replaced at any time whether before or after the date of this deed.

- (e) headings shall not effect the construction or interpretation of this deed.
- (f) references to a clause, paragraph or a schedule is a reference to the same in this deed.
- (g) a reference to "party" means a party to this deed and shall include that party's personal representatives, successors in title and assigns.
- (h) an agreement, representation or warranty on the part of two or more persons binds them jointly and severally.
- (i) an agreement, representation or warranty in favour of two or more persons is for the benefit of them jointly and severally.
- (j) a reference to a document includes that document as amended or replaced.
- (k) the covenants on the part of the Grantor bind the Grantor, its assignees, transferees and successors in title only for so long as such party remains registered as the proprietor of the Land.

2. Grant of easement

2.1 Grant

The Grantor hereby grants to the Grantee:

- (a) the right to use the portion of the wall and the footing in respect of the Building erected on the Servient Tenement as delineated and stippled on the plan annexed to this deed for the purpose of supporting the Building erected on the Dominant Tenement.
- (b) the right at all reasonable times and on reasonable notice to the Grantor to enter upon that portion of the Servient Tenement as delineated and stippled on the said plan, by its agents, engineers, servants, workmen and others (with or without plant and equipment) to inspect, clean, repair and repaint as is reasonably necessary to maintain that part of the Building erected on the Servient Tenement.

2.2 Term

The easement granted by this deed shall commence at the date of its execution by the Grantor, and shall expire on the date of demolition of the Building.

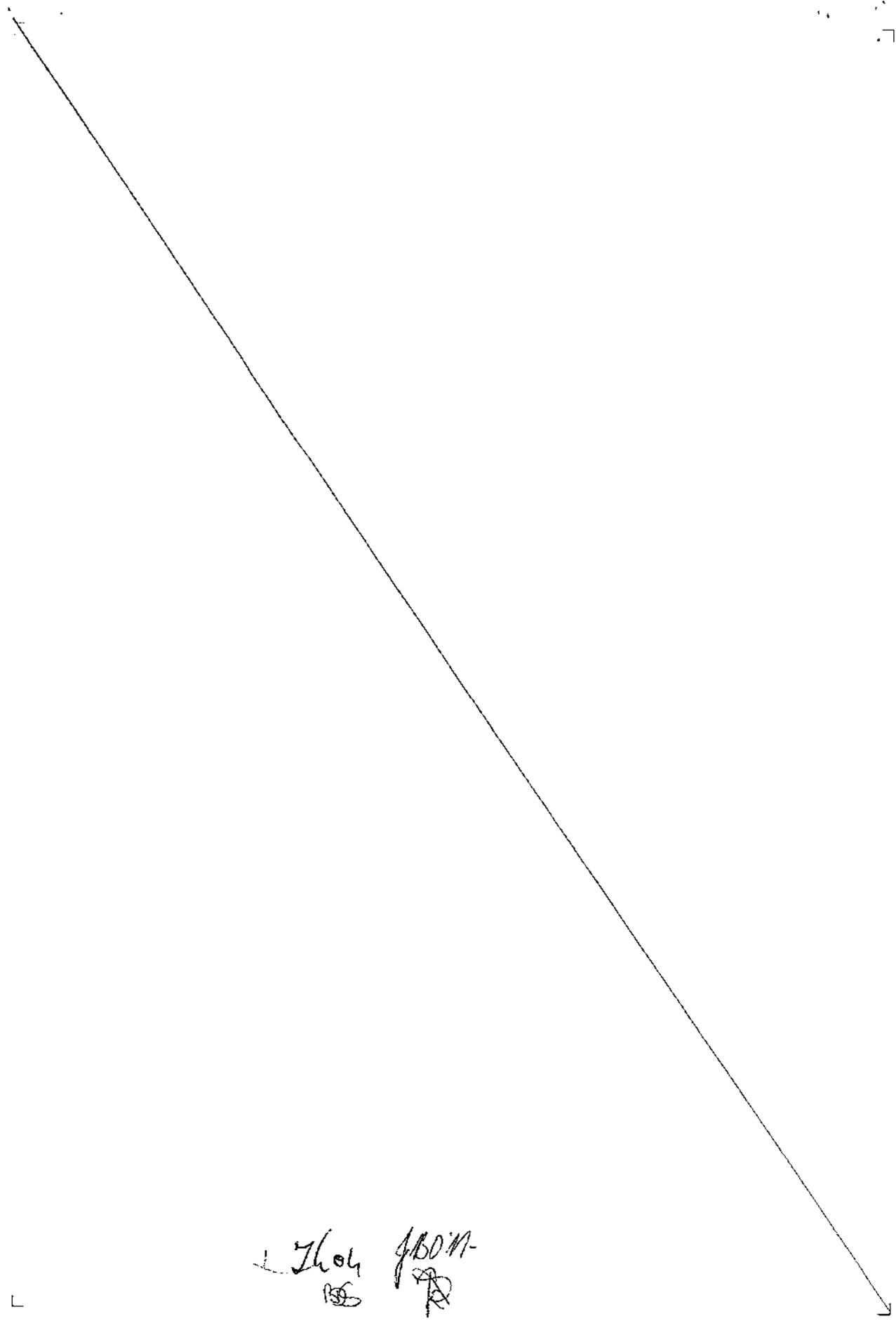
3. Grantor's covenants

3.1 No impediment to easement

The Grantor covenants with the Grantee so as to bind the Servient Tenement, that the Grantor shall not:

- (a) excavate under, or beside or undermine that portion of the wall and the footing in respect of the Building on the Servient Tenement, provided that nothing herein contained shall be construed to prevent the Grantor from making excavation on the Servient Tenement so long as sufficient artificial means of support to the Building is provided while the excavations are open;
- (b) cut, demolish, remove, alter, deface or in any way damage or destroy any portion of the wall and the footing in respect of the Building erected on the Servient Tenement;
- (c) plant or permit to be planted any trees or other plants which may damage any portion of the wall and the footing in respect of the Building erected on the Servient Tenement; and

3
1



L. Thol
135
450M-
R

BLANK INSTRUMENT FORM

Easement

(Note 1)

TAX INVOICE - ABN 45 320 599 544
WESTERN AUSTRALIA STAMP DUTY
DEE 09/08/00 15:14 001685780-001
FEE \$ *****
SD \$ *****20.00 PEN \$*****.00
2000

This deed of easement is made the *8th* day of *August*

Between **Lina Pty Ltd** ACN 009 346 963 of Suite 4, 24 Lawley Street, North Beach, Western Australia, 6020 (**the Grantor**)

and **Trilon Nominees Pty Ltd** ACN 008 872 133 formerly of 5 Alice Street, Bayswater but now of Suite 4, 24 Lawley Street, North Beach, Western Australia, 6020 (**the Grantee**)

Recitals

- A. The Grantor is registered as the proprietor of an estate in fee simple of the Servient Tenement.
- B. The Grantee is registered as the proprietor of an estate in fee simple of the Dominant Tenement.
- C. The Grantee has erected upon the Dominant Tenement the Building. A portion of the wall and the footing in respect of the Building that is necessary for the support of the Building on the Dominant Tenement encroaches on the Servient Tenement and therefore forms part of the Servient Tenement.
- D. The Grantor has now agreed to grant to the Grantee an easement of support for the purpose of allowing the portion of the wall and the footing in respect of the Building erected on the Servient Tenement to remain so erected so as to provide support for the Building on the Dominant Tenement.

This deed witnesses and the parties mutually covenant and agree as follows:

1. Interpretation

1.1 Definitions

In this deed, unless the context otherwise requires:

Building means the building comprising the factory units constructed on the Dominant Tenement by the Grantor.

Dominant Tenement means lot 24 on diagram 60011 and being the whole of the land comprised in certificate of title volume 1596 folio 391.

Servient Tenement means lot 25 on diagram 60011 and being the whole of the land comprised in certificate of title volume 1596 folio 392.

1.2 Interpretation

In this deed, unless the context otherwise requires:

- (a) words importing the singular include the plural and vice versa.
- (b) words of one gender include every other gender.
- (c) words denoting individuals include a firm, body corporate, an unincorporated association and any governmental or other public body or authority of any kind and vice versa.
- (d) references to any statute or other law shall mean such statute or other law as amended or

RICHARD PAYNE & ASSOCIATES

Commercial Solicitors
Level 2, Colord House, 33 Colin Street, West Perth, W.A. 6005
Telephone (08) 9481 0844 Facsimile (08) 9481 2434
P.O. Box 1275, West Perth, W.A. 6872
Australia
e-mail - rapayne@strat.net.au

*This email may contain information which is confidential or which is subject to legal professional privilege.
If this facsimile has been sent to you by mistake please return it without making a copy.*

Our Ref: RP

Your Ref:

25 August 2000

Mr Joe Carniniti
Department of Land Administration
MIDLAND WA 6056

Fax 9273 7665 (1 of 1 page)

Dear Sir,

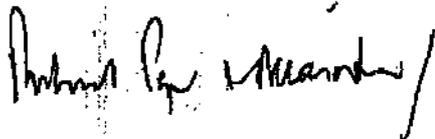
Requisition notice documents H523108

We refer to your requisition dated 24 August 2000 and our discussion with you on 24 August 2000.

We confirm that we act on behalf of Lina Pty Ltd and Trilon Nominees Pty Ltd, the grantor and grantee respectively pursuant to the deed of easement dated 8 August 2000. On behalf of our clients we request that caveat G247223 be included as an encumbrance in the deed of easement dated 8 August 2000.

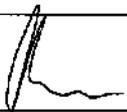
We will be lodging an application to include the benefit of the easement by the owner of C/T volume 1596 folio 391 with your office very shortly.

Yours faithfully,
Richard Payne & Associates



File Copy Do Not Destroy

Stopped Document Disposal Instructions

Fees to be Refunded	Document Type	Fee \$
Form 10 No.:		
Date:		
To:		
Address:		
Parties:		
Reference:		
Document Numbers:		
Computer Records Adjusted: WDR/MTF/DPA/BKC		
Documents to be Rejected:		
Documents to be Withdrawn:		
Documents for Registration:		
Caveats to be made Null & Void:		
Authorised by: 		

Examiner's notes:

Stopped Case: 523108

Original Cs/T:
1596 392
1596 391

Notice Sent: *24.8.00*
Action:

Examination Instructions

Allocate New C/T
Complete Nom. Index
Pass to
Other
Joe Caminiti
Supervisor: **P R F**



Requisition Notice

Document Nos: H523108

Section 192 of the Transfer of Land Act

Your Ref: Lina Pty Ltd

First and Final

Our Ref: Joe Caminiti

Phone (08) 9273 7356 Fax (08) 9273 7665

Date: 24 August 2000

Lodging Party
ANZ

Other Parties Contacted
Richard Payne & Assoc

Registration of the above documents cannot be effected until all requisitions listed below are complied with and the fee payable is received.
A time limit of 14 days applies from the date stated above.

Requisitions

Doc. No.	Description	Req. Fee
108	As caveat G247223 encumbers the grantors land the caveat should be shown as an encumbrance in the easement. ✓	\$35.00
	An application to include the benefit of easement by the owner of title Vol., 1596 Fol. 391 Trion Nominees Pty Ltd is also required. ✓	97 669
	A registration fee of \$70 is payable	

Requisition Sub Total \$
Additional Fee \$
TOTAL FEE Payable \$

Ian Hyde
Registrar of Titles

Requisitions may be attended to by ;

1. fax direct to the Examiner referred to above.
2. personal attendance to the Stopped Documents Section, Midland Square. (all documents held at Midland Office)
3. the lodging of evidence at the Perth Branch Office, Mt Newman House, 200 St. Georges Terrace, Perth. (note: no advice/discussions re: requisitions)
4. Post to P O Box 2222, Midland WA 6936 or DX88

Correspondence by representatives of parties to documents must state the capacity in which they act and confirm that they are duly authorised to do so. Amendment by letter is at the discretion of the Registrar of Titles.

Unless these requisitions are satisfied, the documents will be rejected and one half the registration fee forfeited

Documents may be withdrawn from registration for which a fee of \$35 per document will be retained. The balance of the fees will be refunded.

See payment options on page 2.

On the front of First Schedule CT 1596-391 orig only.

EASEMENT H523108. A right to use the wall and footings erected upon the portion of Lot 25 on Diagram 60011 stippled on the map in Easement H523108 for the purpose of supporting the building erected upon the within land, together also with the right to enter upon the said portion for the purpose of maintaining the said wall and footings, all as set out in the said Easement is granted to the proprietor or proprietors for the time being of the within land. Registered 11.8.2000 at 8.20 hrs.

Second Schedule (cont) CT 1596-392 orig and dup.

EASEMENT H523108. A right to use the wall and footings erected upon the portion of the within land marked 'A' on the map in the margin for the purpose of supporting the building erected upon Lot 24 on Diagram 60011, together also with the right to enter upon the said portion for the purpose of maintaining the said wall and footings, all as set out in the said Easement is granted to the proprietor or proprietors for the time being of Lot 24 on the said Diagram. Registered 11.8.2000 at 8.20 hrs.

Lodging Party ANZ

28.8.2000 @ 8:04AM

SERVICE DELIVERY DIVISION, DOLA	Amount	
	\$	c
DEALINGS: H 523108	35	00
Application H 535101		
Joe Caminiti		
TOTAL \$	35	00

Fees

Assur. Fund

DUPLICATE
This copy
to be retained by Cashier

97689

LT 24

Appendix 3 Development Plans

LEGEND	
	Subject Site (Lot 25)
6	Existing Lot Number
	Existing Lot Boundary
Lot A	Proposed Lot Number
	Proposed Lot Boundary
110	Existing Lot Dimensions
110	Proposed Lot Dimensions
	Common Access
	Existing Level Contour





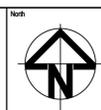
LOCALITY PLAN
SCALE 1:500



SURVEY PLAN
SCALE 1:200

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Rev No.	Description	Date	By
C	SITE AREA-EASEMENT COLLIER RD MEDIAN DA ISSUE	13.02.17	RFH
B		03.02.17	RFH
A		13.12.16	RFH



Project: PROPOSED 7ELEVEN CONVENIENCE STORE
300 COLLIER ROAD, BASSENDEAN WA
Client: 7-ELEVEN

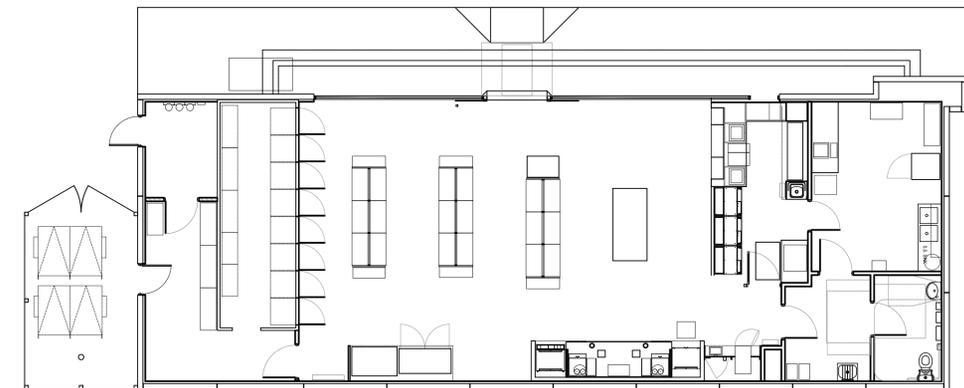
Sheet: LOCALITY PLAN SURVEY PLAN

DR:	RFH	
DN:	PARA	
DATE:	DECEMBER 2016	
SCALE:	AS SHOWN	
APPROVED:		
<small>39 Woodley Rd Glen Iris WA 6111 439 897946</small>		<small>6111 439 897946</small>
<small>1616-TP1</small>		

SCHEDULE OF AREAS		
TOTAL SITE AREA:	1828m ²	100%
7-ELEVEN TENANCY:	193m ²	10.5%
BINS:	16m ²	0.9%
FUEL CANOPY AREA:	304m ²	16.7%
CAR PARKING SPACES:	17 - inc. bowser bays.	
LANDSCAPING AREA:	198.5m ²	10.86%

SIGNAGE LEGEND	
S1	EXTERNALLY ILLUMINATED SID SIGN
S2	EXTERNALLY ILLUMINATED DIRECTIONAL SIGN
S3	NON ILLUMINATED AIR AND WATER SIGN
S4	NON ILLUMINATED RETAIL BUILDING
S5	EXTERNALLY ILLUMINATED FUEL CANOPY FASCIA
S6	EXTERNALLY ILLUMINATED TOWER SIGN
S7	NON ILLUMINATED WINDOW DECALS
S8	NON ILLUMINATED POSTER HOLDER
S9	EXTERNALLY ILLUMINATED SCROLLING SIGN

REFER TO SHEET TP4 FOR SIGNAGE REFERENCE ELEVATIONS AND DETAILS



SITE PLAN
SCALE 1:200

7-ELEVEN FLOOR PLAN
SCALE 1:100

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Rev No.	Description	Date	By
D	SITE AREA-EASEMENT	13.02.17	RFH
C	COLLIER RD MEDIAN	03.02.17	RFH
B	RETAINING RL'S	19.12.16	RFH
A	DA ISSUE	13.12.16	RFH

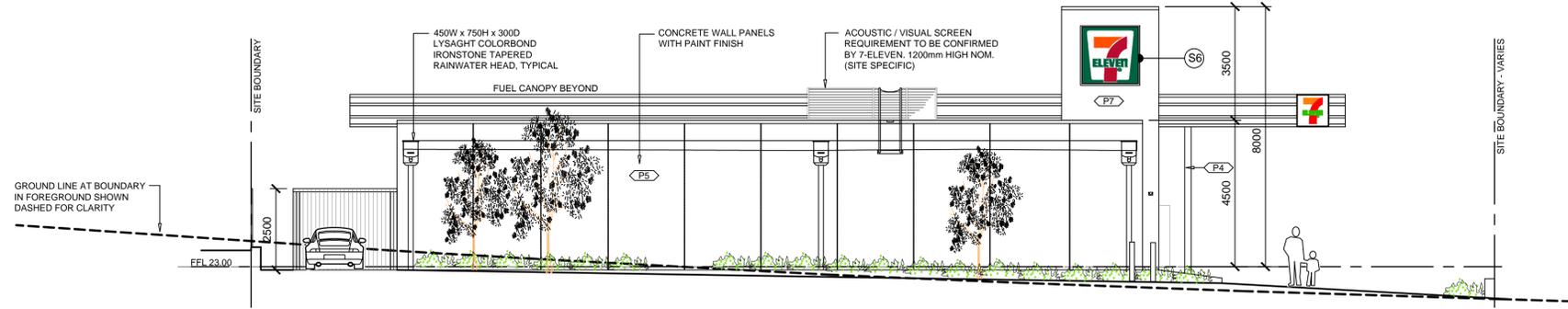


Project: PROPOSED 7-ELEVEN CONVENIENCE STORE
300 COLLIER ROAD, BASSENDEAN WA
Client: 7 ELEVEN

Sheet: LAYOUT PLANS

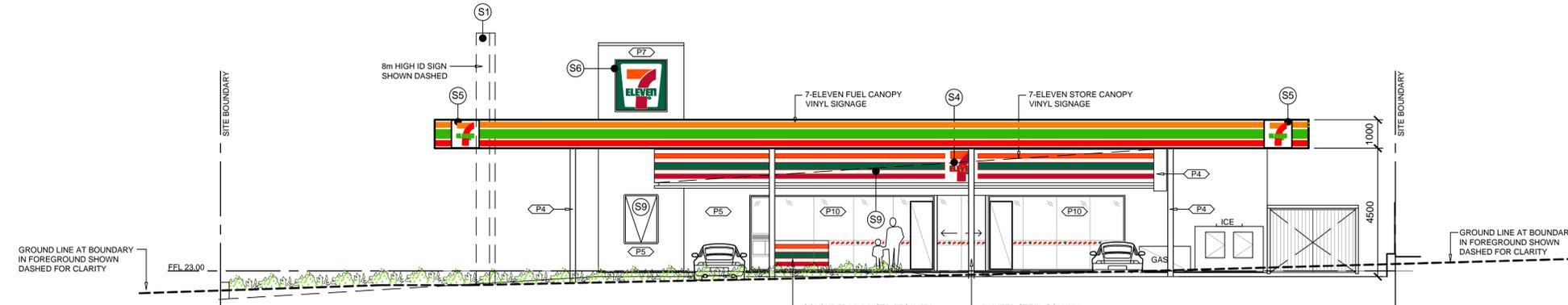
Clk:	RFH
Dn:	PARA
Date:	DECEMBER 2016
Scale:	AS SHOWN
Surv/Stan:	33 Woodley Rd Glen Iris WA 6111 439 89794
Approved:	Drawing

1616-TP2

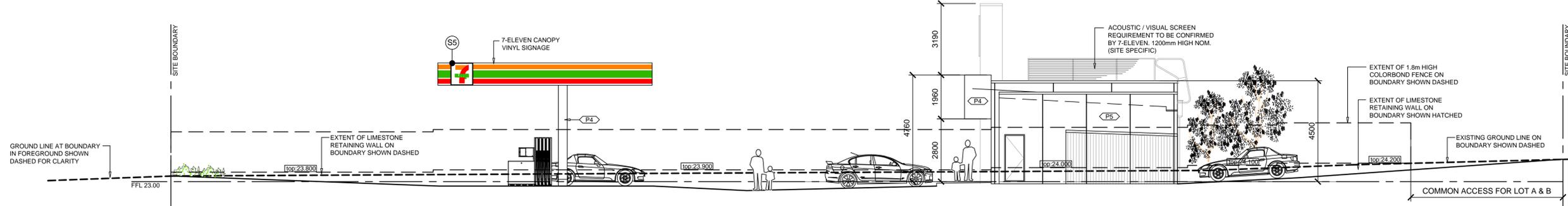


SOUTH EAST ELEVATION (convenience store)
Scale 1:100

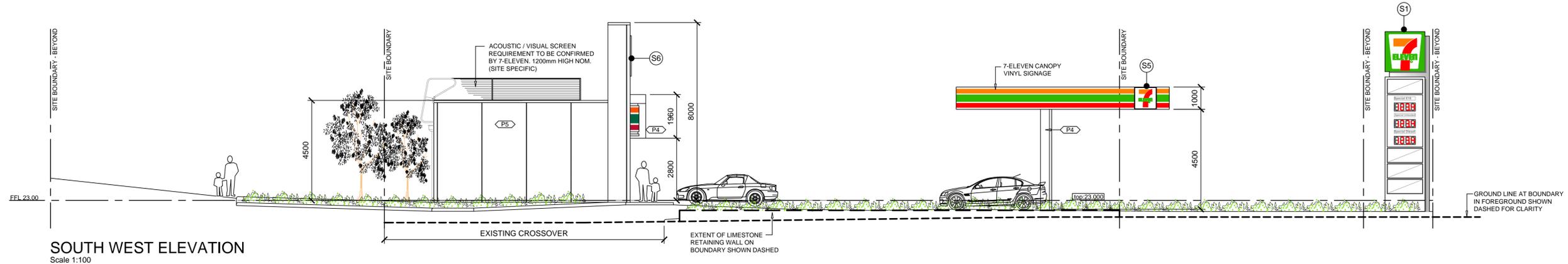
CODE	FINISH SPECIFICATION	LOCATION
P2	150x150 ROUGH SAWN POSTS COLOUR: CREAM	VERANDAH POSTS FASCIA & GUTTERS
P3	CORRUGATED IRON COLOUR: COLORBOND SLATE GREY	ROOF CLADDING
P4	DULUX WEATHERSHIELD X10 GLOSS ACRYLIC COLOUR: VIVID WHITE PW1H9	7-ELEVEN PYLON POLES, LIGHTING POLES, VENT PIPES, PETROL CANOPY COLUMNS & DOWNPIPES
P5	TEXTURED (ROUGH) FINISH GREY COLOUR	BUILDING - EXTERNAL CONCRETE WALL PANELS
P6	DULUX WEATHERSHIELD X10 GLOSS ACRYLIC COLOUR: BLACK	STEEL CANOPY COLUMNS AND BOLLARDS 300mm ABOVE GROUND LEVEL
P7	TIMBER LOOK CLADDING TO SIGNAGE TOWER	BUILDING TOWER CONCRETE WALL PANELS
P10	CLEAR GLAZING IN A POWDERCOATED ALUMINIUM FRAME WITH FRAMELESS MULLIONS	SHOPFRONT



NORTH WEST ELEVATION (fuel canopy)
Scale 1:100



NORTH EAST ELEVATION
Scale 1:100



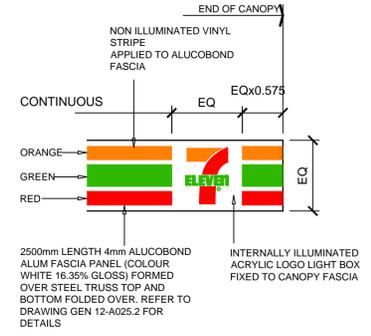
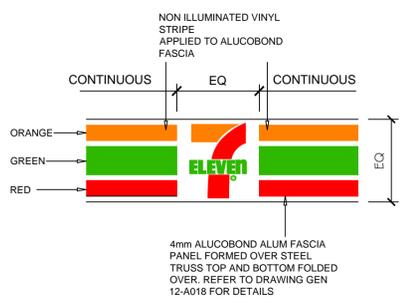
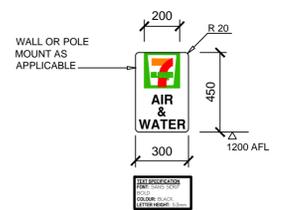
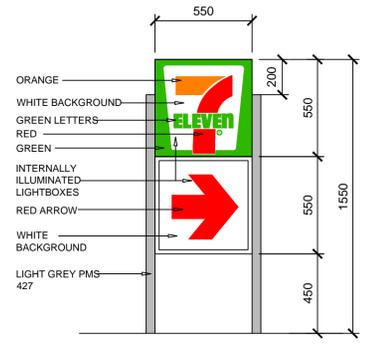
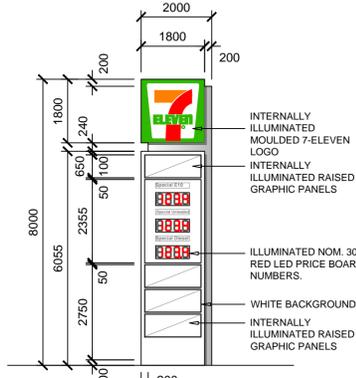
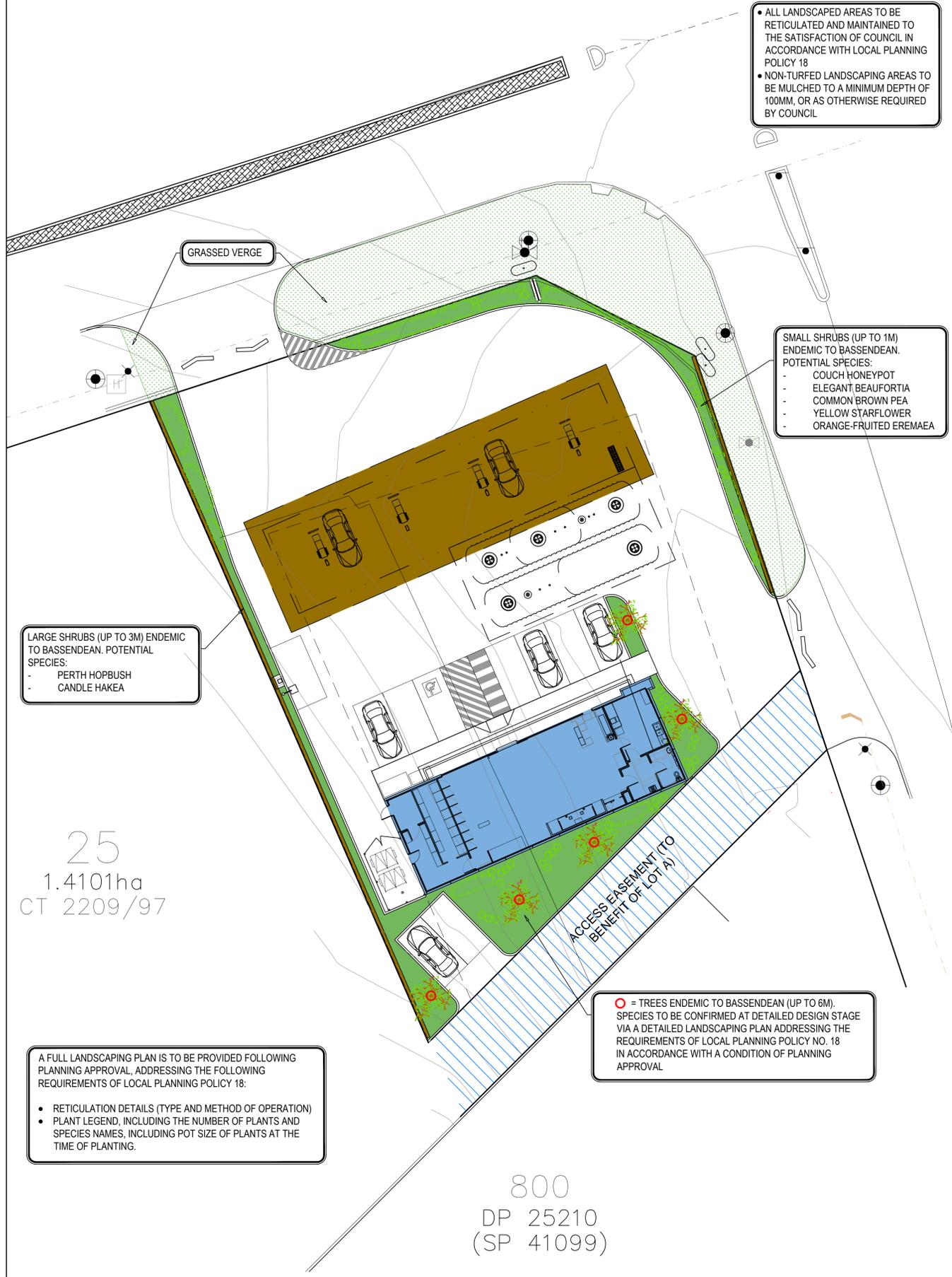
SOUTH WEST ELEVATION
Scale 1:100

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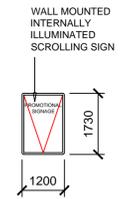
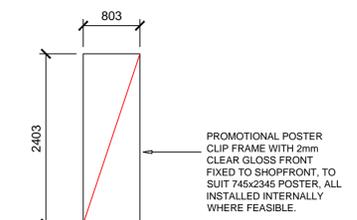
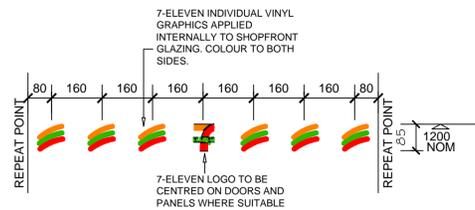
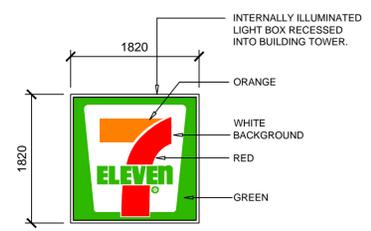
Rev No.	Description	Date	By
B	RETAINING RL'S	19.12.16	RFH
A	DA ISSUE	13.12.16	RFH

North	Project	PROPOSED 7ELEVEN CONVENIENCE STORE 300 COLLIER ROAD, BASSENDEAN WA
	Client	7 ELEVEN
	Sheet	ELEVATIONS

Clk	PARA	FHM CONSULTING
Dn	AS SHOWN	
Date	DECEMBER 2016	
Scale	AS SHOWN	
Surv/Stan	33 Woodley Rd Glen Iris WA	01-11-439 89794
Approved	1616-TP3	



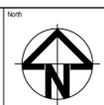
COLOUR LEGEND			
COLOUR	PANTONE	NON-ILLUM.	ILLUM.
ORANGE	PMS 165	7725-14	3630-13
GREEN	PMS 341	7725-56	3630-26
RED	PMS 485	7725-13	3630-43



LANDSCAPE PLAN
SCALE 1:200

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--

Rev No.	Description	Date	By
C	SITE AREA EASEMENT	13.02.16	RFH
B	VERGE GRASS PLANTING GUIDANCE	03.02.16	RFH
A	PLANTING GUIDANCE	19.12.16	RFH
	DA ISSUE	13.12.16	RFH



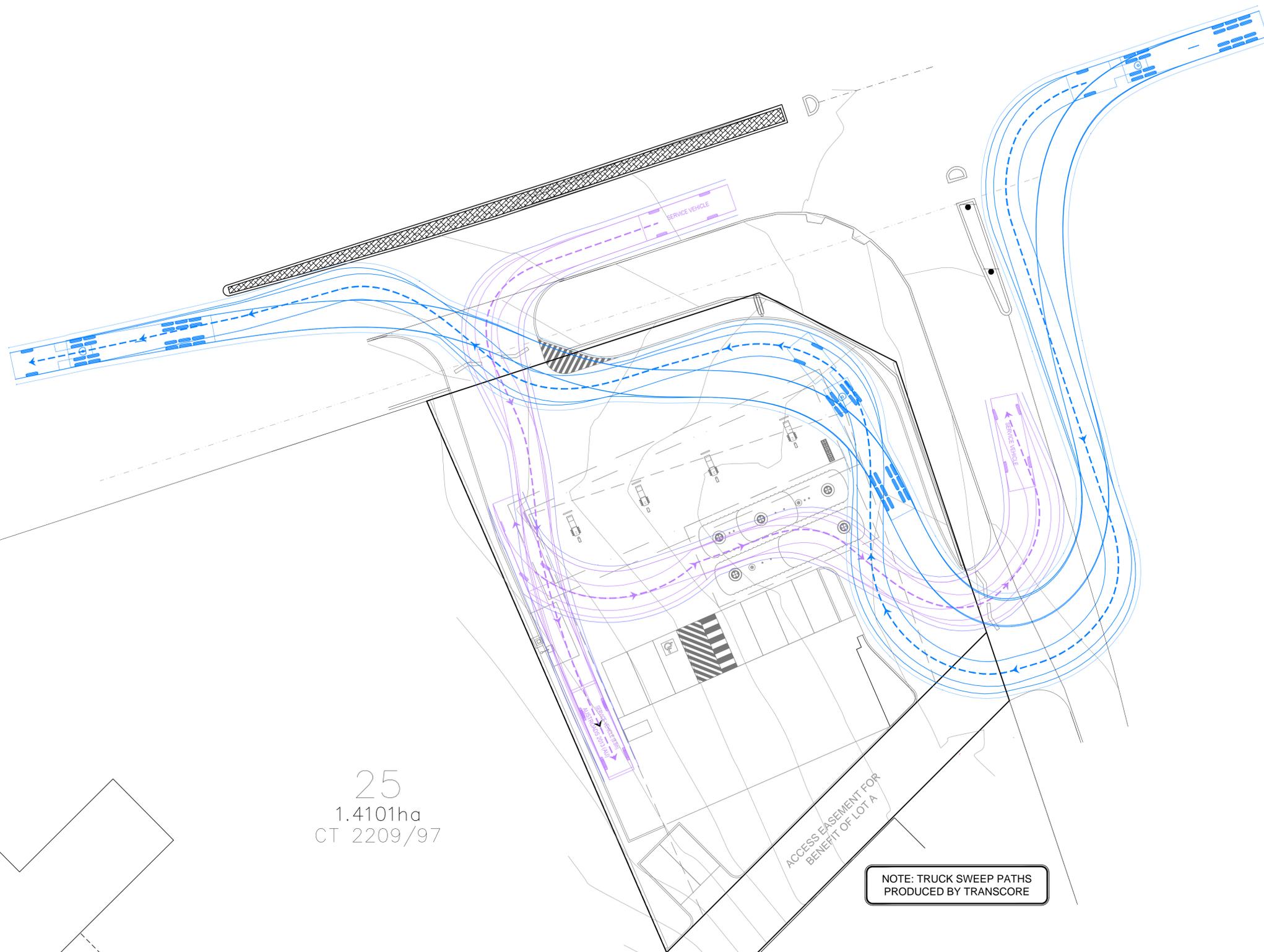
Project: **PROPOSED 7ELEVEN CONVENIENCE STORE**
300 COLLIER ROAD, BASSENDEAN WA

Client: **7 ELEVEN**

Sheet: **LANDSCAPE PLAN**
SIGNAGE DETAILS

Drawn: **RFH**
Checked: **PARA**
Date: **DECEMBER 2016**
Scale: **A1 AS SHOWN**





25
1.4101ha
CT 2209/97

800
DP 25210
(SP 41099)

NOTE: TRUCK SWEEP PATHS
PRODUCED BY TRANSORE

TRUCK PATH PLAN
SCALE 1:200

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Rev No.	Description	Date	By
C	LOT AREA - EASEMENT	13.02.17	RFH
B	COLLIER RD MEDIAN	03.02.17	RFH
A	DA ISSUE	13.12.16	RFH



Project **PROPOSED 7ELEVEN CONVENIENCE STORE
300 COLLIER ROAD, BASSENDEAN WA**

Client **7 ELEVEN**

Sheet **TRUCK PATH**

Clk	RFH
Dn	PARA
Date	DECEMBER 2016
Scale@ A1	AS SHOWN
Surv/Stan	
Approved	



Appendix 4 Transport Impact Assessment



**Proposed 7- Eleven - Cnr Collier Rd
& Alice Street, Bassendean**

300 Collier Road, Bassendean

Transport Impact Assessment

PREPARED FOR:
Collier Rd WA Pty Ltd ATFT
Collier Road WA Unit Trust

February 2017

Document history and status

Author	Revision	Approved by	Date approved	Revision type
Ali Rasouli	r01	B Bordbar	19/11/16	Draft
Ali Rasouli	r01a	B Bordbar	13/12/16	Final
Ali Rasouli	r01b	B Bordbar	14/02/17	Revised Final

File name: t16.147arr01b.docx

Author: Ali Rasouli

Project manager: Behnam Bordbar

Client: Collier Rd WA Pty Ltd ATFT Collier Road WA Unit Trust

Project: Proposed 7- Eleven - Cnr Collier Rd & Alice Street,
Bassendean

Document revision: r01b

Project number: t16.147

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The Client alone shall have a license to use the documents referred to above for the purpose of completing the Project, but the Client shall not use, or make copies of, such documents in connection with any work not included in the Project, unless written approval is obtained from the Consultant or otherwise agreed through a separate contract.

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1.0 Summary

This Transport Impact Assessment (TIA) is prepared on behalf of Collier Rd WA Pty Ltd ATFT Collier Road WA Unit Trust with respect to the proposed 7- Eleven development to be located at 300 Collier Road in the Town of Bassendean.

The development site as shown in Figure 1 forms a portion of 300 Collier and is zoned ‘*General Industry*’ in the Town of Bassendean Local Planning Scheme.

As part of the development a new left in/ left out crossover on Collier Road proposed and the existing full movement crossover on Alice Street will also be used to service the proposed development. The crossover arrangement is required to facilitate fuel tanker movements and it was presented and discussed in a meeting with the Town of Bassendean’s Engineering Department on 17th July 2016. Following the meeting the proposed access arrangement was supported by the Engineering Department and was ‘in principle’ approved.

The aim of this TIA is to estimate the traffic which will be generated by the proposed development and to establish and assess the impact of the traffic pattern on the surrounding road network.

Accordingly, traffic generation of the proposed development was estimated and capacity assessment of the intersection of Collier Road/ Alice Street was undertaken.

2.0 Introduction

This Transport Impact Assessment (TIA) is prepared with respect to the proposed 7-Eleven development to be located at 300 Collier Road in the Town of Bassendean. A map showing the development site in context of the surrounding area is included in Figure 1.

The development site forms part of a bigger lot and is proposed to be subdivided. It is located at the south-western corner of the intersection of Collier Road and Alice Street.

The existing access and egress system for 300 Collier Road comprises one full movement crossover each on Collier Road and Alice Street.

Key issues that will be addressed in this report include the traffic generation of the proposed development, proposed access and egress arrangement, capacity assessment of Collier Road/ Alice Street intersection and fuel tanker movements to and from the proposed development.



Figure 1: Location of the development site

3.0 Existing Situation

3.1 Existing Site Use, Access and Parking

The site is zoned “General Industry” in the Town of Bassendean Local Planning Scheme. The development site is located within a primarily an industrial area and forms portion of 300 Collier Road.

The existing access/egress system and the intersection of Collier Road/Alice Street are shown in Figure 2.

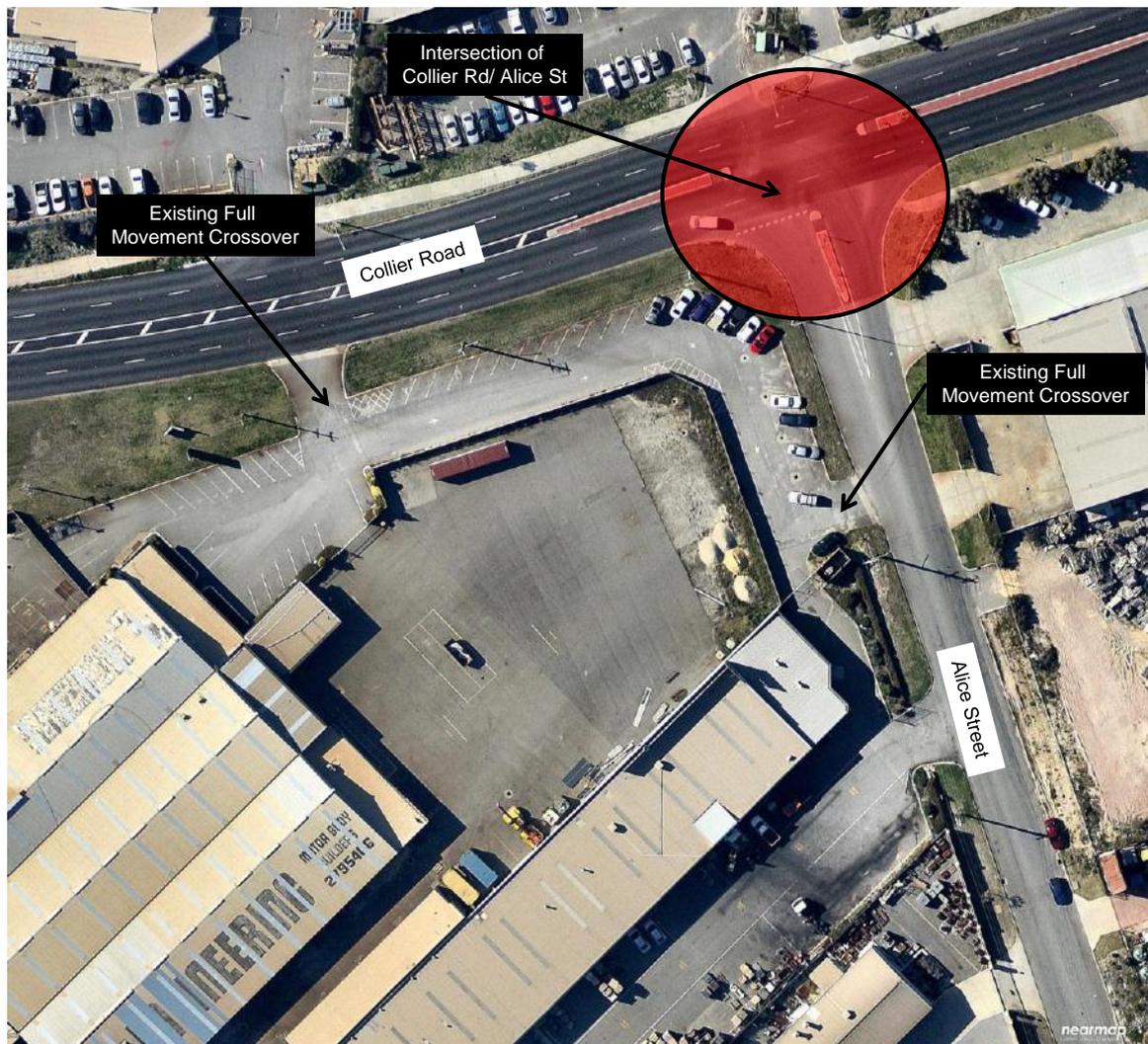


Figure 2: Existing intersection and access/egress arrangements

Existing crossovers description:

- Alice Street Crossover is an existing ~8m full movement crossover which is shown in Figure 3;

- Collier Road crossover is an existing ~10m full movement crossover and is shown in Figure 4.



Figure 3: Existing Alice Street full movement crossover – looking west



Figure 4: Existing Collier Road full movement crossover – looking south

The proposal intends to utilise the existing full movement crossover on Alice Street and creates a new left in/left out crossover on Collier Road. This crossover arrangement for the proposed development is required to facilitate the fuel tanker movements. This arrangement has been discussed in a meeting on 17th July 2016 with the Engineering Department at Town of Bassendean and “in principle” support was obtained.

3.2 *Surrounding Road Network and Traffic Management on Frontage Roads*

Collier Road is a dual divided carriageway and operates under a speed limit of 70km/hr in this vicinity (refer Figure 5 and Figure 6).



Figure 5: Collier Road looking east in the vicinity of the development site



Figure 6: Collier Road looking west in the vicinity of the development site

Collier Road is classified as a *Distributor A* road in the Main Roads WA Metropolitan Functional Road Hierarchy. It is covered by *Other Regional Road (ORR – Blue Road)* reservation in the MRS. A copy of the MRS map for this vicinity is shown in Figure 7.



Figure 7: MRS Map

Alice Street in the vicinity of the development site is a 12m two lane undivided road with no pedestrian facilities on either side of it (refer to Figure 8 Figure 9).

Alice Street is classified as *Access Road* in the Main Roads WA *Metropolitan Functional Road Hierarchy* and operates under a default speed limit of 50km/h.



Figure 8: Alice Street adjacent to the development site - looking north



Figure 9: Alice Street adjacent to the development site - looking south

Collier Road and Alice Street form a give way controlled T-intersection in the vicinity of the development site (Figure 10).



Figure 10: Collier Road/ Alice Street –Give Way T-Intersection

3.3 Existing Traffic Volumes

Collier Road

According to Main Roads WA traffic counts, Collier Road east of Tonkin Highway carried an Average Weekday Traffic flow of about 17,210vpd with 14% heavy vehicle in September 2015. As shown in Figure 11 the AM peak hour for Collier Road in this vicinity is recorded between 8.00AM to 9:00AM with 1,245vph and as shown in Figure 12 the PM peak hour is recorded between 3:00PM – 4:00PM with 1,500vph.

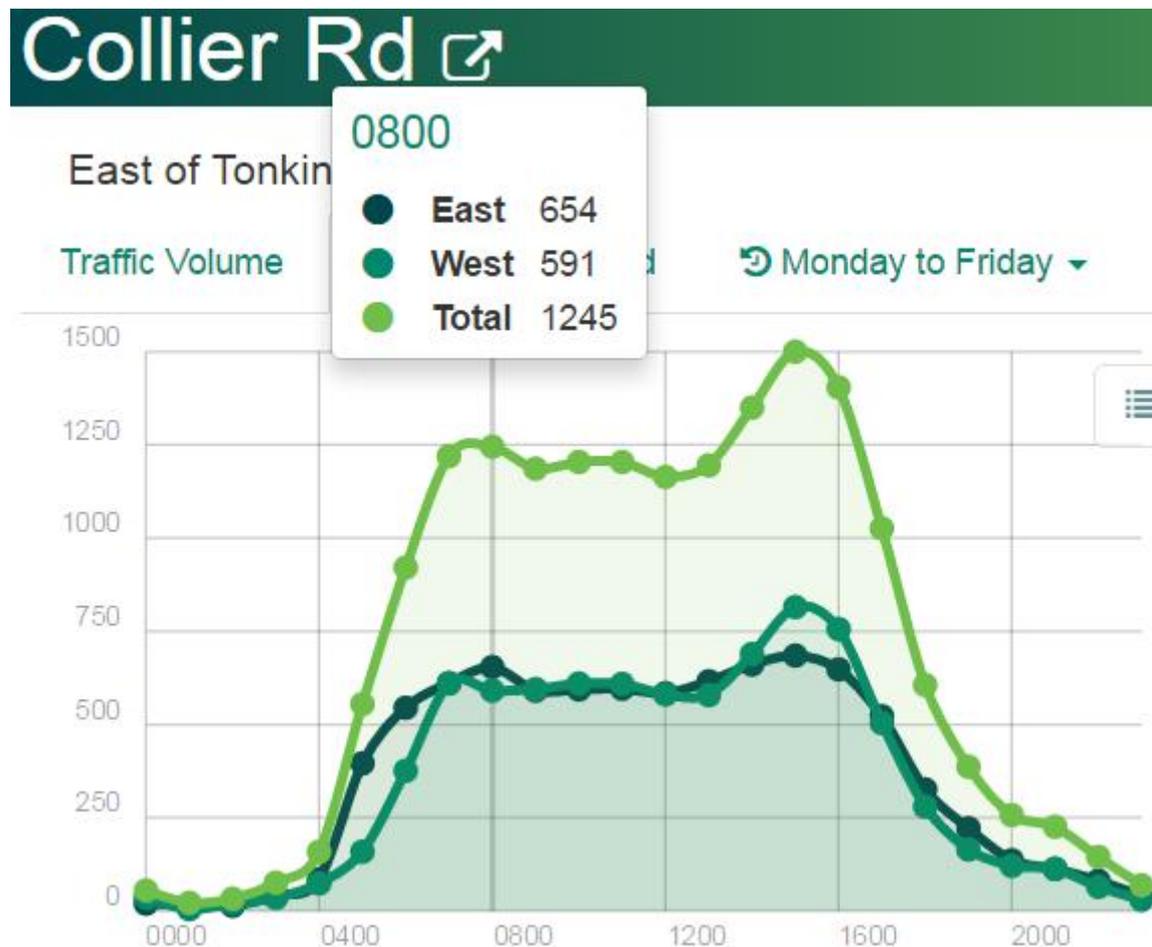


Figure 11: Collier Road Directional Split AM Peak Hour Traffic Count

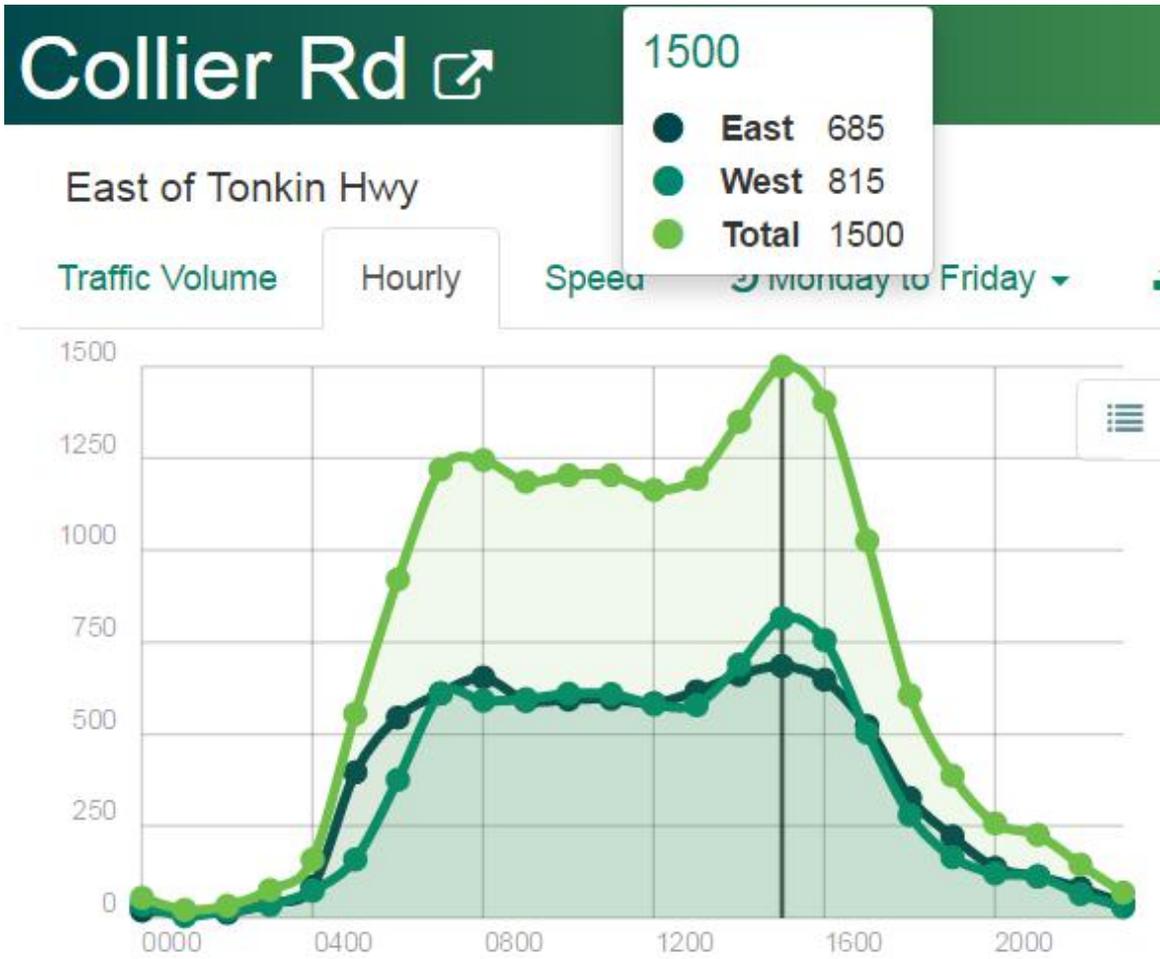


Figure 12: Collier Road Directional Split PM Peak Hour Traffic Count

Alice Street

Traffic count surveys undertaken by Transcore at the intersection of Collier Road/ Alice Street indicate that Alice Street carried 151vph and 230vph during AM and PM peak hours respectively on the day of the survey. This translates to to approximately 2,000vpd.

The existing traffic flows are detailed in Figure 21 in section 7.2 of this report.

3.4 Operation of Surrounding Intersections

Collier Road/ Alice Street Intersection

This intersection is controlled by “Give Way” and was observed to operate satisfactorily in both the AM and PM peak hours with minimal queues and delays.

A SIDRA analysis was undertaken to establish the existing and post development operational conditions for this intersection. The results of the SIDRA analysis are discussed in Section 7.4 of this report.

3.5 Public Transport Access

The development site enjoys good accessibility to public transport via bus route and nearby train stations in this vicinity. Bus route 955 along Collier Road runs in proximity of the development site and provides connectivity to nearby train and bus stations including Bassendean Train Station to the east and Morley Bus Station to the west. Furthermore, the development site is also located close to Ashfield Train Station and enjoys good connectivity to Perth CBD via Midland Train Line. Nearby public transport services are shown in Figure 13.

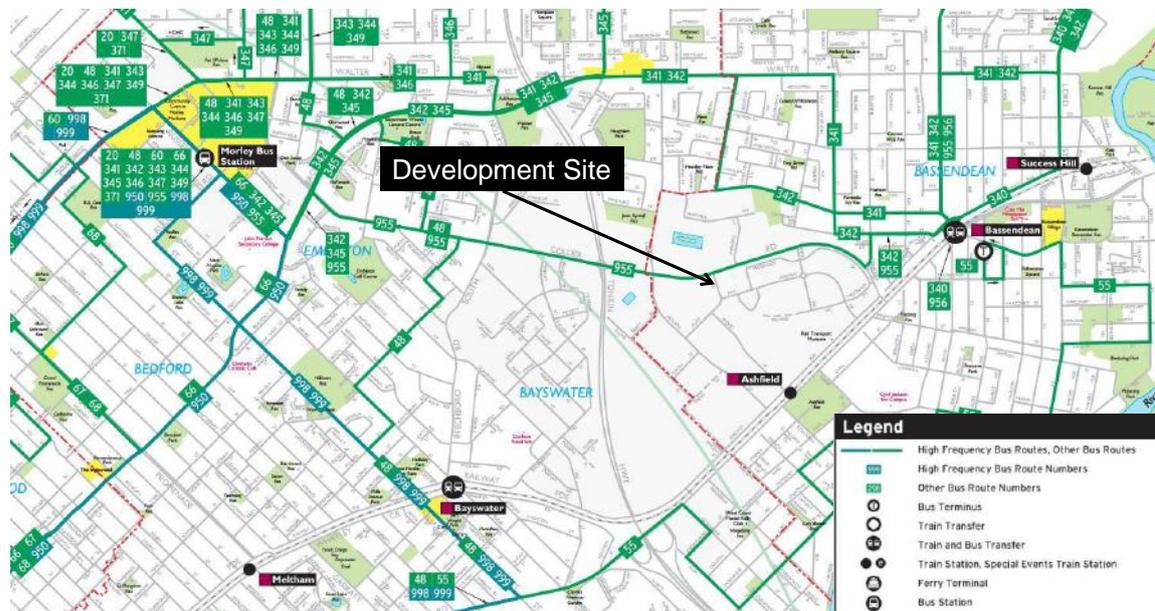


Figure 13: Existing Bus Routes

The closest bus stop is located on Collier Road about 55m to the east of the intersection of Collier Road/ Alice Street as shown in Figure 14.



Figure 14: Bus stop on Collier Road – Close to the development site

3.6 Pedestrian and Cyclist Facilities

The Department of Transport's *Perth Bike Map* series (see Figure 15) indicates that Tonkin Highway and Guildford Road are both classified as *Principal Shared Path (PSP)* routes in the vicinity of the development site.

This map also shows that Collier Road to the north of the development site entails a shared path and Railway Parade in a close proximity to the development site is classified as *Good Riding Environment*. Jackson Street to the west of the development site is also identified as a Continuous Signed Route within the Perth Bicycle Network (PBN).



Figure 15: Bike map (source: Department of Transport)

There is currently no footpaths provided on Collier Road and Alice Street in the vicinity of the development site. However, as shown in the proposed development site plan attached in **Appendix A**, 2.0m footpath is proposed along Collier Road to provide pedestrian accessibility for the proposed development.

3.7 Crash Data

Information available on the Main Roads WA website indicates that the intersection of Collier Road / Alice Street in the vicinity of the development site recorded only 1 road crash during the five-year period ending in December 2015.

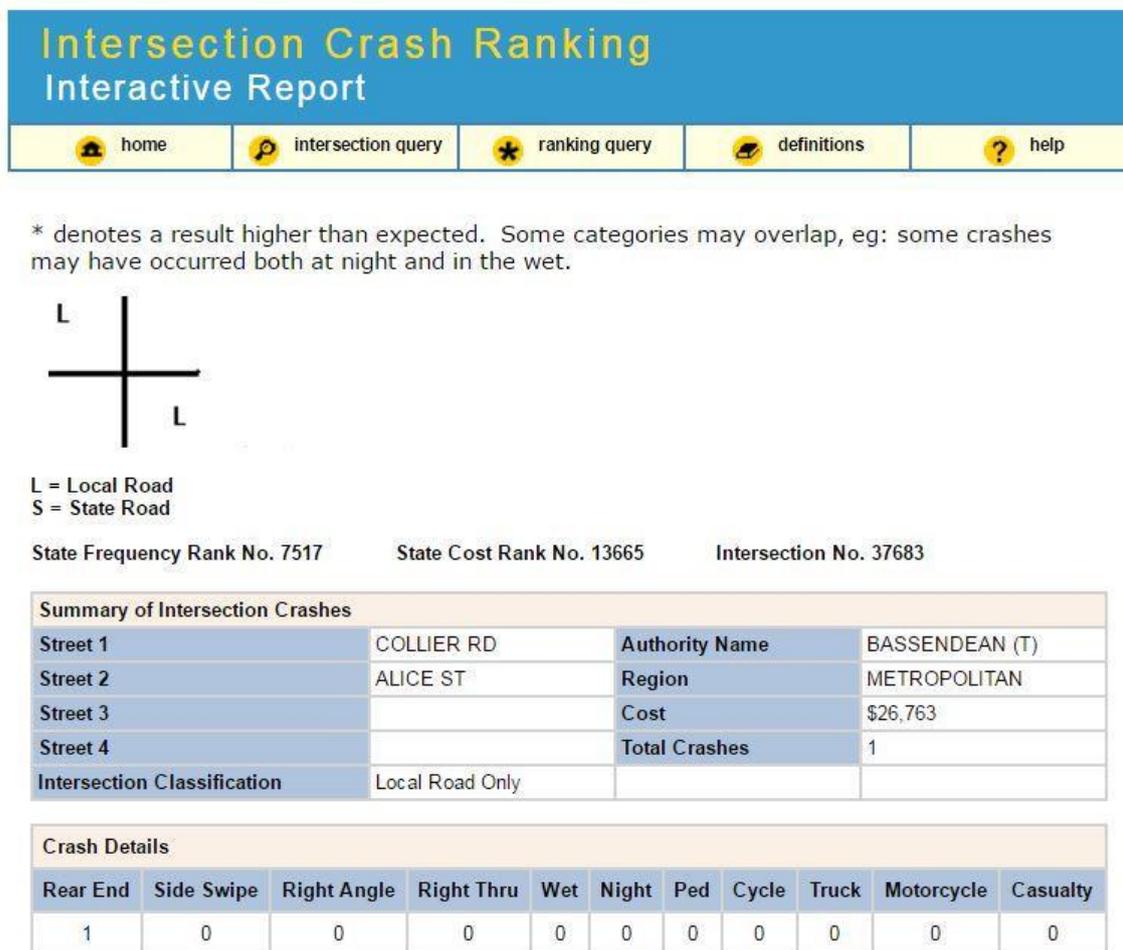


Figure 16: Crash statistics for Collier Road / Alice Street

4.0 Development Proposal

4.1 Proposed Land Uses

According to the proposed development plan Attached in Appendix A the proposal includes a 7- Eleven development comprising:

- ✚ 8 fuel filling points for private vehicles;
- ✚ A convenience store building;
- ✚ 7 car parking bays including 1 accessible bay to the front of the proposed convenience store plus two staff car parking bays to the rear of the proposed convenience store;
- ✚ 1 Air and Water Bay; and,
- ✚ A loading bay.

It is proposed to provide 10 on-site car parking bays including one accessible, 2 staff parking bays and 1 Air and Water bay. A loading bay is also proposed at the western end of the proposed convenience store. 8 vehicles can use the fuel bowsers simultaneously. Therefore, minimum of 19 vehicles can comfortably be accommodated on-site.

According to the advice provided to Transcore the access easement provided to the south of the proposed development will be as part of the adjacent Lot (Lot B) and the proposed development will have the benefit to use it.

A copy of the proposed development site plan is included in Appendix A.

4.2 Proposed Access

It is proposed to provide a new left in/ left out crossover on Collier Road and also to utilise the existing Alice Street crossover to service the proposed development. The creation of the Collier Road crossover is essential for effective and convenient traffic circulation for the proposed development particularly for the fuel tanker movements. With respect to the Collier Road new left in/ left out crossover the it is proposed to extend the solid median island on Collier Road to restrict the crossover to left in/ left out only. The design of the median island extension will be addressed during the detailed design stage of the project.

Based on the advice provided to Transcore 19.0m fuel tankers will be used for fuel delivery to this site. Tankers will enter the site from Alice Street crossover, access the fill point and then exit the site onto Collier Road via a left turn movement. Turn path analysis has been undertaken to review the fuel tanker movements. A copy of the results of the turn path analysis is attached in Appendix B.

Deliveries and waste collections will also be accommodated within the development site. The loading bay provided at the western end of the proposed convenience store will accommodate the delivery vehicles and waste collection trucks.

Fuel tankers and service vehicles are expected to access the site no more than 3-4 times a week and generally outside the peak road network and peak site activity periods. As service vehicles are expected to access the site outside the peak operating times, no traffic conflicts between customer, employee and service vehicles are expected.

Main Roads WA Driveway Policy Compliance Check:

Section 3.3.5 MRWA's Driveway Policy deals with service station specific conditions. This clause states that *'' Service stations on a corner lot may have one driveway up to 11.0m wide to a State road and another to the minor road. Those not on a corner may have two driveways, each up to 11.0m wide. As a general rule, median openings solely to service station driveways shall not be provided''*.

The access/egress system for the proposed development is in-line with MRWA's Driveway Policy for service stations on following grounds:

- The proposed development is located at the corner of Collier Road (a major road) and Alice Street (a minor road);
- Only one crossover which is a left in/ left out crossover is proposed on Collier Road (major road) and the other crossover is an existing full movement crossover on Alice Street (minor road). and,
- The Collier Road left in/ left out crossover is proposed to be 10m.

5.0 Surrounding Transport Networks

Based on the advice provided by the Engineering Department of the Town of Bassendean, no road network modification/upgrades are proposed or planned in the immediate vicinity.

Other future road upgrade in the locality:

The existing priority-controlled intersection of Collier Road and Jackson Street is proposed to be signalised as part of the North Link WA project. As part of this project the existing Tonkin Highway/Collier Road intersection is proposed to become grade separated.

Discussions with Main Roads WA have also indicated that there are future plans to duplicate the bridge north of the Tonkin Highway southbound off-ramp to Guildford Road. As part of this duplication, Tonkin Highway between Guildford Road and Collier Road would be realigned.

6.0 Integration with Surrounding Area

The proposed development land use is in line with existing and planned land uses in the locality.

7.0 Traffic Assessment

7.1 Assessment Period

The selection of the assessment period for the proposed development considered average transaction data for a number of established, operating and comparable 7-Elevens provided to Transcore.

As demonstrated in Figure 17, weekday customer patronage for a typical 7-Eleven exhibits a morning peak at around 6:00am, then decreases slightly and remains fairly consistent throughout the day before exhibiting a second peak at around 4:00pm.

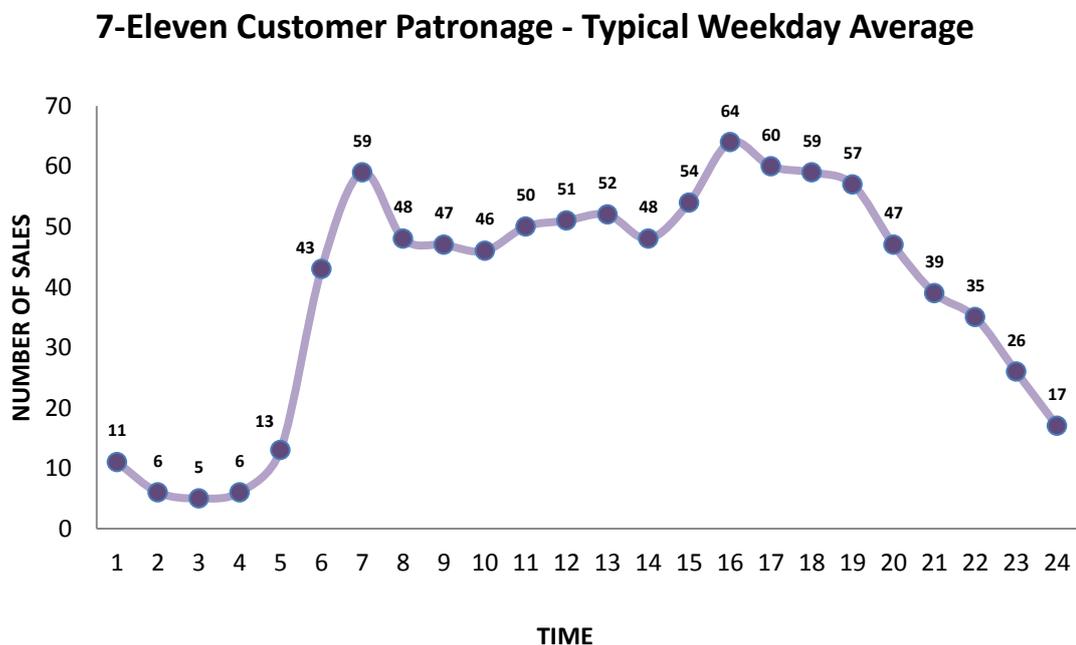


Figure 17: Hourly customer demand profile (average weekday)

A review of the existing traffic counts for the surrounding road network and the sales profile presented in Figure 17, suggests that the combination of the traffic expected to be generated by the development and the peak road network traffic is likely to result in the greatest demand on the road network during the typical weekday morning and afternoon peak hours between 8:00–9.00am and 3:00-4:00pm. As such, trip generation is estimated and traffic analysis is undertaken for these periods.

7.2 Trip Generation and Distribution

Estimated existing traffic generation:

The existing turning traffic at the Alice Street crossover was established by traffic surveys undertaken by Transcore as documented in section 7.3 of this report.

Proposed 7-Eleven traffic generation

The traffic volumes likely to be generated by the proposed 7-Eleven has been estimated based on the sales transaction data presented in Figure 17.

Review of the comparable store transaction data indicates that the proposed development is likely to attract approximately 942 transactions during an average 24-hour weekday with 47 transactions during weekday AM peak period and 65 transactions during the weekday PM peak hour.

Accordingly, the vehicle trip generation for the proposed development is estimated to be approximately 1,790 vpd (vehicles per day) with about 90vph during the AM and about 126vph during the PM peak hours.

The following conservative trip rate assumptions were adopted in deriving the daily vehicle trip generation:

- ✚ 95% of transactions result in 2 vehicle trips (1 trip in and 1 trip out).
- ✚ 5% of transactions are assumed to be from walk-ins or multiple transactions per vehicle which do not contribute to trip generation.

Trips associated with the proposed fuel-filling station and, to a lesser degree, the proposed convenience store also comprise passing-trade trips (i.e. trips already on the road network and not specifically generated by the proposed development). Based on available information, the passing trade component can be as high as 75% of the total generated traffic. A passing trade component of 70% is assumed for the proposed 7-Eleven development.

Therefore, it is estimated that the proposed development would generate approximately **+537** additional daily trips on the road network of which **+28vph** would be during AM and **+38vph** are expected during the afternoon peak hours.

These trips include both inbound and outbound vehicle movements. It is anticipated that most vehicle types would be passenger cars and to a lesser extent 4WDs.

The traffic distribution detailed in Table 1 was based on the following directional split assumptions:

- ✚ Morning (AM) and Afternoon (PM) peak split estimated at 50%/50% inbound/outbound for all land uses.

The proposed development is estimated to generate the following number of peak hour trips:

Table 1: Peak hour trips for the proposed development

Time period	Direction	Total Peak Hour Trips	
		Split	Total
AM Peak	Inbound	45	90
	Outbound	45	
PM Peak	Inbound	62	124
	Outbound	62	

The distribution of traffic to and from the proposed development has been evaluated by considering the catchment area of the proposed development and the identified traffic routes. The traffic distribution from new proposed Collier Road crossover is assumed to be left in/ left out only.

Two traffic distributions have been modelled for the PM peak hour:

- ✚ Passing trade traffic: 63vph during AM peak hour and 87vph during PM peak hour distributed as detailed in Figure 18.
- ✚ Non passing trade traffic: 27vph during AM peak hour and 37vph during PM peak hour distributed as detailed in Figure 19.

The total traffic modelled for the immediately post development scenario is detailed in Figure 20.

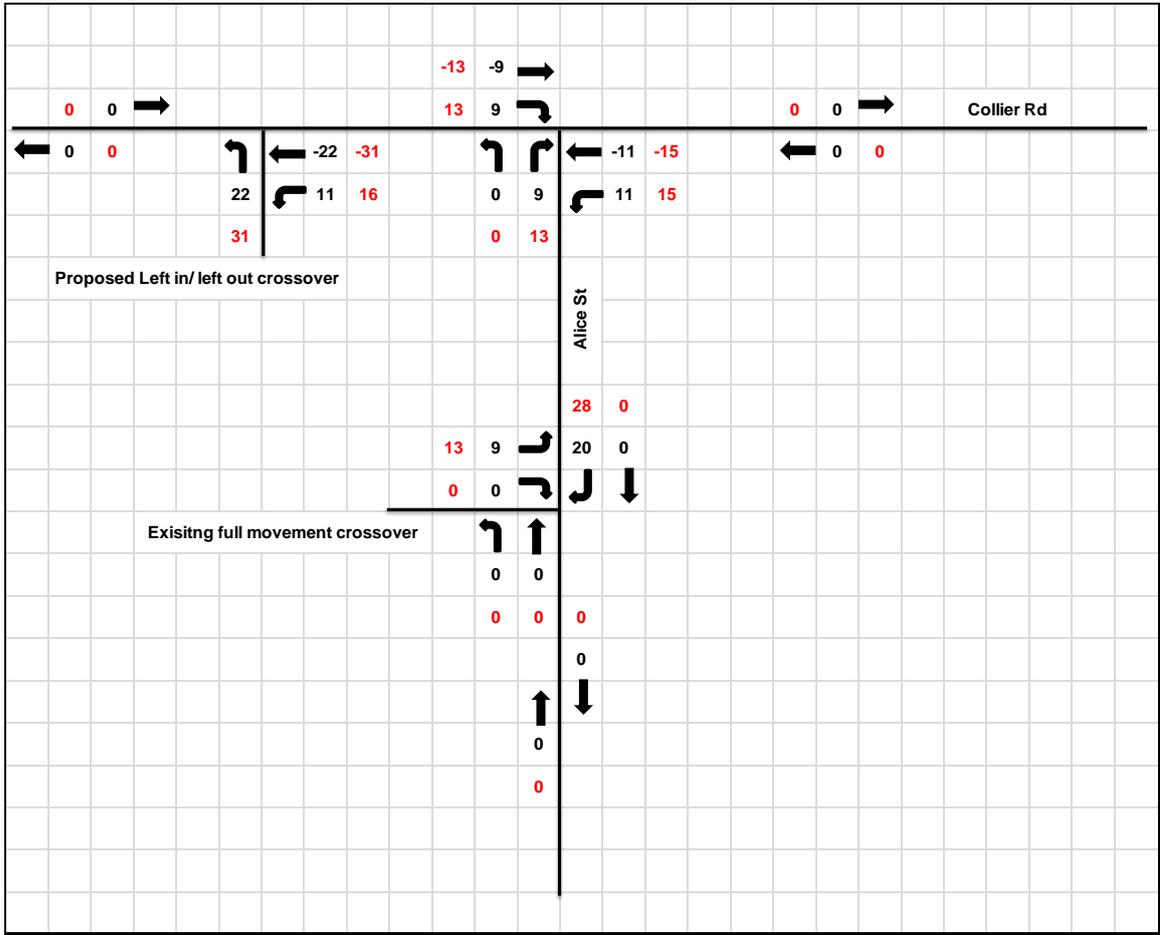


Figure 18: Passing trade peak hour traffic diverted by the proposed development - AM and PM hour

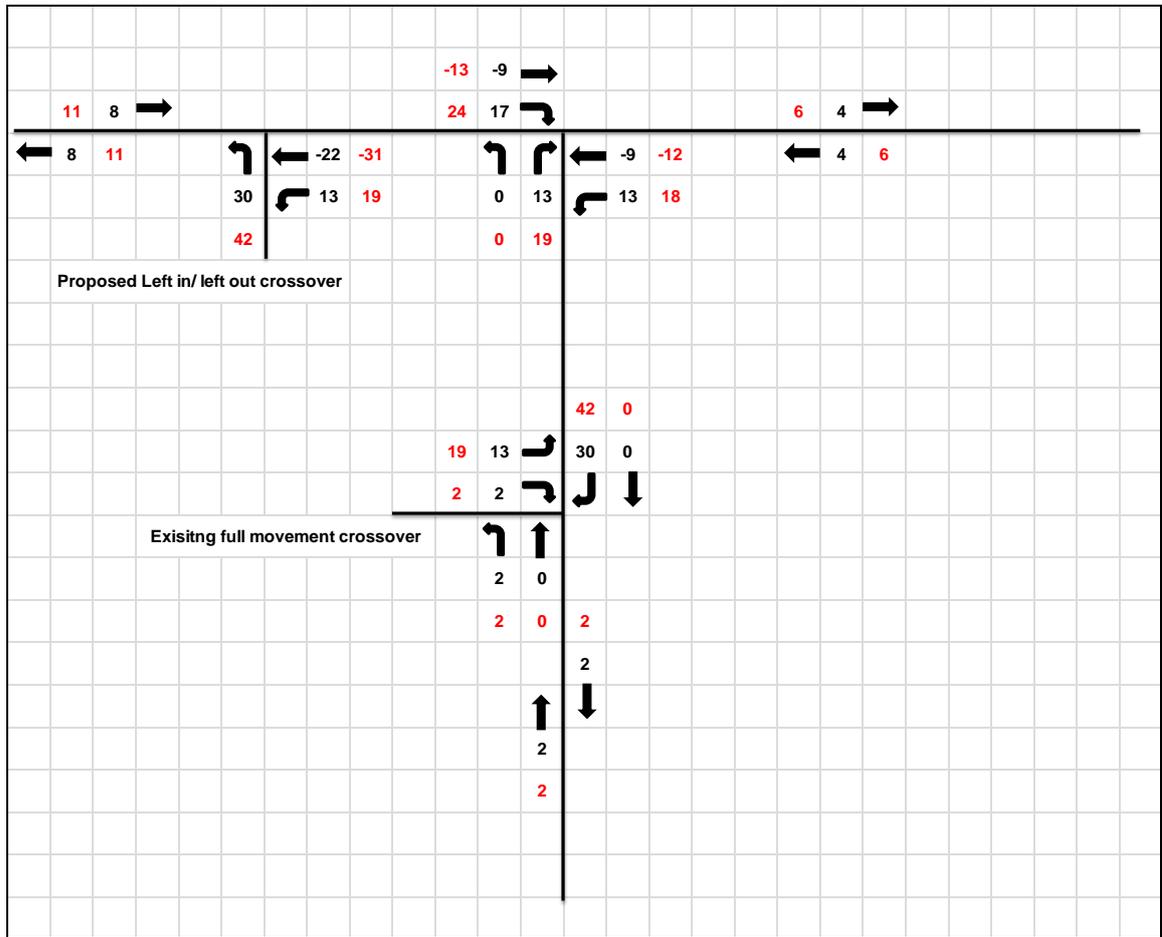


Figure 20: Net combined AM and **PM** peak hour traffic generated by the proposed development

7.3 Traffic Flows

Existing traffic flows along this section of Collier Road were obtained from Main Roads WA traffic count data. The existing turn traffic volumes at the intersection of Collier Road/ Alice Street and the existing Alice Street crossover were established from manual traffic count surveys undertaken by Transcore.

The existing turn traffic flows detailed in Figure 21 were collected on Wednesday 9th of November 2016 for the AM and PM peak hours from 8:00am to 9:00am and 3.00pm to 4.00pm.

The immediately post development traffic at the crossovers and the intersection of Collier Road/ Alice Street for AM and PM peak hour traffic flows immediately post-development are detailed in Figure 22.

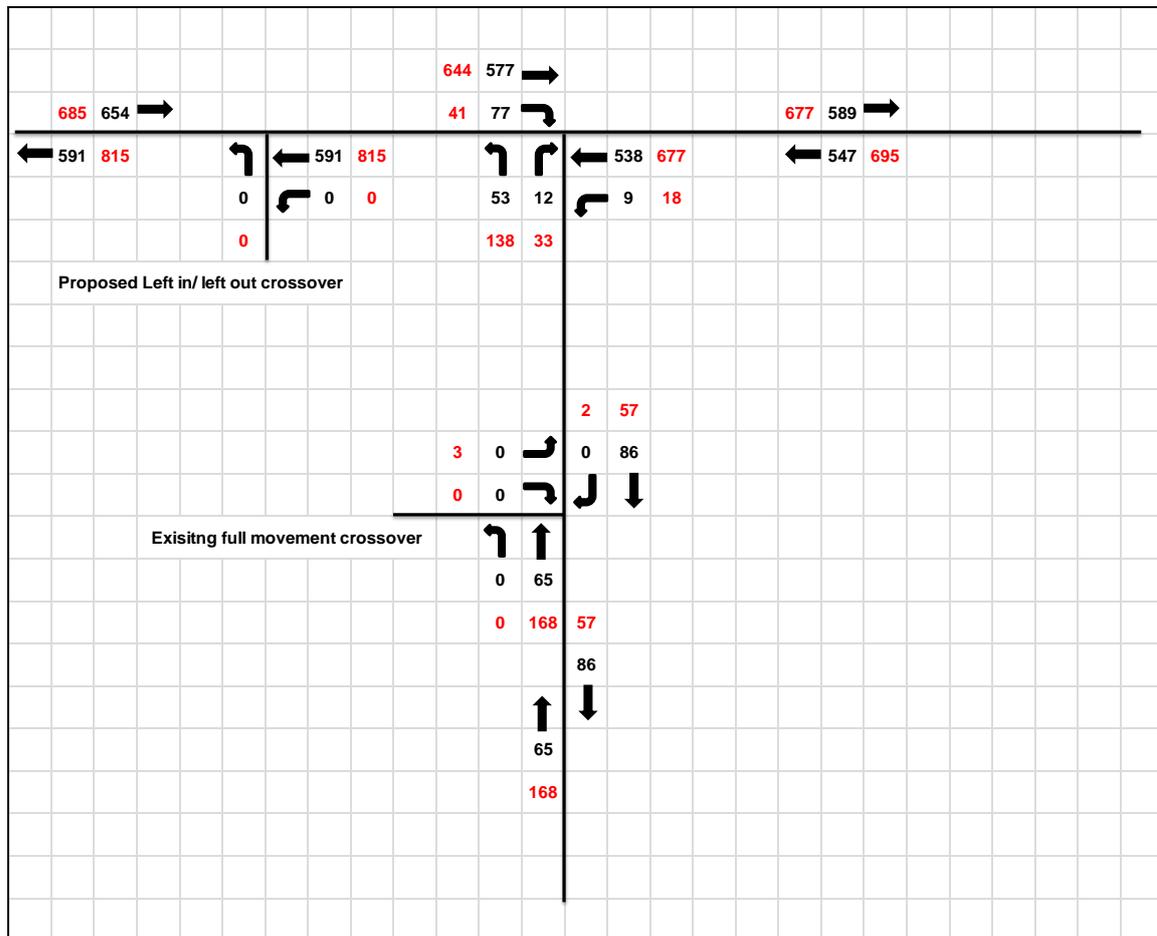


Figure 21: Existing AM and PM peak hour traffic flows

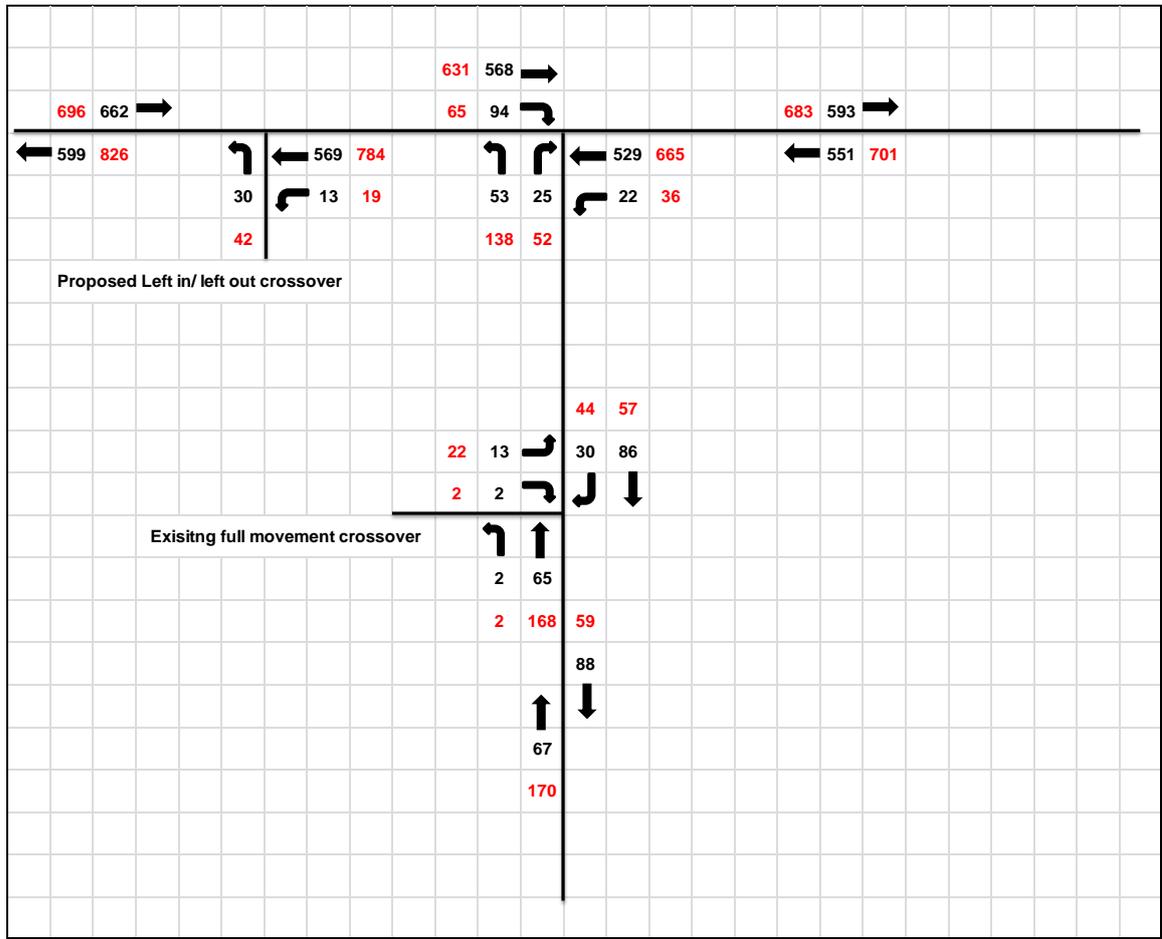


Figure 22: Total AM and **PM** peak hour traffic flows – Immediately post development scenario

7.4 Analysis of Intersections and Development Accesses

The operation of the intersection of Collier Road/ Alice Street for the existing and post development scenarios was assessed using SIDRA software.

SIDRA is an intersection modelling tool commonly used by traffic engineers for all types of intersections. SIDRA outputs are presented in the form of Degree of Saturation, Level of Service, Average Delay and 95% Queue. These characteristics are defined as follows:

- **Degree of Saturation (DoS):** is the ratio of the arrival traffic flow to the capacity of the approach during the same period. The Degree of Saturation ranges from close to zero for varied traffic flow up to one for saturated flow or capacity.
- **Level of Service (LoS):** is the qualitative measure describing operational conditions within a traffic stream and the perception by motorists and/or passengers. In general, there are 6 levels of service, designated from A to F, with Level of Service A representing the best operating condition (i.e. free flow) and Level of Service F the worst (i.e. forced or breakdown flow).
- **Average Delay:** is the average of all travel time delays for vehicles through the intersection.
- **95% Queue:** is the queue length below which 95% of all observed queue lengths fall.

The results of the SIDRA analysis are detailed in Appendix B and briefly discussed in the following paragraphs:

- The results of the SIDRA analysis indicate that the intersection of Collier Road and Alice Street operates at a good levels of services 'A' and 'B' with minimal queues for each movement during the AM peak hour except the right turn out movement from Alice Street onto Collier Road eastbound which currently operates at LoS 'E' with an estimated queue distance of one car. The SIDRA results for the post development scenario during the AM peak hour indicate same operational condition for all the movements as the existing situation; and,
- The results of the SIDRA analysis indicate that the intersection of Collier Road and Alice Street operates at a good levels of services 'A' and 'B' with minimal queues for each movement during PM peak hour except the right turn out movement from Alice Street onto Collier Road eastbound which currently operates at LoS 'F' with an estimated queue distance of about two cars. The SIDRA results for the post development scenario during the PM peak hour indicate same operational condition for all the movements as the existing situation.

As states in section 5.0 of this report there is a proposal to upgrade the existing four-way intersection of Collier Road/ Jackson Street/ Grey Street about 350m to the west of the intersection of Collier Road/ Alice Street to a signalised intersection. This upgrade will improve the operation of the intersection of Collier Road and Alice Street by creating gaps in the Collier Road traffic allowing more opportunities for the right turn movements from Alice Street onto Collier Road.

The proposed site crossover on Collier Road will be in the form of left in/ left out only and as such it is expected to operate without capacity issues during the post development scenario.

The Alice Street through traffic fronting the site crossover and the Alice Street crossover turning volumes are relatively low during the AM and PM peak hours. Therefore, it is expected that this crossover would work satisfactorily during the post development scenario.

7.5 Impact on Surrounding Roads

The WAPC Transport Assessment Guidelines for Developments (2016) provides guidance on the assessment of traffic impacts:

“As a general guide, an increase in traffic of less than 10 per cent of capacity would not normally be likely to have a material impact on any particular section of road, but increases over 10 per cent may. All sections of road with an increase greater than 10 per cent of capacity should therefore be included in the analysis.

For ease of assessment, an increase of 100 vehicles per hour for any lane can be considered as equating to around 10 per cent of capacity. Therefore any section of road where the structure plan traffic would increase flows by more than 100 vehicles per hour for any lane should be included in the analysis.”

The proposed development will not increase traffic flows anywhere near the quoted WAPC threshold to warrant further detailed analysis. As shown in Figure 19, the proposed development will not increase traffic on any lanes on the surrounding road network by more than 100vph, therefore the impact on the surrounding road network is considered to be insignificant.

7.6 Impact on Neighbouring Areas

The traffic generated by the proposed development would have minimal impact on nearby areas as the majority of traffic attracted to the development is from passing traffic.

7.7 Traffic Noise and Vibration

It generally requires a doubling of traffic volumes on a road to produce a perceptible 3dB (A) increase in road noise. The proposed development will not increase traffic volumes or noise on surrounding roads anywhere near this level to result in any perceptible increase in noise.

7.8 Road Safety

No particular road safety issues have been identified for this site.

8.0 Parking and Access

8.1 Parking Provision

It is proposed to provide 10 on-site car parking bays including one accessible bay, 2 staff bays and 1 Air and Water bay. A loading bay is also proposed at the western end of the proposed convenience store. 8 vehicles can use the fuel bowsers simultaneously. Therefore, minimum of 19 vehicles can comfortably be accommodated on-site.

8.2 Access

The proposal intends to utilise the existing Alice Street crossover which currently serves the site. It is also proposed to create a new left in/ left out only crossover on Collier Road to service the proposed development.

9.0 Provision for Heavy Vehicles

It is proposed that 19.0m tankers to be used for fuel delivery tankers. The fuel tankers will enter the site from Alice Street, circulate within the site and access the fill point and then egress the site onto Collier Road via a left turn.

Turn path analysis undertaken for 19.0m fuel tanker confirms that the tanker movement is satisfactory. The result of the turn path analysis is included in Appendix C.

Deliveries and waste collections will also be accommodated within the development site. The loading bay provided at the western end of the proposed convenience store will accommodate the delivery vehicles and waste collection trucks.

Fuel tankers and other service vehicles are expected to access the site no more than 3-4 times a week and generally outside the peak road network and peak site activity periods. As service vehicles are expected to access the site outside the peak operating times of the business, no traffic conflicts between customer, employee and service vehicles are expected.

10.0 Conclusions

This Transport Impact Assessment (TIA) is prepared on behalf of Collier Rd WA Pty Ltd ATFT Collier Road WA Unit Trust with respect to the proposed 7- Eleven development to be located at 300 Collier Road in the Town of Bassendean.

The proposed development will be located at the eastern corner of the site and at the south west corner of the intersection of Collier Road and Alice Street. This portion of Lot 300 will be subject to a future subdivision application to accommodate the proposed development.

It is proposed to utilise the existing full movement crossover on Alice Street which serves the site for the proposed development. It is also proposed to create a new left in/left out crossover on Collier Road to service the site. The left in/left out nature of this crossover will be reinforced by extension of the existing solid centre median on Collier Road.

The proposed crossover system facilitates efficient and convenient vehicular access and egress to and from the proposed development for private cars and fuel tankers. The creation of the Collier Road crossover is essential for effective and convenient traffic circulation for the proposed development particularly for the fuel tanker movements. The proposed access and egress system and the tanker movements have been presented and discussed with the Engineering Department at the Town of Bassendean (in a meeting dated 17th July 2016) and 'in principle' approval obtained.

A total of 19 vehicles can comfortably be accommodated on-site. The access easement provided to the south of the proposed development will be as part of the adjacent Lot (Lot B) and the proposed development will have the benefit to use it.

A significant component of the proposed development traffic would be from passing trade on Collier Road. As such, minimal traffic increase is anticipated on the surrounding road network as a result of the proposed development.

SIDRA analysis indicates that the intersection of Collier Road/ Alice Street will continue to operate at the same level of service in the post-development scenario as compared to the existing situation.

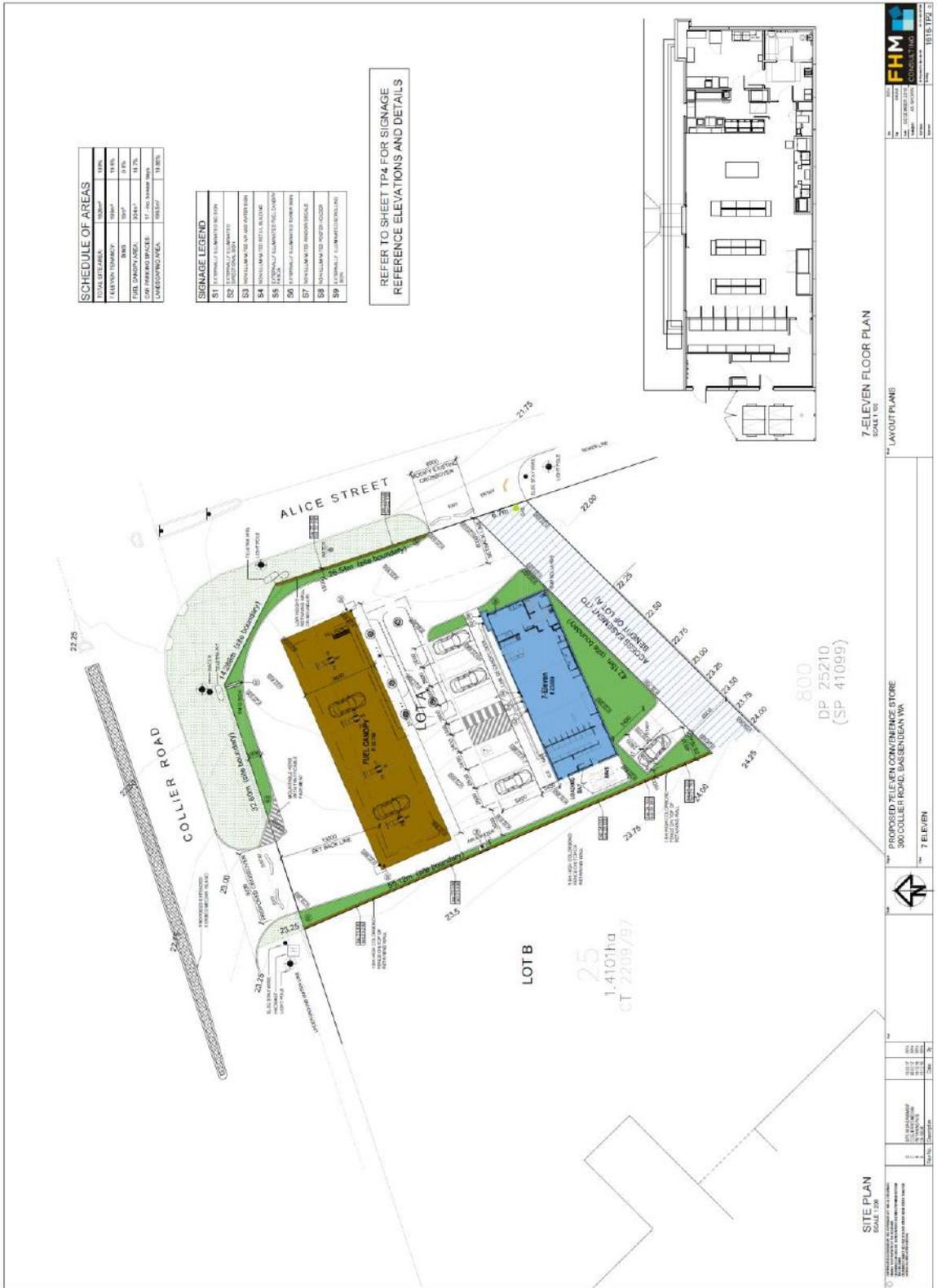
The turn path analysis undertaken for a 19.0m fuel tanker indicates that the site layout design and the Collier Road and Alice Street crossover geometries can accommodate the movements of fuel tankers.

In conclusion, the findings of this TIA are supportive of the proposed development, and demonstrate that the traffic resulting from the proposed development can be accommodated satisfactorily by the surrounding road network.

Appendix A



DEVELOPMENT PLAN



SCHEDULE OF AREAS

AREA	AREA NO.	AREA NAME	AREA TYPE	AREA CODE
LOT B	1	LOT B	RESIDENTIAL	10000
LOT A	2	LOT A	RESIDENTIAL	10000
DRIVEWAY	3	DRIVEWAY	DRIVEWAY	10000
PAVING	4	PAVING	PAVING	10000
LANDSCAPING	5	LANDSCAPING	LANDSCAPING	10000

SIGNAGE LEGEND

SYMBOL	DESCRIPTION
S1	CONCRETE SIGNPOST
S2	CONCRETE SIGNPOST
S3	CONCRETE SIGNPOST
S4	CONCRETE SIGNPOST
S5	CONCRETE SIGNPOST
S6	CONCRETE SIGNPOST
S7	CONCRETE SIGNPOST
S8	CONCRETE SIGNPOST
S9	CONCRETE SIGNPOST

REFER TO SHEET TP4 FOR SIGNAGE REFERENCE ELEVATIONS AND DETAILS

7.11 LEVEL FLOOR PLAN
SCALE 1:50

LAYOUT PLAN
SCALE 1:50

PROPOSED RETAIL CONVENIENCE STORE
300 COLLIER ROAD, BASSENDEAN WA



SITE PLAN
SCALE 1:500

NO.	REVISION	DATE
1	ISSUED FOR PERMIT	15/08/2024
2	ISSUED FOR PERMIT	15/08/2024

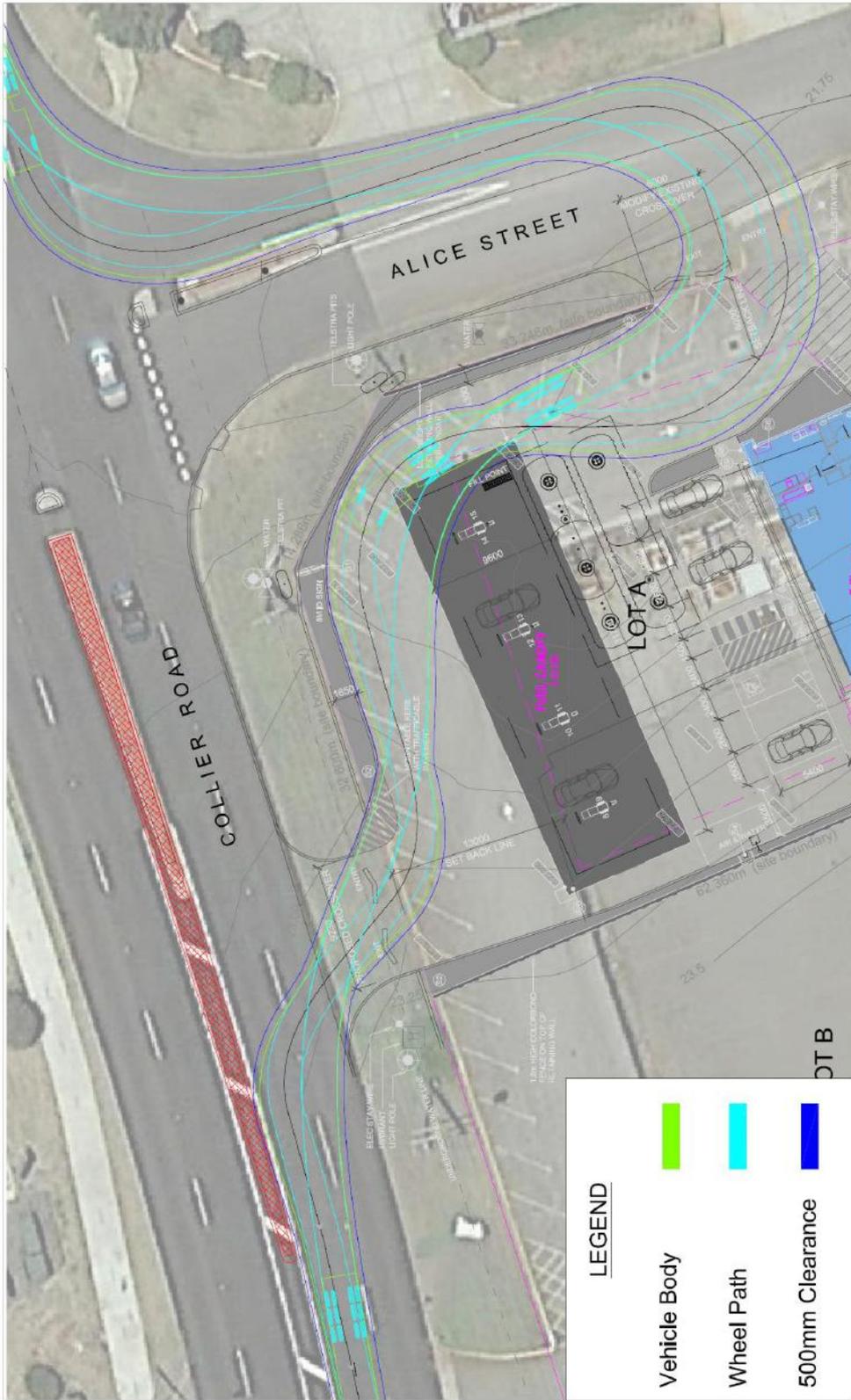
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DRAWN BY: [Name]
CHECKED BY: [Name]
DATE: 15/08/2024



FHM CONSULTING
15/08/2024

Appendix B

TURN PATH ANALYSIS



Proposed 7-Eleven - Lot 300 Collier Road, Bassendean
 Austrorads 2013 - 19m Semi-trailer
 Left turn onto Alice Street, access the fill point and left turn exit onto Collier Road

t16.147.sk08b

31/01/2017

Scale: 1/250 @ A3



Appendix C

SIDRA OUTPUTS

Table 2: Collier Road/ Alice Street SIDRA results – Existing AM Peak Hour

Movement Performance - Vehicles											
Mov ID	OD Mov	Demand Total veh/h	Flows HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: Alice St											
1	L2	56	14.0	0.061	6.2	LOS A	0.2	1.7	0.38	0.59	48.2
3	R2	13	14.0	0.109	35.2	LOS E	0.3	2.6	0.90	0.95	35.0
Approach		68	14.0	0.109	11.5	LOS B	0.3	2.6	0.47	0.66	45.0
East: Collier Rd - East											
4	L2	9	14.0	0.160	6.6	LOS A	0.0	0.0	0.00	0.02	61.5
5	T1	566	14.0	0.160	0.0	LOS A	0.0	0.0	0.00	0.01	69.8
Approach		576	14.0	0.160	0.1	NA	0.0	0.0	0.00	0.01	69.7
West: Collier Rd - West											
11	T1	607	14.0	0.229	0.9	LOS A	1.2	9.3	0.14	0.07	67.9
12	R2	81	14.0	0.229	11.2	LOS B	1.2	9.3	0.43	0.23	53.5
Approach		688	14.0	0.229	2.1	NA	1.2	9.3	0.17	0.09	65.8
All Vehicles		1333	14.0	0.229	1.7	NA	1.2	9.3	0.11	0.09	65.8

Level of Service (LOS) Method: Delay (HCM 2000).

Vehicle movement LOS values are based on average delay per movement

Minor Road Approach LOS values are based on average delay for all vehicle movements.

NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road movements.

SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.

Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).

HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

Table 3: Collier Road/ Alice Street SIDRA results – Existing PM Peak Hour

Movement Performance - Vehicles											
Mov ID	OD Mov	Demand Total veh/h	Flows HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: Alice St											
1	L2	145	14.0	0.175	6.9	LOS A	0.7	5.2	0.45	0.67	47.8
3	R2	35	14.0	0.432	64.6	LOS F	1.4	11.2	0.95	1.04	27.3
Approach		180	14.0	0.432	18.0	LOS C	1.4	11.2	0.55	0.74	41.7
East: Collier Rd - East											
4	L2	19	14.0	0.203	6.6	LOS A	0.0	0.0	0.00	0.03	61.4
5	T1	713	14.0	0.203	0.0	LOS A	0.0	0.0	0.00	0.02	69.7
Approach		732	14.0	0.203	0.2	NA	0.0	0.0	0.00	0.02	69.5
West: Collier Rd - West											
11	T1	678	14.0	0.229	0.9	LOS A	1.0	8.1	0.12	0.04	68.0
12	R2	43	14.0	0.229	13.5	LOS B	1.0	8.1	0.32	0.10	54.5
Approach		721	14.0	0.229	1.7	NA	1.0	8.1	0.14	0.04	67.0
All Vehicles		1633	14.0	0.432	2.8	NA	1.4	11.2	0.12	0.11	63.8

Level of Service (LOS) Method: Delay (HCM 2000).

Vehicle movement LOS values are based on average delay per movement

Minor Road Approach LOS values are based on average delay for all vehicle movements.

NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road movements.

SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.

Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).

HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

**Table 4: Collier Road/ Alice Street SIDRA results – Immediately Post Development
AM Peak Hour**

Movement Performance - Vehicles											
Mov ID	OD Mov	Demand Total veh/h	Flows HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	Queue Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: Alice St											
1	L2	56	14.0	0.060	6.1	LOS A	0.2	1.7	0.37	0.58	48.2
3	R2	26	14.0	0.232	39.5	LOS E	0.7	5.8	0.91	0.98	33.6
Approach		82	14.0	0.232	16.8	LOS C	0.7	5.8	0.54	0.71	42.3
East: Collier Rd - East											
4	L2	23	14.0	0.161	6.6	LOS A	0.0	0.0	0.00	0.05	61.2
5	T1	557	14.0	0.161	0.0	LOS A	0.0	0.0	0.00	0.02	69.6
Approach		580	14.0	0.161	0.3	NA	0.0	0.0	0.00	0.03	69.3
West: Collier Rd - West											
11	T1	598	14.0	0.241	0.9	LOS A	1.3	10.6	0.14	0.08	67.8
12	R2	99	14.0	0.241	11.3	LOS B	1.3	10.6	0.50	0.31	52.7
Approach		697	14.0	0.241	2.4	NA	1.3	10.6	0.19	0.12	65.1
All Vehicles		1359	14.0	0.241	2.4	NA	1.3	10.6	0.13	0.11	64.7

Level of Service (LOS) Method: Delay (HCM 2000).

Vehicle movement LOS values are based on average delay per movement

Minor Road Approach LOS values are based on average delay for all vehicle movements.

NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road movements.

SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.

Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).

HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

**Table 5: Collier Road/ Alice Street SIDRA results – Immediately Post Development
PM Peak Hour**

Movement Performance - Vehicles												
Mov ID	OD Mov	Demand Total veh/h	Flows HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h	
South: Alice St												
1	L2	145	14.0	0.171	6.7	LOS A	0.6	5.1	0.44	0.66	47.9	
3	R2	55	14.0	0.710	94.6	LOS F	2.8	21.6	0.98	1.15	22.3	
Approach		200	14.0	0.710	30.8	LOS D	2.8	21.6	0.59	0.79	36.4	
East: Collier Rd - East												
4	L2	38	14.0	0.205	6.6	LOS A	0.0	0.0	0.00	0.06	61.1	
5	T1	700	14.0	0.205	0.0	LOS A	0.0	0.0	0.00	0.03	69.5	
Approach		738	14.0	0.205	0.4	NA	0.0	0.0	0.00	0.03	69.0	
West: Collier Rd - West												
11	T1	664	14.0	0.249	1.3	LOS A	1.5	11.7	0.16	0.06	67.4	
12	R2	68	14.0	0.249	13.6	LOS B	1.5	11.7	0.48	0.20	52.7	
Approach		733	14.0	0.249	2.4	NA	1.5	11.7	0.19	0.08	65.7	
All Vehicles		1671	14.0	0.710	4.9	NA	2.8	21.6	0.15	0.14	61.1	

Level of Service (LOS) Method: Delay (HCM 2000).

Vehicle movement LOS values are based on average delay per movement

Minor Road Approach LOS values are based on average delay for all vehicle movements.

NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road movements.

SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.

Gap-Acceptance Capacity: SIDRA Standard (Akçelik M3D).

HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

Appendix 5
Clause 42 Certificate



Enquiries: Trevor Servaas (08) 655 19110
Our Ref: 42/52307463
Your Ref:

PLANNING SOLUTIONS (AUST) PTY LTD

296 FITZGERALD STREET
PERTH WA 6000

Dear Sir/Madam

**CERTIFICATE UNDER CLAUSE 42 OF THE METROPOLITAN REGION SCHEME ISSUED BY THE
WESTERN AUSTRALIAN PLANNING COMMISSION**

In Reply to your request, please find enclosed
Certificate number 52307463.

It is advised that the enclosed Certificate has been prepared to conform with the current Statutory requirements
of the Metropolitan Region Scheme as at the date of signature.

The following documents are forwarded for your retention.
NO DOCUMENTS ENCLOSED

Yours faithfully

A handwritten signature in black ink that reads "Kerrine Blenkinsop". The signature is written in a cursive, flowing style.

Kerrine Blenkinsop
Secretary
Western Australian Planning Commission

27 October 2016

Enc.



METROPOLITAN REGION SCHEME
FORM 5
CERTIFICATE NUMBER 52307463

Lot Number(s) : 25
Street : COLLIER ROAD
Diagram : 60011
Location : N/A
Certificate of Title
Volume Folio
2209 97

THE WHOLE OF THE LAND IS ZONED INDUSTRIAL AND FRONTS OTHER REGIONAL ROADS
RESERVATION (COLLIER ROAD)

A handwritten signature in black ink that reads "Kerrine Blenkinsop". The signature is written in a cursive, flowing style.

Kerrine Blenkinsop
Secretary
Western Australian Planning Commission

27 October 2016

This certificate relates only to the provisions of the Approved Metropolitan Region Scheme and does NOT purport to indicate the land use allocation under any Local Government Authority provision.





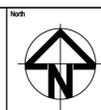
LOCALITY PLAN
SCALE 1:500



SURVEY PLAN
SCALE 1:200

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Rev No.	Description	Date	By
C	SITE AREA-EASEMENT COLLIER RD MEDIAN DA ISSUE	13.02.17	RFH
B		03.02.17	RFH
A		13.12.16	RFH



Project	PROPOSED 7ELEVEN CONVENIENCE STORE 300 COLLIER ROAD, BASSENDEAN WA
Client	7-ELEVEN

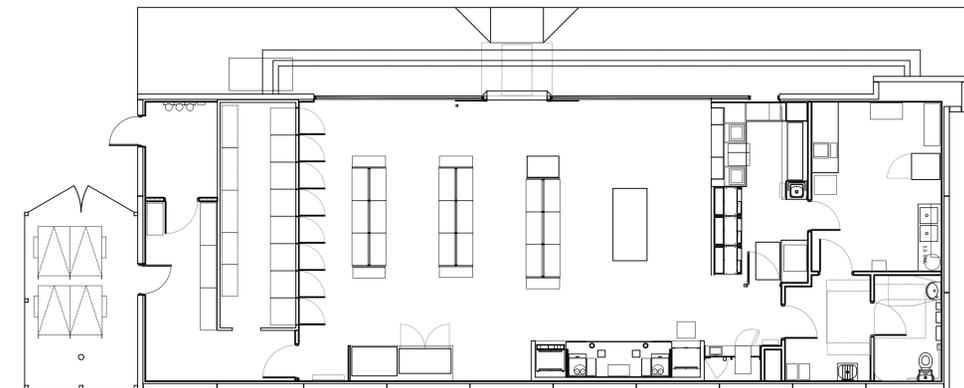
Sheet	LOCALITY PLAN SURVEY PLAN
-------	---------------------------

Client	RFH	
Design	PARA	
Date	DECEMBER 2016	
Scale	AS SHOWN	
Survey	AS SHOWN	
Approved		<small>39 Woodley Rd Glen Iris WA 6111 1439 897948</small> <small>1616-TP1 c</small>

SCHEDULE OF AREAS		
TOTAL SITE AREA:	1828m ²	100%
7-ELEVEN TENANCY:	193m ²	10.5%
BINS:	16m ²	0.9%
FUEL CANOPY AREA:	304m ²	16.7%
CAR PARKING SPACES:	17 - inc. bowser bays.	
LANDSCAPING AREA:	198.5m ²	10.86%

SIGNAGE LEGEND	
S1	EXTERNALLY ILLUMINATED SID SIGN
S2	EXTERNALLY ILLUMINATED DIRECTIONAL SIGN
S3	NON ILLUMINATED AIR AND WATER SIGN
S4	NON ILLUMINATED RETAIL BUILDING
S5	EXTERNALLY ILLUMINATED FUEL CANOPY FASCIA
S6	EXTERNALLY ILLUMINATED TOWER SIGN
S7	NON ILLUMINATED WINDOW DECALS
S8	NON ILLUMINATED POSTER HOLDER
S9	EXTERNALLY ILLUMINATED SCROLLING SIGN

REFER TO SHEET TP4 FOR SIGNAGE REFERENCE ELEVATIONS AND DETAILS



SITE PLAN
SCALE 1:200

7-ELEVEN FLOOR PLAN
SCALE 1:100

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Rev No.	Description	Date	By
D	SITE AREA-EASEMENT COLLIER RD MEDIAN	13.02.17	RFH
C	RETAINING RL'S	03.02.17	RFH
B	DA ISSUE	19.12.16	RFH
A		13.12.16	RFH



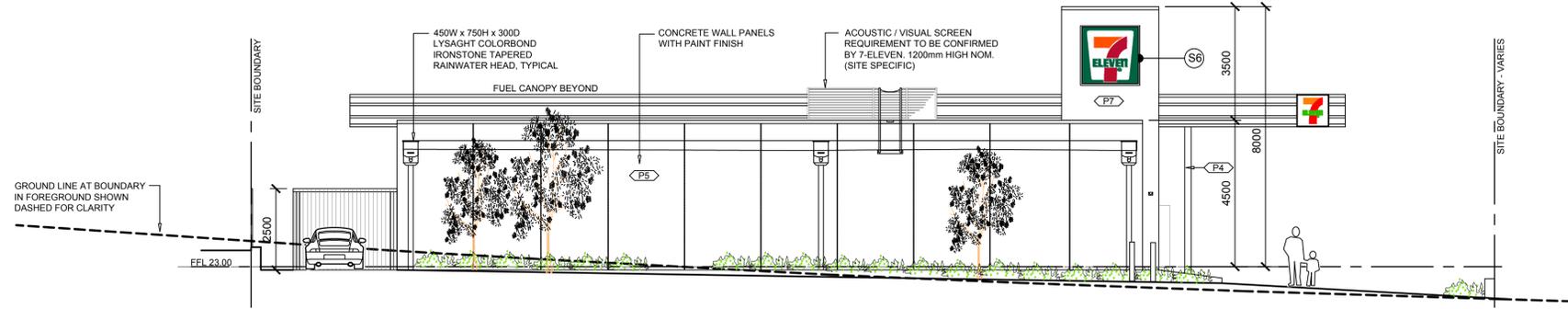
Project: PROPOSED 7EVEN CONVENIENCE STORE
300 COLLIER ROAD, BASSENDEAN WA

Client: 7 ELEVEN

Sheet: LAYOUT PLANS

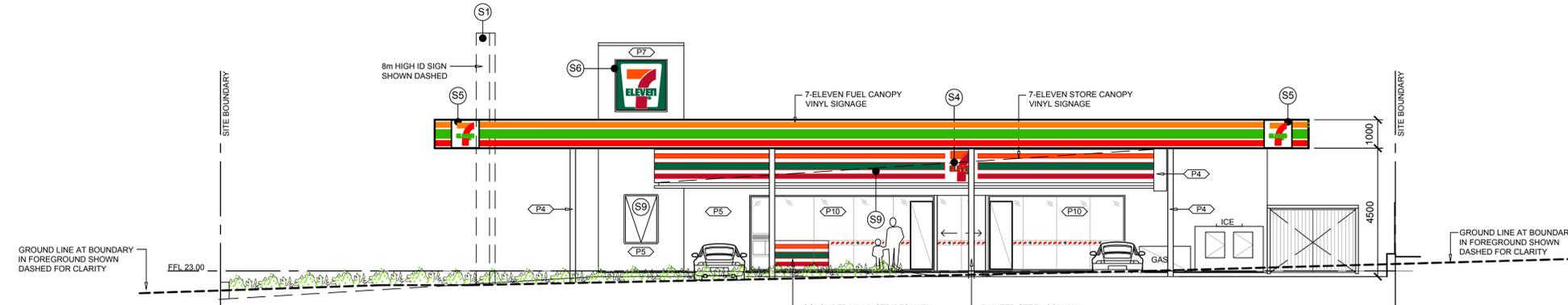
Clk:	RFH
Dn:	PARA
Date:	DECEMBER 2016
Scale@A1:	AS SHOWN
Surv/Stan:	35 Woodley Rd Glen Iris WA 6111 439 89794
Approved:	Drawing

1616-TP2

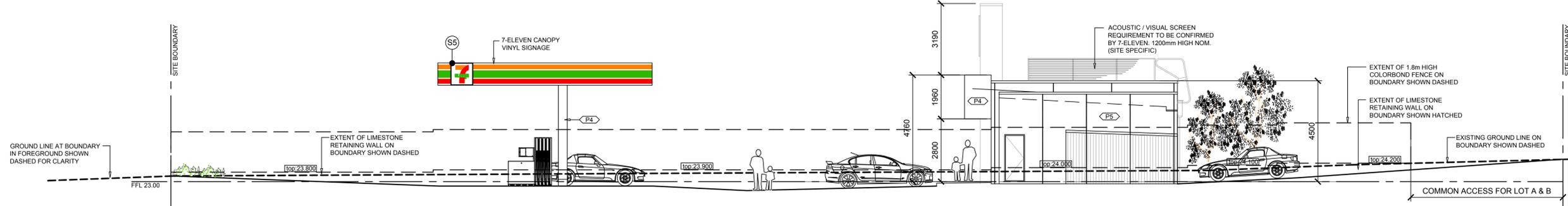


SOUTH EAST ELEVATION (convenience store)
Scale 1:100

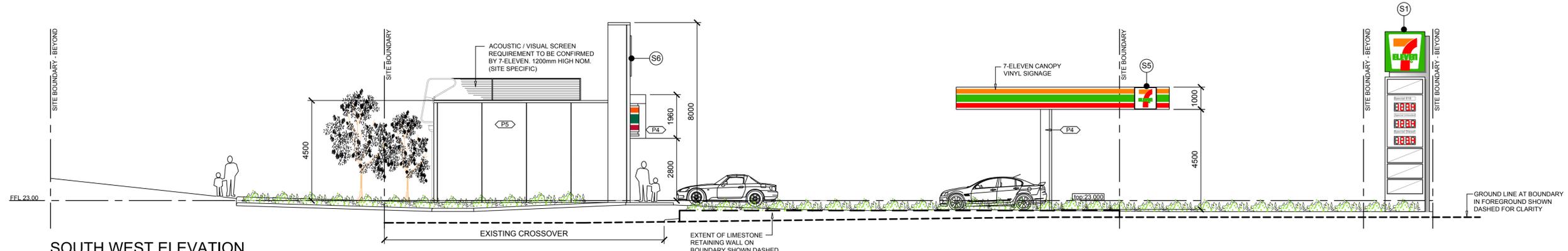
CODE	FINISH SPECIFICATION	LOCATION
P2	150x150 ROUGH SAWN POSTS COLOUR: CREAM	VERANDAH POSTS FASCIA & GUTTERS
P3	CORRUGATED IRON COLOUR: COLORBOND SLATE GREY	ROOF CLADDING
P4	DULUX WEATHERSHIELD X10 GLOSS ACRYLIC COLOUR: VIVID WHITE PW1H9	7-ELEVEN PYLON POLES, LIGHTING POLES, VENT PIPES, PETROL CANOPY COLUMNS & DOWNPIPES
P5	TEXTURED (ROUGH) FINISH GREY COLOUR	BUILDING - EXTERNAL CONCRETE WALL PANELS
P6	DULUX WEATHERSHIELD X10 GLOSS ACRYLIC COLOUR: BLACK	STEEL CANOPY COLUMNS AND BOLLARDS 300mm ABOVE GROUND LEVEL
P7	TIMBER LOOK CLADDING TO SIGNAGE TOWER	BUILDING TOWER CONCRETE WALL PANELS
P10	CLEAR GLAZING IN A POWDERCOATED ALUMINIUM FRAME WITH FRAMELESS MULLIONS	SHOPFRONT



NORTH WEST ELEVATION (fuel canopy)
Scale 1:100



NORTH EAST ELEVATION
Scale 1:100



SOUTH WEST ELEVATION
Scale 1:100

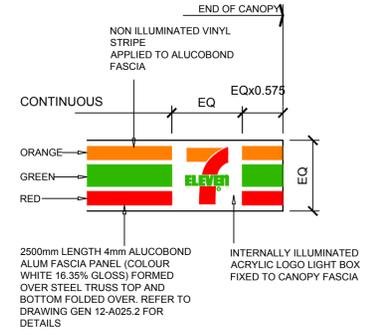
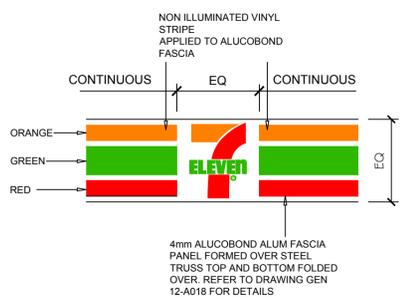
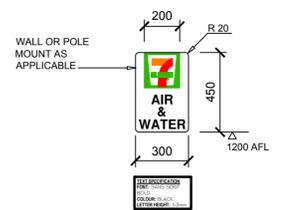
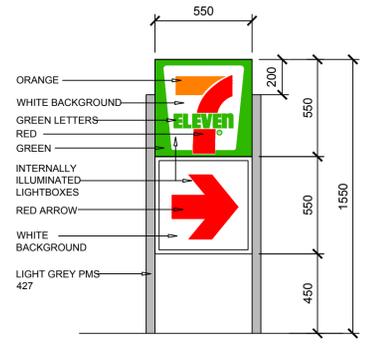
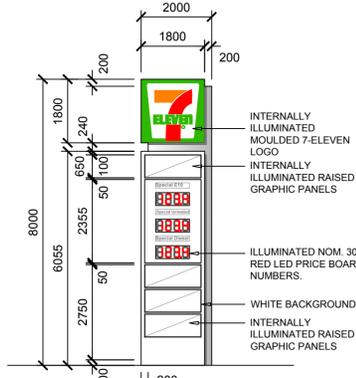
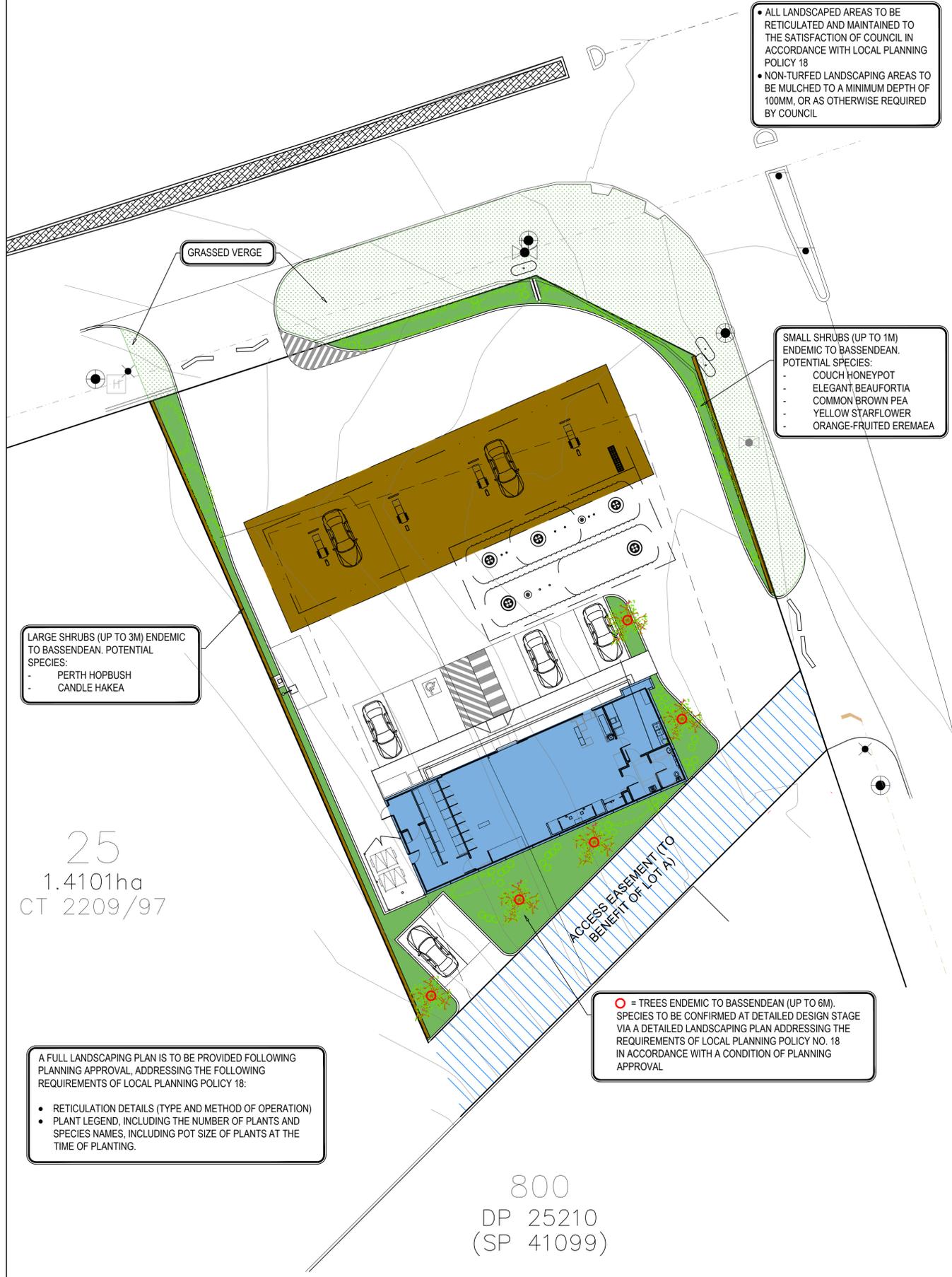
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Rev No.	Description	Date	By
B	RETAINING RL'S	19.12.16	RFH
A	DA ISSUE	13.12.16	RFH

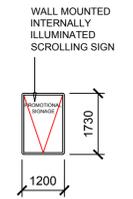
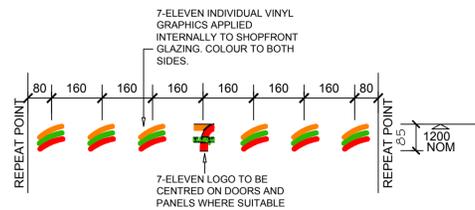
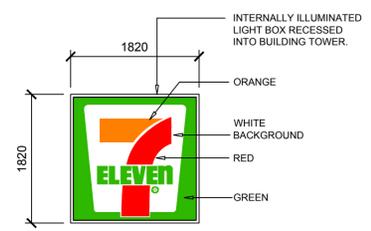
Project: **PROPOSED 7ELEVEN CONVENIENCE STORE**
300 COLLIER ROAD, BASSENDEAN WA
Client: **7 ELEVEN**

Sheet: **ELEVATIONS**

Clk	PARA	FHM CONSULTING
Dn	PARA	
Date	DECEMBER 2016	
Scale	AS SHOWN	
Surv/Stan	33 Woodley Rd Glen Iris WA	61-11-439 89794
Approved	1616-TP3	



COLOUR LEGEND			
COLOUR	PANTONE	NON-ILLUM.	ILLUM.
ORANGE	PMS 165	7725-14	3630-13
GREEN	PMS 341	7725-56	3630-26
RED	PMS 485	7725-13	3630-43

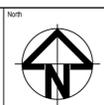


LANDSCAPE PLAN
SCALE 1:200

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Rev No.	Description	Date	By
C	SITE AREA EASEMENT	13.02.16	RFH
B	VERGE GRASS PLANTING GUIDANCE	03.02.16	RFH
A	PLANTING GUIDANCE	19.12.16	RFH
	DA ISSUE	13.12.16	RFH

Rev No.	Description	Date	By

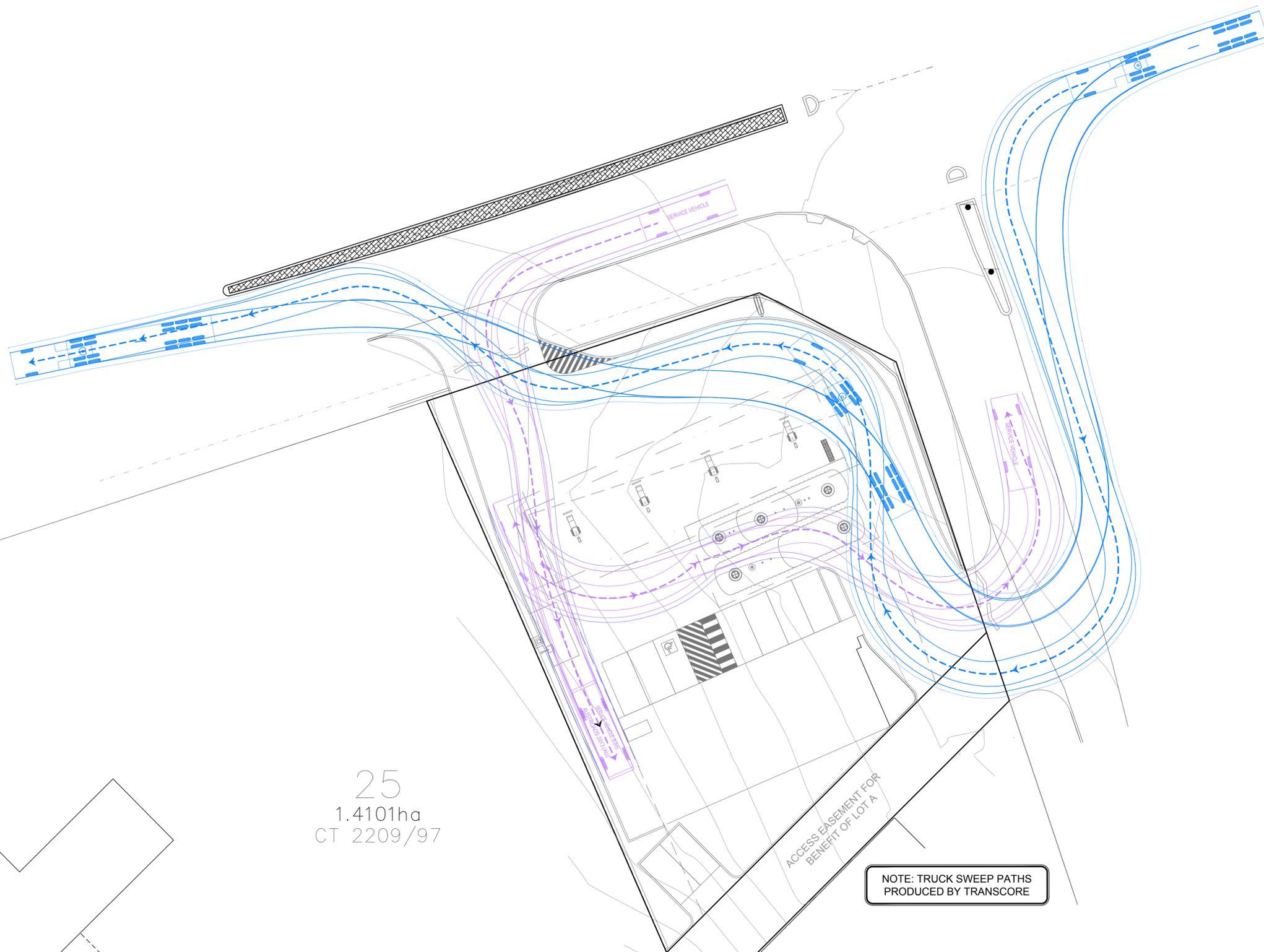


Project: **PROPOSED 7ELEVEN CONVENIENCE STORE**
300 COLLIER ROAD, BASSENDEAN WA

Client: **7 ELEVEN**

Sheet: **LANDSCAPE PLAN**
SIGNAGE DETAILS

Clk	RFH	FHM
Dn	PARA	
Date	DECEMBER 2016	CONSULTING
Scale	AS SHOWN	
Surf/Stat	33 Woodley Rd Glen Iris WA	61 11 439 8979
Approved		1616-TP4



25
1.4101ha
CT 2209/97

800
DP 25210
(SP 41099)

NOTE: TRUCK SWEEP PATHS
PRODUCED BY TRANSORE

TRUCK PATH PLAN
SCALE 1:200

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WORKS ON THE LISTED HEREON.

Rev No.	Description	Date	By
C	LOT AREA - EASEMENT	13.02.17	RFH
B	COLLIER RD MEDIAN	03.02.17	RFH
A	DA ISSUE	13.12.16	RFH



Project **PROPOSED 7ELEVEN CONVENIENCE STORE
300 COLLIER ROAD, BASSENDEAN WA**

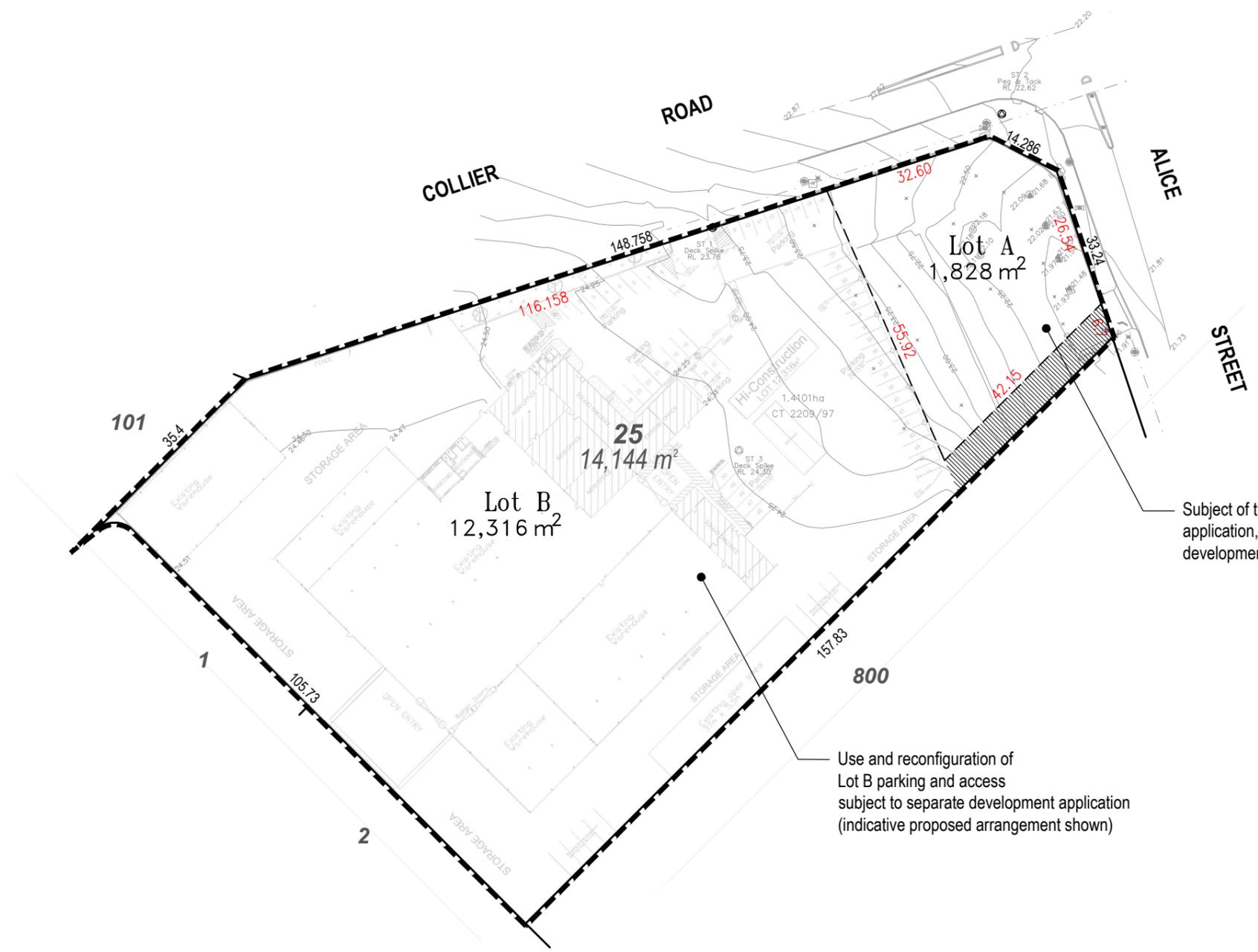
Client **7 ELEVEN**

Sheet **TRUCK PATH**

Clk	RFH
Dn	PARA
Date	DECEMBER 2016
Scale@A1	AS SHOWN
Surv/Stan	
Approved	



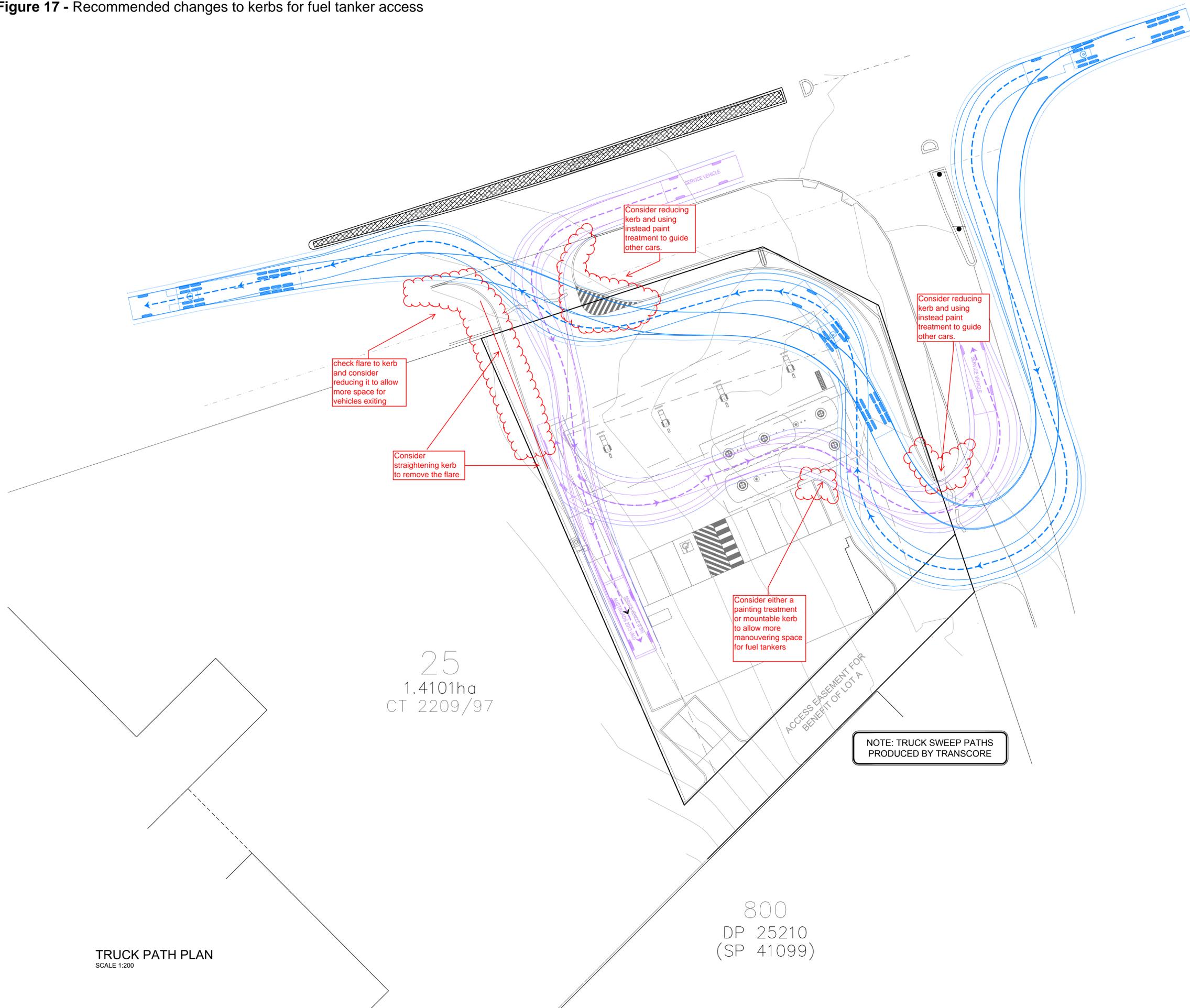
LEGEND	
	Subject Site (Lot 25)
6	Existing Lot Number
	Existing Lot Boundary
Lot A	Proposed Lot Number
	Proposed Lot Boundary
110	Existing Lot Dimensions
110	Proposed Lot Dimensions
	Common Access
	Existing Level Contour



Subject of this development application, refer to detailed development plans

Use and reconfiguration of Lot B parking and access subject to separate development application (indicative proposed arrangement shown)

Figure 17 - Recommended changes to kerbs for fuel tanker access



TRUCK PATH PLAN
SCALE 1:200

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Rev No.	Description	Date	By
C	LOT AREA - EASEMENT	13.02.17	RFH
B	COLLIER RD MEDIAN	03.02.17	RFH
A	DA ISSUE	13.12.16	RFH



Project
**PROPOSED 7ELEVEN CONVENIENCE STORE
300 COLLIER ROAD, BASSENDEAN WA**

Client
7 ELEVEN

Sheet
TRUCK PATH

Clk	RFH
Dn	PARA
Date	DECEMBER 2016
Scale@A1	AS SHOWN
Surv/Stan	30 Woodley Rd Glen Iris WA 6111 439 89794
Approved	1616-TP5



PSA Ref: 4765

21 February 2017

Chief Executive Officer
Town of Bassendean
PO Box 87
BASSENDEAN WA 6934

Attention: Christian Buttle, Development Services

Dear Sir,

**PORTION OF LOT 25 (300) COLLIER ROAD, BASSENDEAN
PROPOSED CONVENIENCE STORE**

Planning Solutions acts on behalf of Collier Road WA Pty Ltd in support of an Application for Development Approval for Lot 25 (300) Collier Road, Bassendean (**subject site**).

Please find enclosed a development application package, comprising:

1. A letter of consent for the lodgement of relevant statutory planning application forms, signed by the landowner.
2. Development Assessment Panel Form 1 application form, signed by the applicant.
3. Metropolitan Region Scheme Form 1 and the Town of Bassendean Application for Development Approval forms, signed by the applicant.
4. Development application report and 2x copies of development plans to scale.
5. Certificate of Title and Diagram applicable to the subject site.
6. Supporting transport impact assessment.
7. A CD containing electronic copies of the development application and supporting documentation.

The planning application fee for this proposal is **\$9,421**, pursuant to the Town's and DAP's schedule of fees. The proponent wishes to pay this fee directly with the Town. Please contact Steve Kennedy on 0423 098 197 for direct credit card payment.

We refer to correspondence from the Town dated 16 December 2016, requesting additional information be provided with respect to the proposed convenience store development on the subject site. We provide the following information.

Extent / Nature of Development Site

The development plans have been supplemented with an 'overall site plan', confirming:

- Lot dimensions of entire development site.
- Proposed lot dimensions and boundaries of future 'Lot A' and 'Lot B' (subject to separate subdivision application, lodged with the WAPC).

Impact of Proposed Development on Existing Development

The 'overall' site plan provided with the development plans delineates future 'Lot A' (subject of the proposed convenience store) and future 'Lot B' (containing the existing warehouse facility and associated parking/storage areas).

The 'overall' site plan depicts the indicative use, layout and reconfiguration of future 'Lot B' in relation to the proposed convenience store development on future 'Lot A'. The plan demonstrates how the existing building and activities on Lot B can continue to operate with a modified parking and access configuration. Importantly, the 'overall' site plan also demonstrates both of the developments on future 'Lot A' and future 'Lot B' can operate as separate entities on separate lots.

This development application relates only to the proposed convenience store development on future 'Lot A'. All site and facility reconfigurations relating to future 'Lot B' are subject to a separate development application, being lodged with the Town and progressed by Highway Construction (the landowner).

Proposed Road Modifications

Development plans have been updated to depict the proposed extension to the Collier Road median strip, designed by Transcore.

Stormwater

At the outset of this project, our preferred civil engineering consultant McDowall Affleck liaised with the Town's technical services department and the Town's planning department. It was confirmed that detailed stormwater plans would not be required at the planning application stage, given the site's sandy geotechnical features and distance from the Swan River.

The site is capable of handling and disposing of all stormwater on site, evidenced by the existing stormwater systems present on site. A reconfiguration of these systems will be required to allow each lot to act independently of each other, with this detailed design capable of being addressed as a condition of approval.

An email from McDowall Affleck confirming this is enclosed.

Landscaping

A landscaping concept plan is included within the development plans, which provides sufficient detail relating to the nature (indicative species and height) and extent (areas within the site to be landscaped) of landscaping proposed. This is consistent with section 1.6 of Local Planning Policy 18 (**LPP18**), setting out the requirements for a planning application.

More detailed requirements for landscape plans are listed in section 1.5 of LPP18. While there is nothing in LPP18 indicating this level of information is mandatory for the lodgement of a planning application, appropriate notations are provided on the landscaping plan itself confirming the specific requirements of LPP18 can and will be addressed at the detailed design stage.

As is common across metropolitan local authorities, a detailed landscaping plan (including all other details for irrigation, maintenance and management) can be required by the local authority as a condition of approval.

Public Art

Local Planning Policy 15 (**LPP15**) sets out public art requirements. As the proposed convenience store has an estimated cost of development in the order of \$2.1 million, it may be considered an eligible proposal.

There is no specific policy provision stating public art should be required for a commercial type development within a general industrial area. Notwithstanding, this is a matter appropriately addressed as a condition of planning approval in accordance with standard practice – noting there is no specific requirement under LPP15 for public art to be addressed during the development application stage. In any event, it is not envisaged that public art would be provided on site in this instance.

Retaining Wall

The development plans have been updated to include details of retaining walls, including top of wall heights and bottom of wall heights.

Conclusion

Planning Solutions has provided additional information requested by the Town, both in the form of additional detail shown on the development plans and additional justification being provided regarding stormwater, landscaping and public art.

In summary, the development application package meets the explicit requirements of the Town's local planning framework, and contains sufficient detail for the application to be assessed by the Town.

We therefore respectfully request the Town assess the development application at the earliest convenience, and we look forward to the Town's favourable recommendation to Metro Central JDAP.

Yours sincerely,

A handwritten signature in blue ink, appearing to read 'a stagno', with a horizontal line underneath.

ALESSANDRO STAGNO
PLANNING CONSULTANT

PS ref: 4765

11 April 2017

Chief Executive Officer
Town of Bassendean
PO Box 87
BASSENDEAN WA 6934

Attention: Dylan Stokes, Development Services

Dear Sir,

**LOT 25 (300) COLLIER ROAD, BASSENDEAN
PROPOSED CONVENIENCE STORE
RESPONSE TO REQUEST FOR FURTHER INFORMATION**

Planning Solutions acts on behalf of Collier Road WA Pty Ltd, the proponent of the proposed convenience store development at Lot 25 (300) Collier Road, Bassendean (**subject site**).

We refer to the Town's 'Request for Further Information' (**RFI**) dated 30 March 2017 in relation to the proposed convenience store development on the subject site, inviting Planning Solutions to address the Town's assessment comments on various matters.

Planning Solutions provides the following response, as detailed below.

Town's assessment comment	Planning Solutions' response
Waste management plan	
<p><i>Prior to consideration at the DAP, a waste management plan must be prepared by the applicant detailing the following:</i></p> <ul style="list-style-type: none"> <i>o Detail must be provided that ensures that the bin area and waste arrangement shown on the plans is sufficient and will work</i> <i>o Anticipated waste output generated by the development.</i> <i>o Size and type of bins used in the development to accommodate waste on site.</i> <i>o Times and regularity of waste pickup.</i> <i>o Details of private waste pickup, including type of vehicle used to pickup waste on site (noting that the vehicle manoeuvring shown on plans only displays a vehicle entering in forward gear).</i> 	<p>A 'waste management plan' (WMP) is commonly provided at the detailed design stage, in accordance with a condition of planning approval. There is no explicit requirement for a WMP to be prepared under the Town's Local Planning Scheme No. 10 (LPS10) or policy framework.</p> <p>Notwithstanding, we note the siting of, and access into the service yard / bin area is compliant with the requirements of LPS10 and the Town's policy framework, as detailed in the development application report.</p> <p>To address the matter, Planning Solutions suggests the following condition wording:</p> <p><i>Prior to the occupation of the convenience store, a 'waste management plan' must be submitted to the satisfaction of the Town.</i></p>

Town's assessment comment	Planning Solutions' response
Vehicle manoeuvring	
<p><i>The Towns 'Specification for the construction of crossovers' requires a maximum width of crossover at the property line of 10.7m. The internal dimension of the crossover adjoining Alice is proposed approximately 12.5m. It is noted that this crossover is required for combined access and to allow the fuelling vehicles to sufficiently manoeuvre into the site. However an argument could be presented that the site is undersize which has resulted in the excessively wide crossovers to permit adequate vehicle access.</i></p>	<p>The proposed development seeks to modify the existing crossover to Alice Street, by widening it to accommodate access into future 'Lot B'. The one crossover is effectively servicing the two future sites.</p> <p>Patrons of the convenience store will only use the northern 6.0m portion of the crossover. The southern 6.7m portion of the crossover will only be used by convenience store staff and delivery vehicles. The 12.7m wide consolidated arrangement of the crossover allows for the safe/efficient movement of fuel trucks.</p> <p>As confirmed by Transcore (traffic engineers):</p> <ul style="list-style-type: none"> - Irrespective of the size of future 'Lot A', the crossover size is dictated by the "design vehicle" (the largest vehicle accessing the site). - In these circumstances, the extra width required by the movement of large vehicles can be mountable or roll over kerb. - From a traffic engineering perspective, the proposed arrangement is the safest and most functional.
<p><i>Both the delivery truck and the fuel truck pass through bays allocated for fuel. The Town's Local Planning Policy No.6 makes reference to the maneuvering areas being exclusive of any car parking bays.</i></p>	<p>The manoeuvring requirement of LPP6 does not appear to have been explicitly written having regard for standard fuel retailing sites, where this is very common (noting the refuelling bays are only used for very short periods of time).</p> <p>The proposed circulation arrangements are acceptable for the following reasons:</p> <ul style="list-style-type: none"> - Delivery/service vehicles passing through fuel spaces is not uncommon at all for developments of this nature – including convenience store sites. - As detailed in our development application report, fuel deliveries will take place outside of peak hours of operation, having minimal disturbances on the internal traffic operation of the convenience store. - The frequency of deliveries (3-4 times per week for fuel, 1-2 times per week for stock and servicing), are very low over a 7 day period. <p>Having regard to the above, there is little to no likelihood of fuel/stock deliveries impacting on the site's internal operation.</p>
<p><i>Does the possibility exist for trucks to enter from Collier Road and exit from Alice and would the current access arrangements allow this? Why was the vehicle manoeuvring template presented to the Town that opted for the trucks to enter from Alice?</i></p>	<p>Trucks will only ingress the convenience store site via Alice Street and egress via Collier Road, due to the Collier Road access point being left-in/left-out and the location of the fuel filling point.</p>
<p><i>Can the delivery vehicle satisfactorily manoeuvre into the delivery area in reverse?</i></p>	<p>Refer to Appendix 1 for additional swept path plans, depicting service vehicles accessing the site and reversing into the loading area.</p>

Town's assessment comment	Planning Solutions' response
<i>Limited detail in the report regarding the upgrading of the pedestrian island on Collier Road. The applicant will be responsible for all costs associated to upgrading the island. This should be acknowledged by the applicant.</i>	Noted.
<i>As the fuel trucks leave the site onto Collier Road, entry will be prevented into the site by cars turning left from Collier and result in traffic backing up along Collier. The truck exiting will likely preoccupy both lanes of traffic. The report states that the trucks will only enter the site during off peak periods. In this regard the Town may look to request a condition regarding times that the fuel trucks may enter the site.</i>	<p>Fuel tanker deliveries would only occur 3-4 times per week (depending on fuel consumption and general demand) outside of peak period, which results in very minimal risk of traffic impact or disturbance on Collier Road.</p> <p>Given the extremely low likelihood of disturbances to the Collier Road traffic flow arising from low frequency of fuel tankers using the site, a condition is unnecessary.</p>
<i>Can you confirm the height of the trucks used for refuelling and the ability to pass under the canopy roof.</i>	The 4.5m available height clearance is in accordance with the highest clearance outlined in Table 2.1 (Design Vehicle Dimensions) of AS2890.2 for AV trucks of 19.0m in length.
<i>Please clarify that there is a minimum distance of 5.4m between each fuel bowser to permit the parking of vehicles in accordance with the Australian Standards.</i>	A minimum separation of 6.1m exists between fuel bowsers. Notwithstanding, the fuel bowsers and dispensing area is designed in accordance with AS1940 'The Storage and Handling of Flammable and Combustible Liquids'.
Fencing	
<i>The plans demonstrate solid Colourbond fencing all the way to the Collier Road boundary. In accordance with the Town's policy, no solid fencing should protrude forward of the 13m mark. The Town will likely request a condition be implemented reducing the height of the fence or removing it forward of the 13m mark.</i>	<p>This does not appear to be the type of development scenario for which the policy requirement relating to fencing within front setback areas was created.</p> <p>As future 'Lot A' will be a separate lot containing a commercial activity with no connection whatsoever to future 'Lot B', a fence along the entire property boundary is an appropriate form of separative treatment.</p> <p>Given the eastern portion of future 'Lot B' will comprise car parking to service the existing warehouse facility with no pedestrian connection to the proposed convenience store, there is no inherent planning or design purpose for restricting the proposed fence along the boundary in question.</p> <p>Having regard to the above, a concession to this policy requirement is warranted and acceptable.</p>
Landscaping	
<i>It is noted that variations are being proposed to the boundary Collier Road and side lot boundaries. The Town will likely request that a portion of the road reserve is soft landscaped to account for the proposed variation to landscaping in the first 2m. Regarding the minor variations to the western and southern sides of the lot, the Town will likely request a condition for a minimum number of plants to be provided in these areas on the landscaping plan.</i>	As the entire verge area fronting the subject site is already 'soft landscaped' with grass as depicted on the plans, it is considered unnecessary for any additional 'soft landscaping' to be required.
Storage and refuse	
<i>In accordance with the policy, the storage area must be masonry in construction materials and this will likely be</i>	The general intent of this policy requirement is acknowledged. However, in this particular instance, a concession is warranted

Town's assessment comment	Planning Solutions' response
<i>requested by way of condition.</i>	<p>for the following reasons:</p> <ul style="list-style-type: none"> - The service yard itself will be largely concealed from public view, given it is located between the convenience store retail building to the east and fence to the west, and the presence of an existing warehouse building to the south. As any masonry materials would not be visible from the street, the policy requirement would have little aesthetic value. - The retail building will be constructed of concrete panels and other materials. Requiring the storage yard to be constructed of masonry would be inconsistent with the built form of the retail building. - The service yard for all convenience store/service station developments is generally constructed of Colorbond with a gate for access.
Building materials	
<i>It has been noted in the report regarding the building materials but has not adequately been shown on the plans. The plans should make reference to the materials used in the construction.</i>	The building permit plans will confirm all relevant materials used for construction.
Signage	
<i>The Town has concerns with the extent and size of signage proposed as part of the application, specifically the 8m pylon sign adjoining Collier Road. The Town has noted the previous application for a 7-eleven at Caversham (in Swan) and the reduced size of the pylon signage on site. Observing a previous approved pylon sign that would meet the requirements and expectations of the Town of Bassendean's LPP18 and adequately cater for the advertising needs of the site, the Town will look for amendments to the proposed pylon signage.</i>	<p>Planning Solutions notes the following approved pylon sign heights for other service station/convenience store projects:</p> <ul style="list-style-type: none"> • Caltex Ascot (204 Great Eastern Highway): one 9m pylon sign. • 7-Eleven Wanneroo (929-931 Wanneroo Road): one 8m pylon sign. • Puma Balcatta (2 Kenhelm Street): one 8m pylon sign. <p>In this instance, the proposed 8m pylon sign is acceptable/appropriate for the following reasons:</p> <ul style="list-style-type: none"> • Collier Road in the vicinity of the subject site carries a reasonably high volume of traffic. An 8m high pylon sign would ensure adequate exposure for vehicles travelling along Collier Road. • The subject site is within an established industrial area, comprised of industrial activities on large lots of land. The size and scale of the sign is consistent with the industrial context of the locality. • Due to the existing industrial character and design of buildings along Collier Road, the proposed pylon sign would not have detrimental visual impacts. The pylon sign will be constructed of high quality materials and maintained to a high standard.
<i>The policy requires the projection details of each sign. Can you please update the plans to include projection details.</i>	<p>Projection details of signs as follows:</p> <ul style="list-style-type: none"> • S1 – 0.55m. Complies with 0.9m requirement.

Town's assessment comment	Planning Solutions' response
	<ul style="list-style-type: none"> • S2 – n/a (no part of the sign projects). • S3 – n/a (sign is a thin metal sheet and affixed to a fence/wall or pole, does not project). • S4 and S5 – n/a (signs are integrated into fascia, do not project). • S6 – n/a (sign is recessed into building tower, does not project). • S7 and S8 – n/a. • S9 – under 100mm.
<p><i>The plans incorrectly refer to S4 in regards to the proposed Air and Water Sign, please amend this to show S3.</i></p> <p><i>Please clarify the height of the air and water sign above the ground level on the plans.</i></p>	<p>The height of the air and water sign can be confirmed at the detailed design stage.</p>
Retaining wall	
<p><i>Retaining walls have been provided with a top level of 23.00, but the site only goes up to 22.5 on the eastern side of the site. The lot slopes down towards the road and therefore retaining should not be higher than the levels proposed within the site.</i></p>	<p>The proposed retaining wall is acceptable for the following reasons:</p> <ul style="list-style-type: none"> • An additional height of 0.5m is insignificant and will have no adverse planning impacts, or amenity impacts. • There is no applicable scheme or policy requirement restricting the height of the retaining wall. • From a functionality perspective, providing some type of barrier between vehicle manoeuvring areas and the adjoining verge (where the verge sits lower than the site) is common for fuel retailing developments.
<p><i>Please clarify the structure in the verge adjoining Collier Road adjoining the text that says '32.60m (side boundary)'.</i></p>	<p>The "structure" shown in the verge is the site's existing kerb, which will be removed at the time of construction.</p>

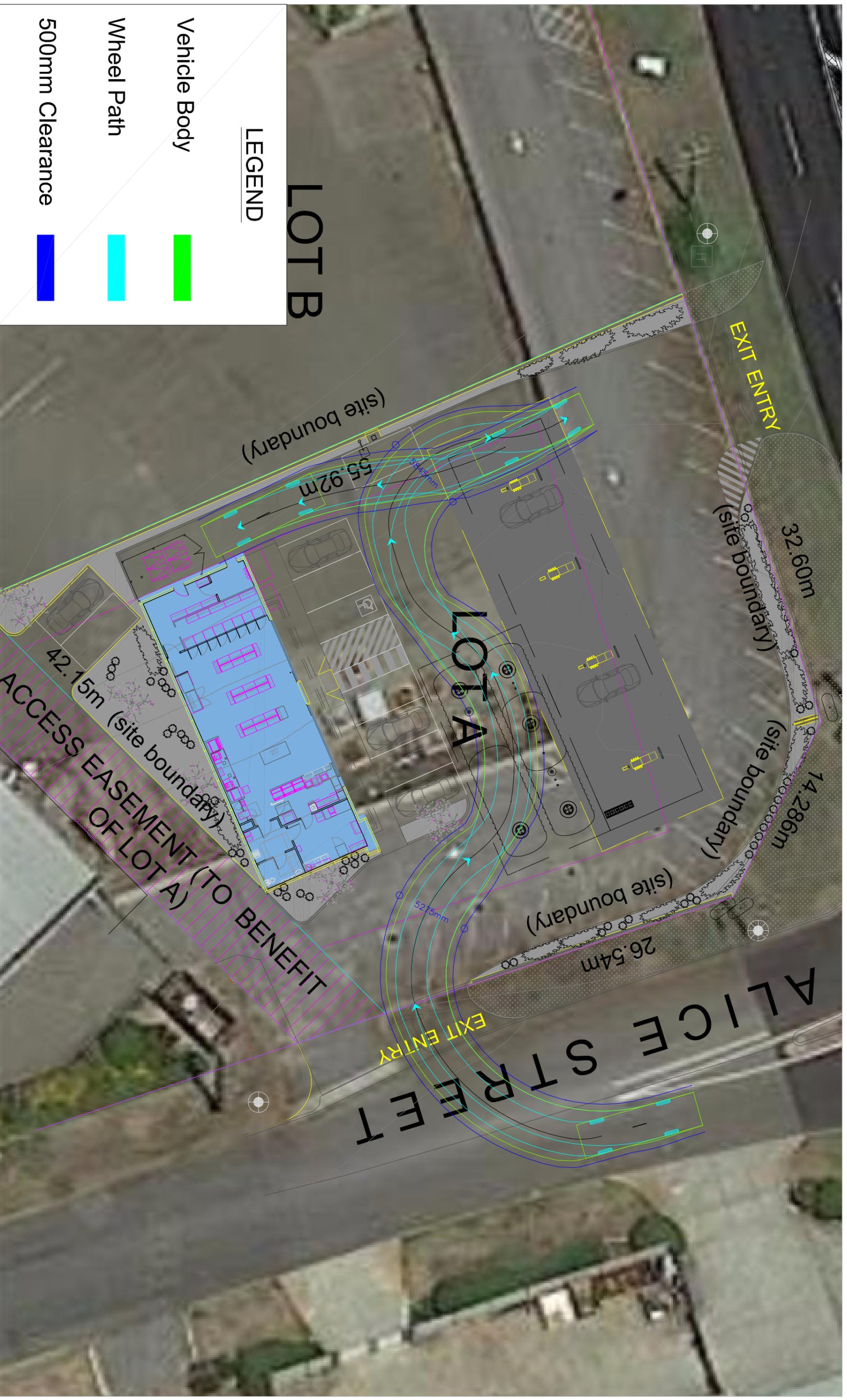
We trust the above satisfactorily addresses the Town's queries regarding the proposed convenience store application. We look forward to the Town's favourable recommendation to the Metro Central JDAP.

Should you have any queries, or wish to discuss the above in further detail, please don't hesitate to contact the undersigned.

Yours sincerely,



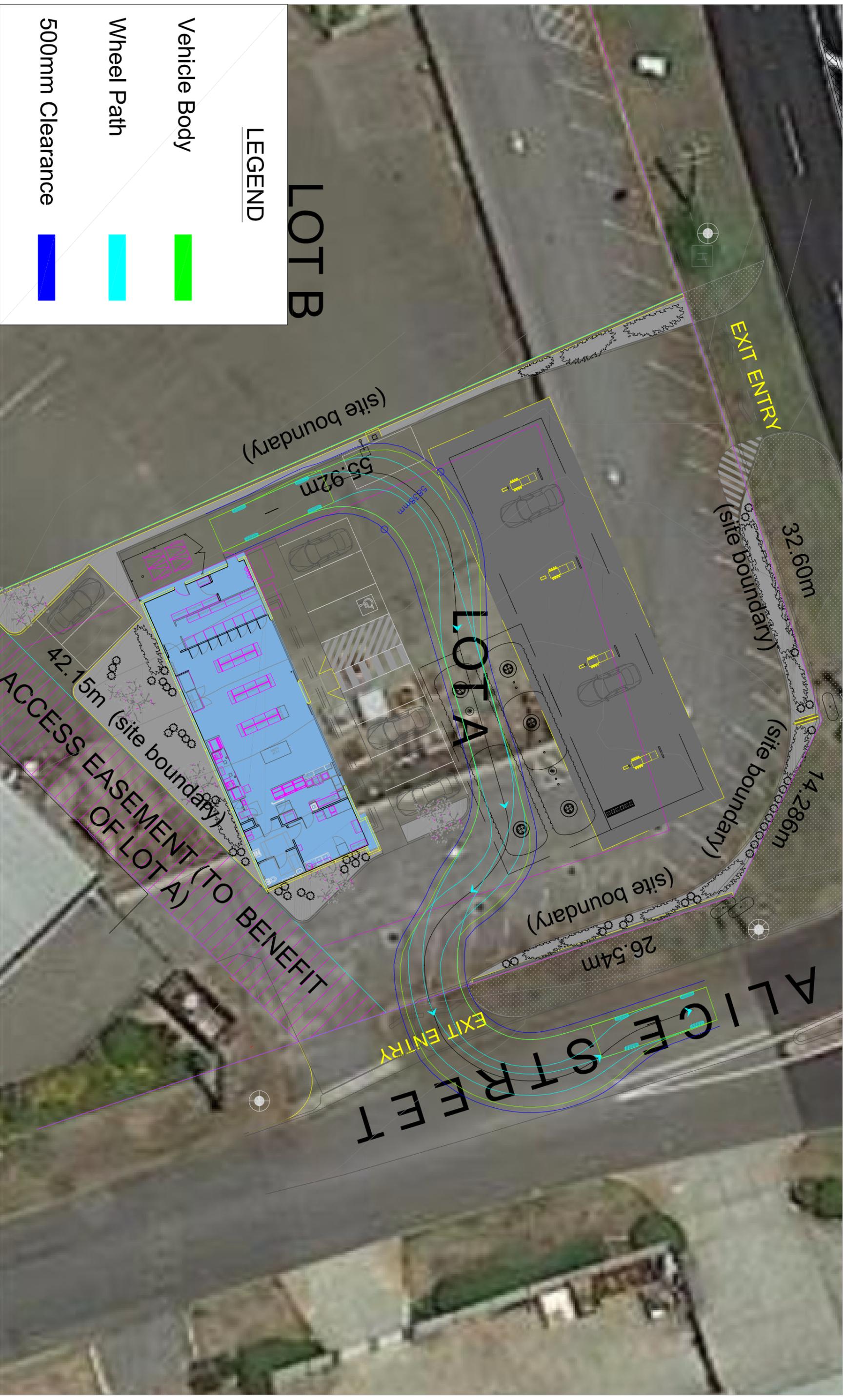
ALESSANDRO STAGNO
PLANNING CONSULTANT



Proposed 7-Eleven - Lot 300 Collier Road, Bassendean
 Austrroads 2013 - 8.8m Service vehicle
 Right turn into the site from Alice Street and reverse into the loading bay

t16.147.sk11
 7/04/2017
 Scale: 1/250 @ A3





Proposed 7-Eleven - Lot 300 Collier Road, Bassendean
 Austrroads 2013 - 8.8m Service vehicle
 Exit from the loading bay in forward gear and exit the site onto Alice Street

t16.147.sk12
 7/04/2017
 Scale: 1/250 @ A3





Your ref: 2017-033:BR
Our ref: 802/02/13/0004P
Enquiries: Jane Maynard (08) 6551 9259
Jane.maynard@planning.wa.gov.au
Date: 10 April 2017

Dylan Stokes
Town of Bassendean
PO Box 87
Bassendean
WA 6934

Dear Mr Stokes

LOT 25 (NO. 300) COLLIER ROAD, BASSENDEAN - DEVELOPMENT

I refer to your letter dated 9th March 2017 regarding the above application. In accordance with the Western Australian Planning Commission's (WAPC) Notice of Delegation dated 18 December 2015, the following transport comments are provided by the Department of Planning (DoP).

Proposed Development

Lot 25 covers approximately 1.4 ha of *Industrial* zoned land, and is currently occupied by a grouping of warehouses, as shown in **Figures 1** and **2**. The proposed development would see a *7-Eleven* convenience store and service station constructed over the existing car parking area in the eastern corner of the lot, as shown in **Figures 3** and **4**.

Subdivision plans

It is understood that a separate subdivision plan (File no. 154842) has been submitted for Lot 25, for the creation of two lots, (Lots A and B), as shown in **Figure 5**. The proposed development is to be located within proposed Lot A.

A separate DA has also been submitted to the Department for extensions to the existing office area within proposed Lot B, as shown in **Figure 6**.

Land Requirements

The subject property abuts Collier Rd, which is reserved as an Other Regional Road (ORR) in the Metropolitan Region Scheme (MRS), and a Category 2 ORR¹ in WAPC

¹ **ORR Categories:** WAPC Plan No. SP 694/4 uses the following ORR categories; **Category 1:** frontage access is not allowed (control of access). **Category 2:** frontage access may be allowed subject to approval. **Category 3:** road reservation not accurately defined or under review. http://www.planning.wa.gov.au/dop_pub_pdf/devwapca_2_2A0.pdf



Plan No. SP 694/4. Lot 25 is not affected by the ORR reservation for Collier Rd, as shown in **Figure 1**.

Access

Vehicle access to Lot 25 is currently provided via two full movement crossovers onto Collier Rd and one onto Alice St, as shown on **Figure 1**, however, given that the lot is to be subdivided; neither the Collier Rd, nor the Alice St crossovers will be available to the proposed convenience store.

To rectify this, the applicant seeks to construct a new left-in, left-out crossover onto Collier Rd; and to widen the existing Alice St crossover so that it extends into proposed Lot A, as shown in in **Figure 4**.

The subdivision application also seeks to create an easement along the southern portion of proposed Lot B to give Lot A access to the existing Alice St crossover.

As the proposed development would see the existing Alice St crossover widened six meters into proposed Lot A, the easement would function to provide Lot A with access to a 12.7m crossover, as shown in **Figure 4**.

The DA report advises that the proposed crossover onto Collier Rd will be Left-in, Left-out (LILO), and that this will be formalised through an extension of the existing median strip. The Department supports this proposal, and advises that this should be completed before the new crossover is constructed.

Fuel tanker access

The DA report advises that the fuel tankers will enter via Alice St and exit via Collier Rd. The Department has no objection to the proposed access arrangements, but recommends access for fuel tankers be limited to outside peak times.

Transport Impact Assessment

A Transport Impact Assessment (TIA), dated 14th February 2017 has been prepared by Transcore in support of the proposal. The following comments relate to Transcore's TIA.

Traffic Generation from proposed development

The development site, which is 1,828 m², as shown in **Figure 5**, will consist of the following:

- The convenience store building;
- Air and water bay;
- Loading bay;



- Eight fuel filling bays;
- Seven car parking bays including one disabled;
- Two staff car parking bays.

A total of 19 vehicles therefore can be accommodated on the site at any one time.

Transcore have estimated trip generation rates by using transaction data on previous 7-eleven stores, as shown in **Figure 6**.

To obtain trip generation estimates, data on the number of transactions made at the surveyed 7-eleven stores was collected, and vehicle trips were assumed to make up 95% of all transactions, (with 5% of transactions being from walk-ins, rather than vehicle visits).

The estimated peak hour trips to and from the site are shown shown in **Table 1**.

Table 1. Peak hour trips for proposed development

Time period	Direction	Total Peak Hour Trips	
		Split	Total
AM Peak	Inbound	45	90
	Outbound	45	
PM Peak	Inbound	62	124
	Outbound	62	

The TIA then estimates that 70% of vehicles entering and exiting the site would likely be passing traffic, with only 30% generated by the business itself.

From here the TIA calculates that the development will generate an estimated 537 vehicles per day (vpd), 28 vehicles per hour (vph) in the AM, and 38 vph in the PM peak hour.

Comparisons with other trip generation rates from the Roads and Traffic Authority (RTA) *Guide to Traffic Generating Developments* and the Institute of Transport Engineers (ITE) *Trip Generation Manual*, arrive at PM peak hour estimates of 130 vph² and 152 vph³ respectively; compared with 124vph, as stated above.

The TIA does not specify how many 7-eleven stores were surveyed to obtain source data; the location of the stores surveyed, or whether they all included service station facilities. It is therefore recommended that future TIAs include this information.

The Department has no objection to the methods used to estimate trip generation rates, but recommends additional information on the data source be provided in future.

² RTA (2002) estimates taken from page 3.7, using the formula of 0.04 x site area + 0.3 x GFA.

³ ITE (2012). Page 1672. Uses formula of 19.07 trips per vehicle fuelling positions.



Distribution of traffic onto surrounding roads

The subject lot abuts Collier Rd to the north and Alice St to the east. Collier Rd is a dual divided carriageway with a posted speed of 70km per hour.

The proposed crossover is located approximately 600m from the intersection with Tonkin Hwy; where upgrades are planned as part of the Northlink road project, as shown in **Figure 8**; and 350m from intersection with Jackson St.

The TIA provides data from the Main Roads WA (MRWA), as shown in **Figure 9**, which shows the PM peak (between 3:00 and 4:00 pm) to have recorded the highest volumes, at 1,500vph, of which 14% were heavy vehicles.

Trip distribution estimates are provided in **Figures 10 – 14**, which break the figures into existing traffic; traffic that is diverted by the proposed development; traffic generated by the development and total estimated post development figures (shown as AM and PM peak figures).

Impact on surrounding intersections

SIDRA analysis has been carried out for the Collier Rd / Alice St intersection as follows:

	Degree of Saturation	Average Delay	Level of Service
Existing AM Peak	0.229	1.7	A - E
Existing PM Peak	0.432	2.8	A - F
Post Development AM Peak	0.241	2.4	A - E
Post Development PM Peak	0.710	4.9	A - F

While the overall Level of Service (LoS) is high in all scenarios, the right turn movement from Alice St onto Collier Rd, (eastbound) as shown in **Figure 1**, shows an LoS of *E* in the AM peak and *F* in the PM peak in both the existing in post development analysis, and delays of up to 94.6 seconds.

The Department of Planning therefore recommends the Town of Bassendean, together with the applicant, give consideration to upgrading the Alice St / Collier Rd intersection.

Vehicle & bicycle parking

The TIA discusses vehicle parking; noting that the site will consist of one air and water bay, one loading bay, seven car parking bays, two staff car parking bays, and eight fuel filling points. No bicycle parking facilities are proposed.

The Department of Planning recommends the Town of Bassendean, together with the applicant, give consideration to whether bicycle parking facilities should be provided, and amend the plans accordingly if applicable.



Bicycle access & movement

The TIA addresses bicycle access and movement; noting that a shared path exists on the other side of Collier Rd, which has links with the principal shared path adjacent to the Midland railway line. There are no other shared paths, footpaths, or bicycle paths along this stretch of Collier Rd.

Pedestrian access & movement

The TIA discusses pedestrian access, noting that there are no footpaths on Alice St or Collier Rd immediately adjacent to the subject lot, but advises that a 2m wide footpath is proposed along Collier Rd.

The TIA refers to the site plan shown at **Figure 4** as evidence of this, however the site plan does not show any footpaths. It is therefore recommended that any future iterations of the TIA be updated to correct for this omission.

Signage

The proposed development includes signage, as shown in **Figures 15** and **16**, and none⁴ of the proposed signage is located within the ORR reserve for Collier Rd.

The Department has no objection to the proposed signage, on condition that the advertisements do not interfere with sight lines, distract drivers, or have the potential to become confused with traffic signals or road signs. All signage must comply with all relevant by-laws and planning schemes made by Council.

Fuel tanker access and swept paths

It is understood that the swept paths shown in **Figure 17** are based on the movements of a 19m long fuel tanker. The Department recommends consideration be given to making minor modifications as follows:

- Reduce the verge kerb at the proposed Alice St and Collier Rd crossovers, and instead use paint treatment to guide smaller vehicles;
- Straighten the (currently flared) kerb adjacent to the boundary between the two proposed lots;
- Ensure that the kerb inside the Alice St entrance which defines the beginning of the parking bays is either painted or mountable.

These recommendations are shown in red on the marked up map shown at **Figure 17**.

⁴ It is noted that there is a drafting error in **Figure 15** which could potentially be interpreted as showing that parts of the signage are to be located within the road reserve, however discussions via email with the applicant on 7th April 2017 have confirmed that no part of the development or signage is to be located outside the lot boundary.



Conclusion

The proposal seeks development approval for the construction of a convenience store and service station. The plan would also see a new vehicle crossover onto Collier Rd.

The Department has no objection to the construction of the new crossover onto Collier Rd, on the condition that it is not constructed before the extension of the median strip (to formalise LILLO access) is completed.

The Department has no objection to the proposed signage, on condition that the advertisements do not interfere with sight lines, distract drivers, or have the potential to become confused with traffic signals or road signs. All signage must comply with all relevant by-laws and planning schemes made by Council.

The Department also offers the following recommendations:

- Fuel tanker access should be permitted only outside of peak times.
- It is recommended that the Town of Bassendean work with the developer to upgrade the intersection of Collier Rd and Alice St, to improve the poor LoS in the right turn movement from Alice St.
- It is also recommended that the Town of Bassendean work with the developer in investigating whether bicycle parking facilities should be included, and modify the plans accordingly if required.
- Given the restrictive movement currently available for fuel tankers, it is recommended minor modifications be made to the plans to reduce the likelihood of fuel tankers hitting the kerb, as shown in **Figure 17**.

The Department has no objection to the proposal, subject to the above.

Sincerely

A handwritten signature in blue ink that reads "moshinmuttaqui".

Moshin Muttaqui
Planning Manager
Infrastructure & Land Use Coordination

Figure 1. MRS and aerial maps of subject lot



Subject Lot

Other Regional Road

crossover

movement with the longest delays, lowest LoS and highest saturation

Figure 2. Location map

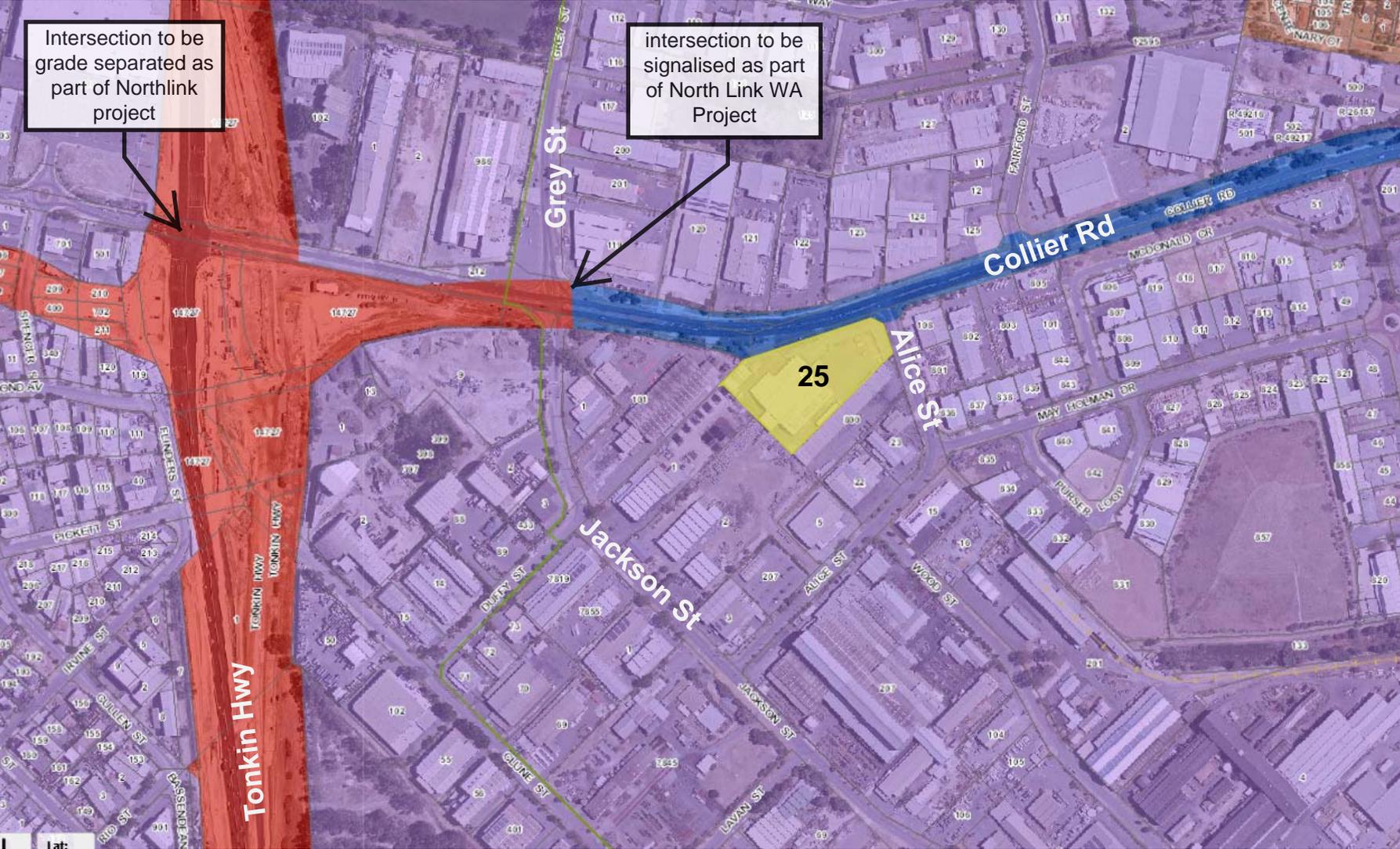
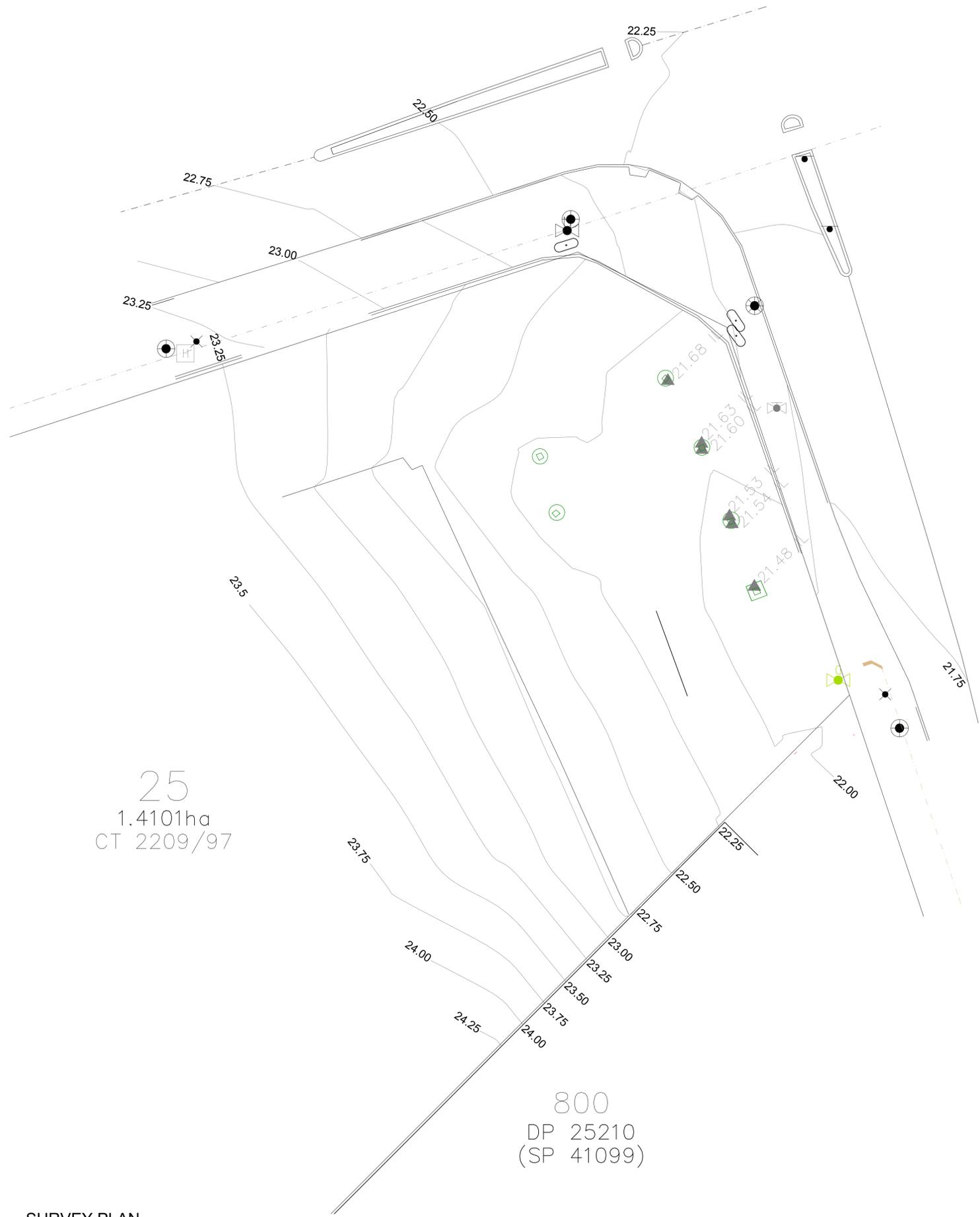


Figure 3 - Locality Plan



LOCALITY PLAN
SCALE 1:500



SURVEY PLAN
SCALE 1:200

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Rev No.	Description	Date	By
C	SITE AREA-EASEMENT COLLIER RD MEDIAN DA ISSUE	13.02.17	RFH
B		03.02.17	RFH
A		13.12.16	RFH



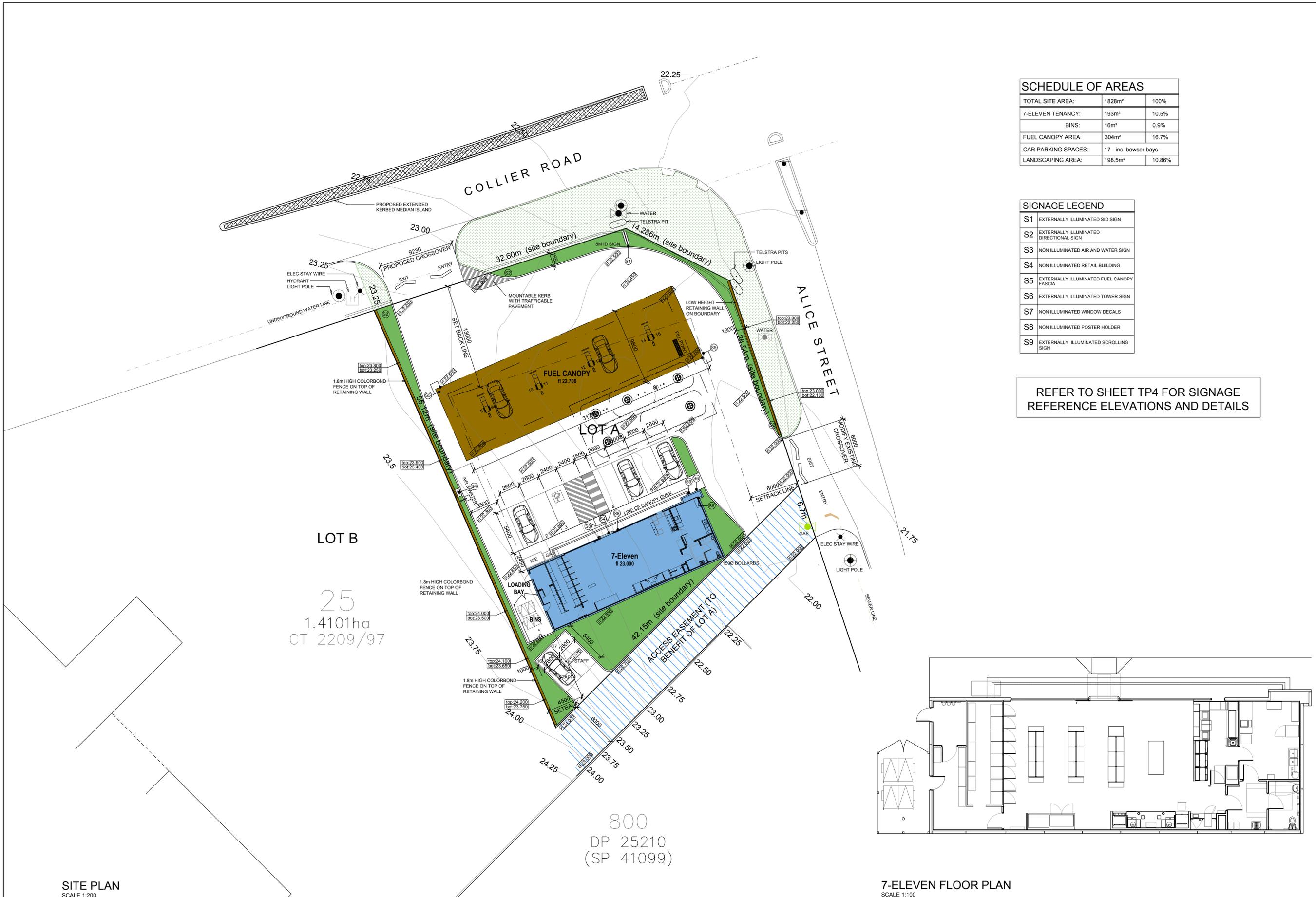
Project **PROPOSED 7-ELEVEN CONVENIENCE STORE**
300 COLLIER ROAD, BASSENDEAN WA

Client **7-ELEVEN**

Sheet **LOCALITY PLAN SURVEY PLAN**

Client	RFH	
Dm	PARA	
Date	DECEMBER 2016	
Scale	AS SHOWN	
Surveyor	AS SHOWN	
Approved		<small>39 Woodley Rd Glen Iris WA 6111 439 89796</small> 1616-TP1

Figure 4 - Site Plan



SCHEDULE OF AREAS		
TOTAL SITE AREA:	1828m ²	100%
7-ELEVEN TENANCY:	193m ²	10.5%
BINS:	16m ²	0.9%
FUEL CANOPY AREA:	304m ²	16.7%
CAR PARKING SPACES:	17 - inc. bowser bays.	
LANDSCAPING AREA:	198.5m ²	10.86%

SIGNAGE LEGEND	
S1	EXTERNALLY ILLUMINATED SID SIGN
S2	EXTERNALLY ILLUMINATED DIRECTIONAL SIGN
S3	NON ILLUMINATED AIR AND WATER SIGN
S4	NON ILLUMINATED RETAIL BUILDING
S5	EXTERNALLY ILLUMINATED FUEL CANOPY FASCIA
S6	EXTERNALLY ILLUMINATED TOWER SIGN
S7	NON ILLUMINATED WINDOW DECALS
S8	NON ILLUMINATED POSTER HOLDER
S9	EXTERNALLY ILLUMINATED SCROLLING SIGN

REFER TO SHEET TP4 FOR SIGNAGE REFERENCE ELEVATIONS AND DETAILS

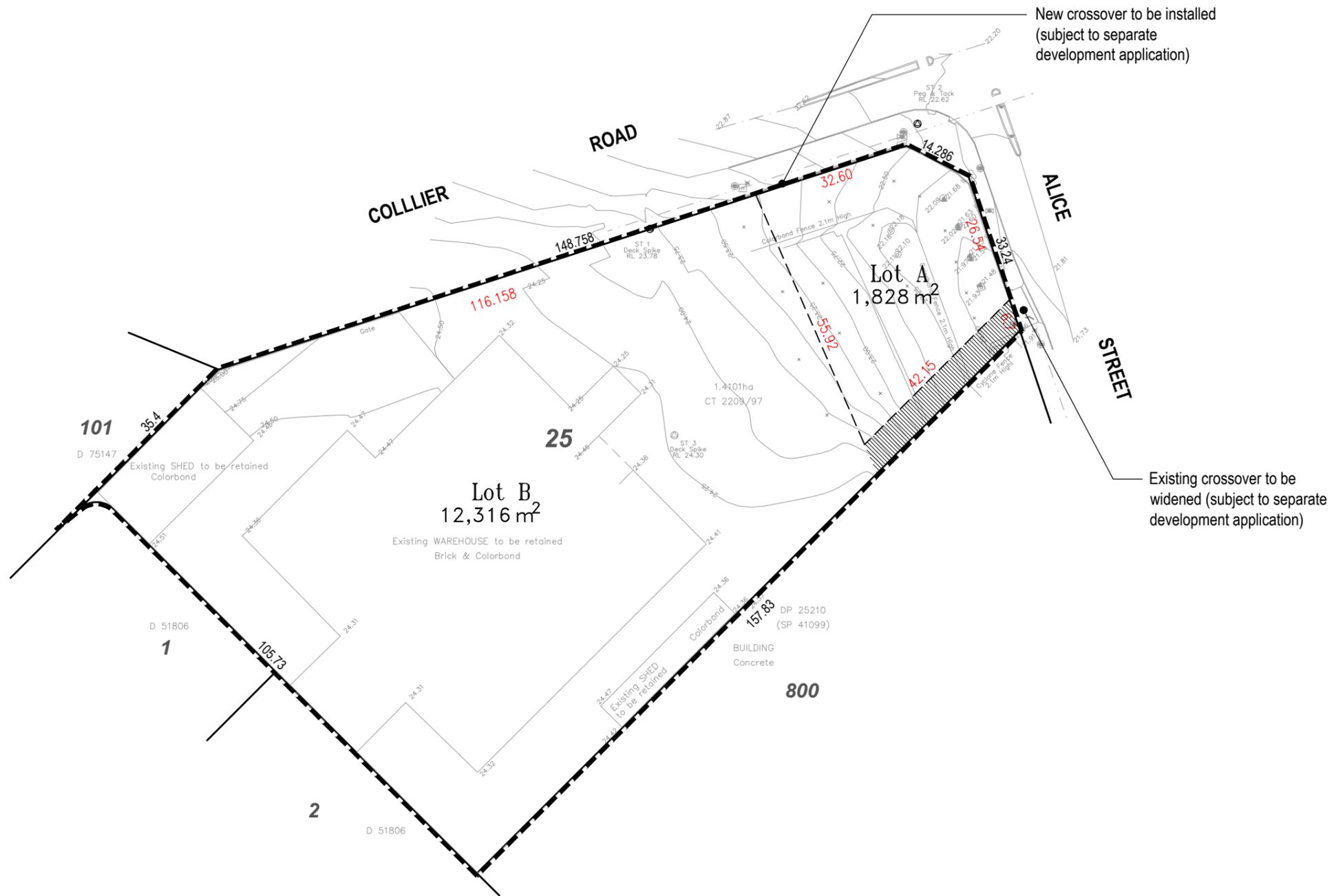
SITE PLAN
SCALE 1:200

7-ELEVEN FLOOR PLAN
SCALE 1:100

Figure 5 - Subdivision proposal no. 154842

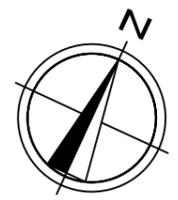
LEGEND	
	Subject Site
6	Existing Lot Number
	Existing Lot Boundary
Lot A	Proposed Lot Number
	Proposed Lot Boundary
110	Existing Lot Dimensions
110	Proposed Lot Dimensions
	Access Easement (to benefit of Lot A)

SUBDIVISION SUMMARY	
Subject Site (ha)	1.4144
No. of Existing Lots	1
No. of Proposed Lots	2
Total Area of Proposed Lots (m ²)	14,144
Average Lot Size (m ²)*	7,072
Minimum Lot Size (m ²)	1,828
Maximum Lot Size (m ²)	12,316

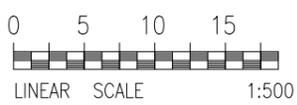
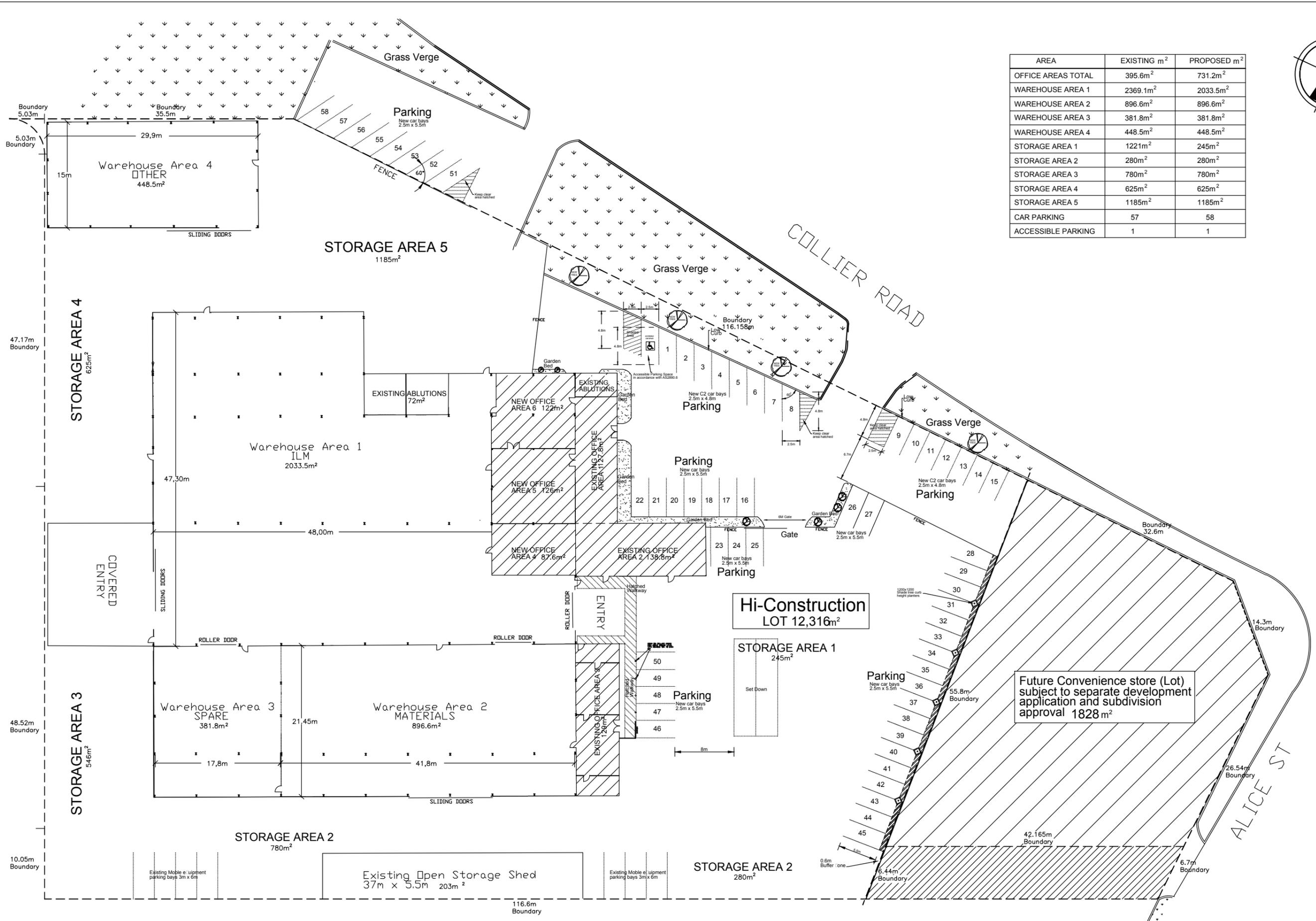


DEPARTMENT OF PLANNING	
DATE	FILE
17/02/2017	154842

Figure 6 - Adjacent Development Application



AREA	EXISTING m ²	PROPOSED m ²
OFFICE AREAS TOTAL	395.6m ²	731.2m ²
WAREHOUSE AREA 1	2369.1m ²	2033.5m ²
WAREHOUSE AREA 2	896.6m ²	896.6m ²
WAREHOUSE AREA 3	381.8m ²	381.8m ²
WAREHOUSE AREA 4	448.5m ²	448.5m ²
STORAGE AREA 1	1221m ²	245m ²
STORAGE AREA 2	280m ²	280m ²
STORAGE AREA 3	780m ²	780m ²
STORAGE AREA 4	625m ²	625m ²
STORAGE AREA 5	1185m ²	1185m ²
CAR PARKING	57	58
ACCESSIBLE PARKING	1	1



LOT NO	AREA m ²	SITE BOUNDARY
25A	2079	---
25B	12,065	---

NEW SITE PLAN

PROJECT 300 COLLIER ROAD, Bassendean, Perth WA			
DWG:TITLE	SITE PLAN PROPOSED SITE PLAN	DWG:NO	DWG:REV Design D.H
DATE	NOV. 23	SCALE	1 : 500
FILENAME		PROJECT NO:	
PLOT REF:	1-1 at A.3		



Figure 7 - TIA excerpt: 7-Eleven Customer Patronage
Hourly customer demand profile (average weekday)

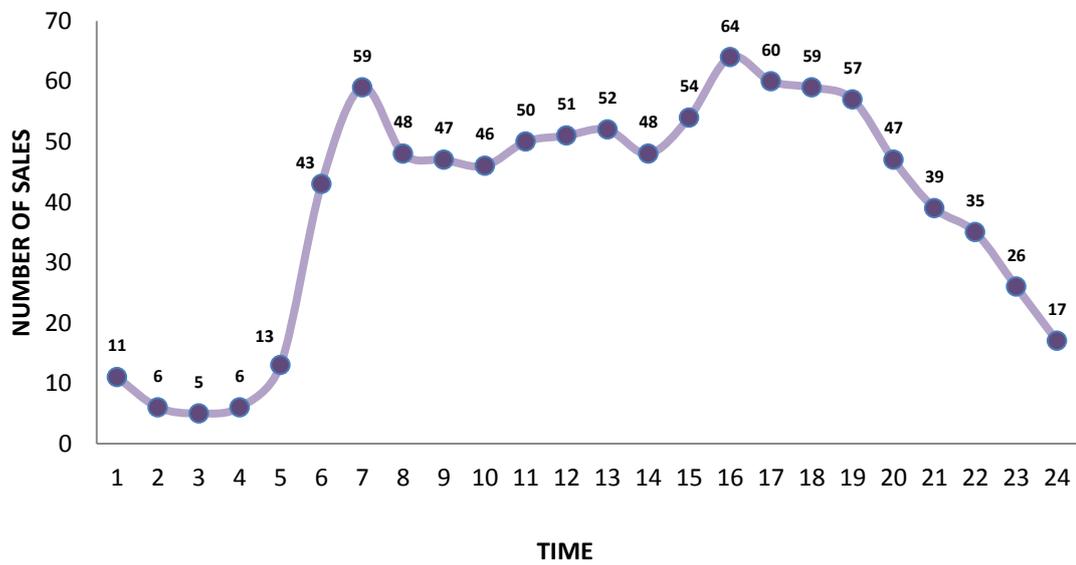


Figure 8 - NorthLink

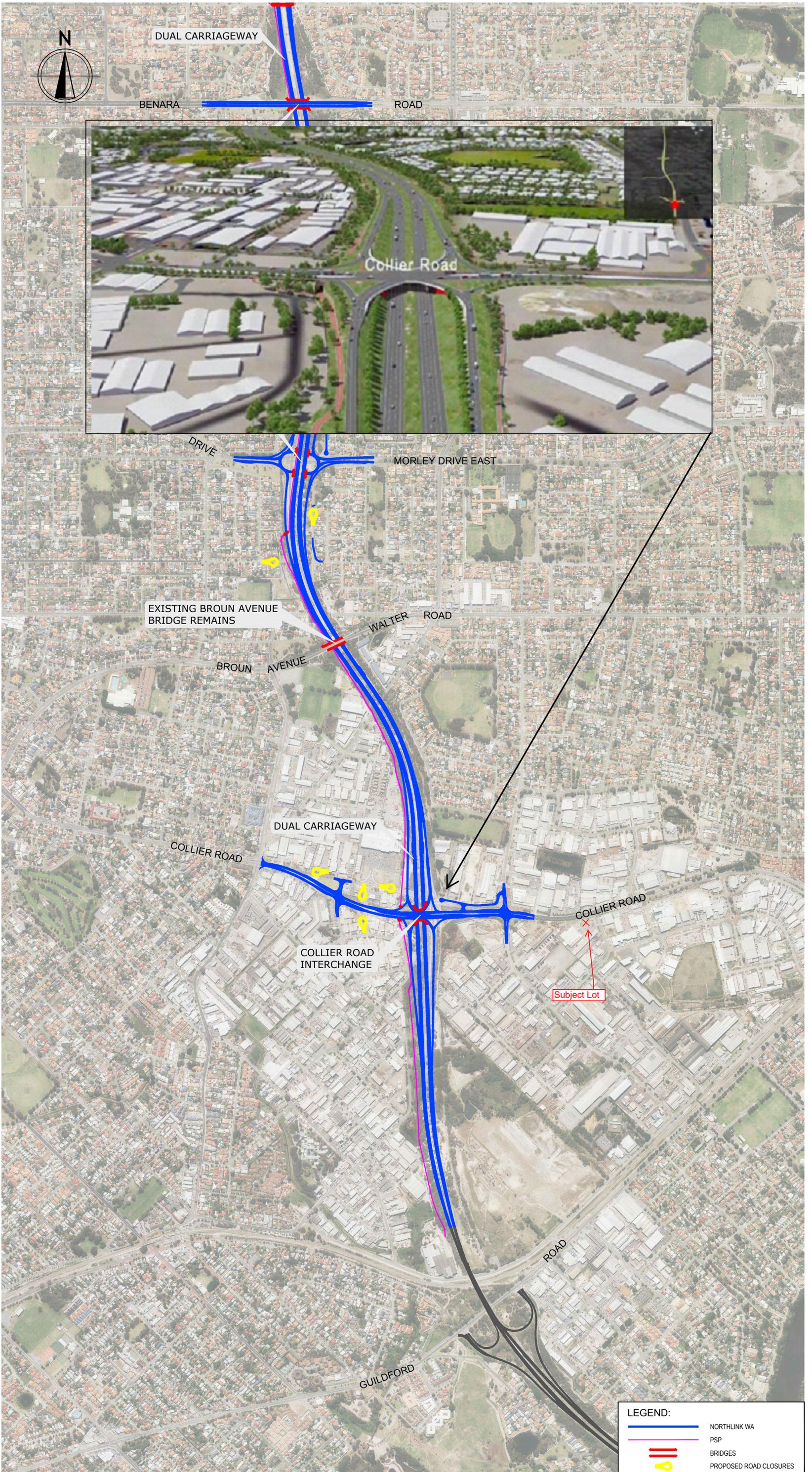


Figure 9 - Main Roads WA Traffic map counts

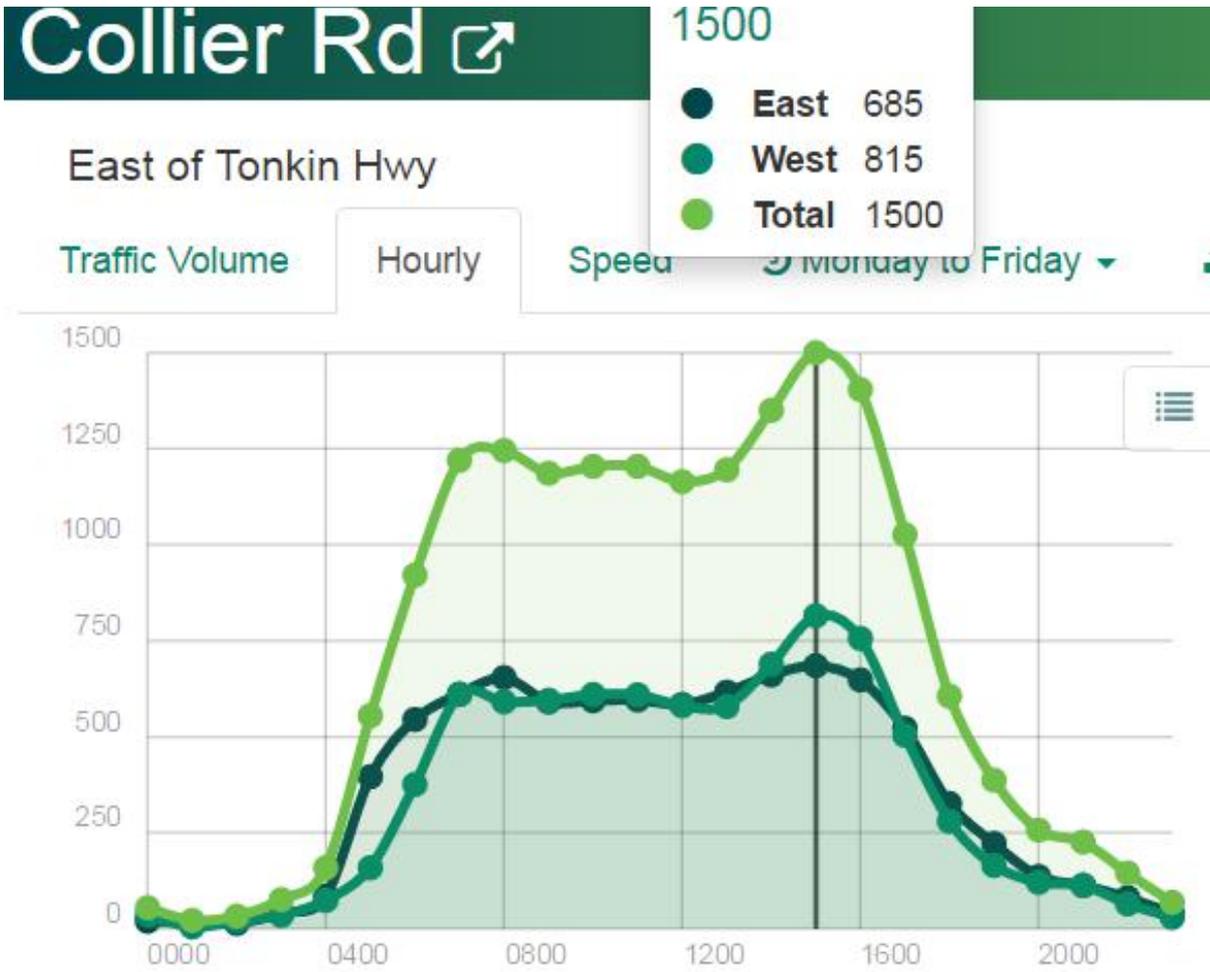


Figure 10. Passing trade peak hour traffic diverted by the proposed development - AM and PM hour

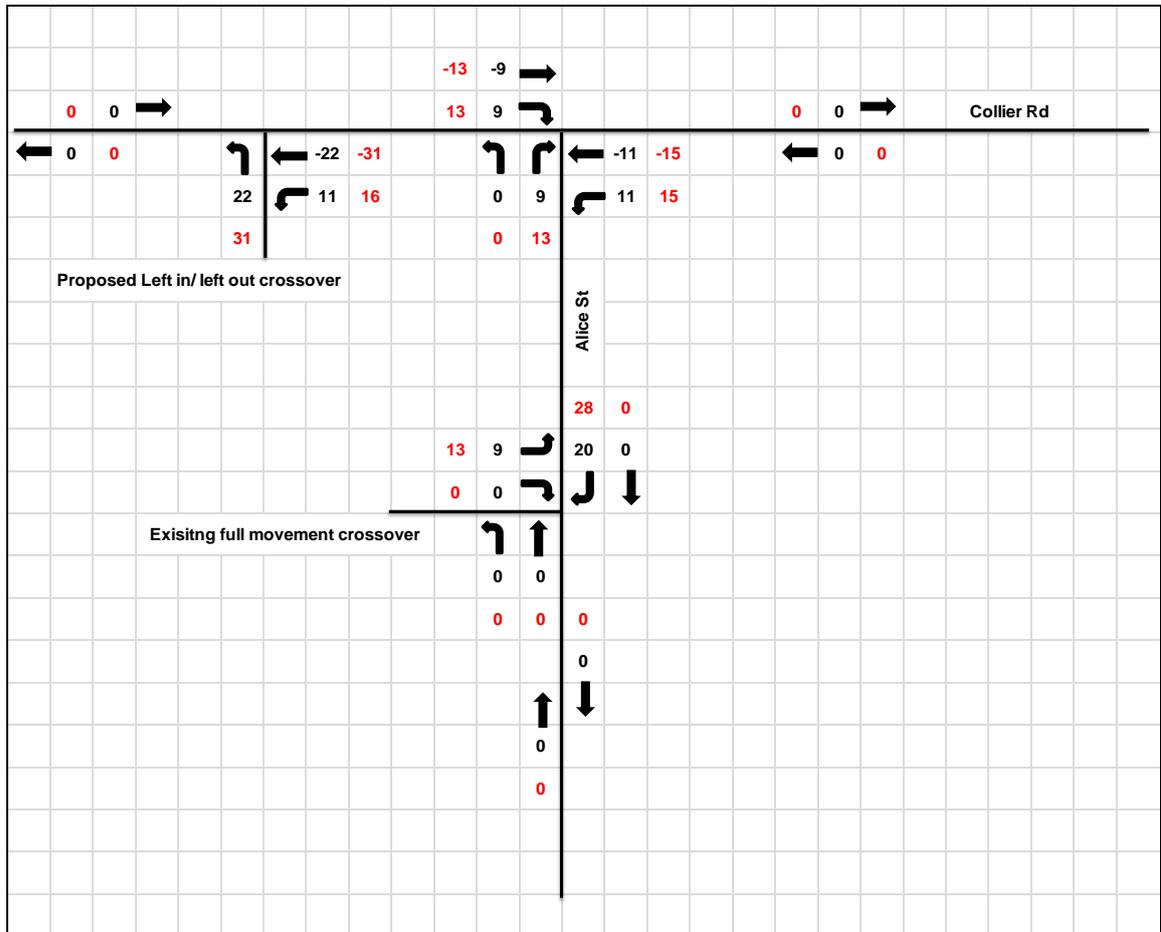


Figure 11: Additional (non-passing trade) AM and PM peak hour traffic generated by the proposed development

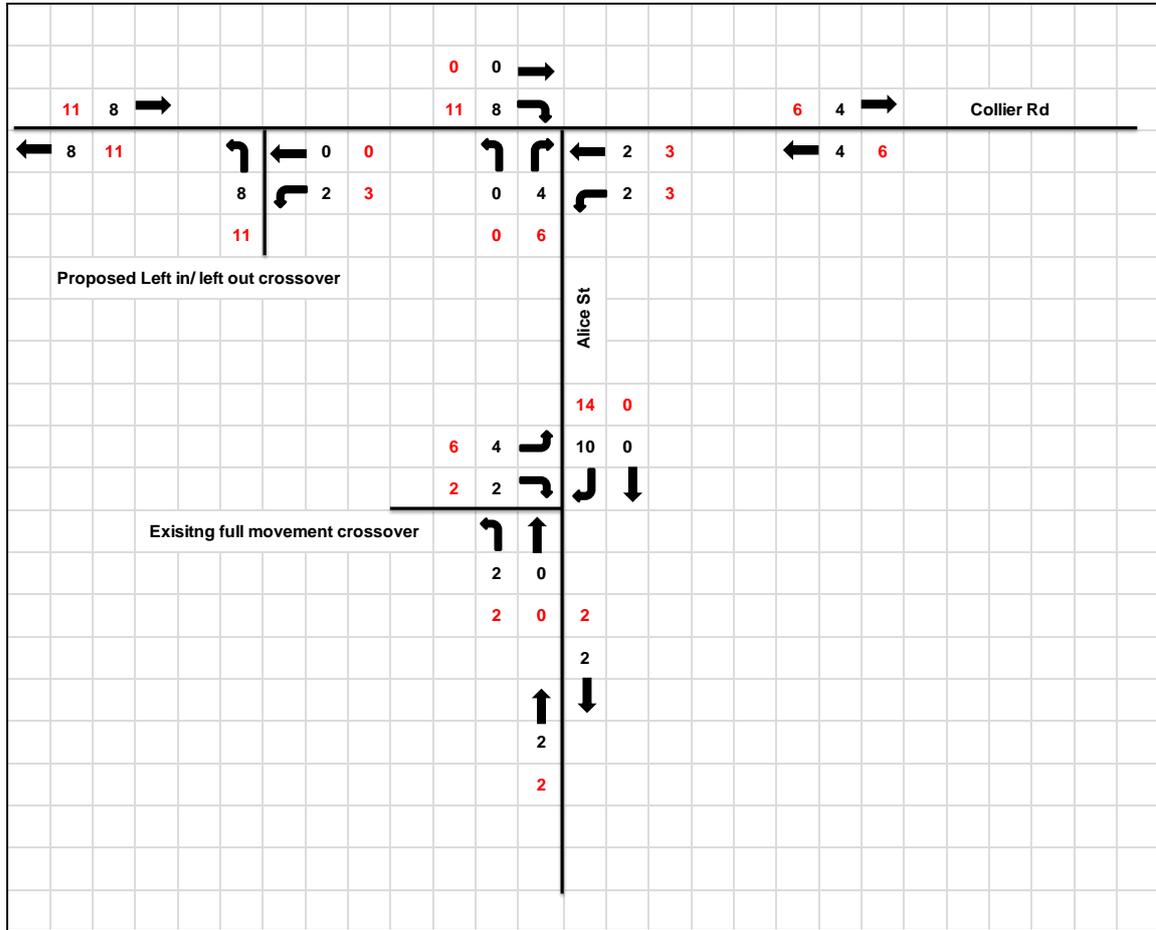


Figure 13: Existing AM and PM peak hour traffic flows

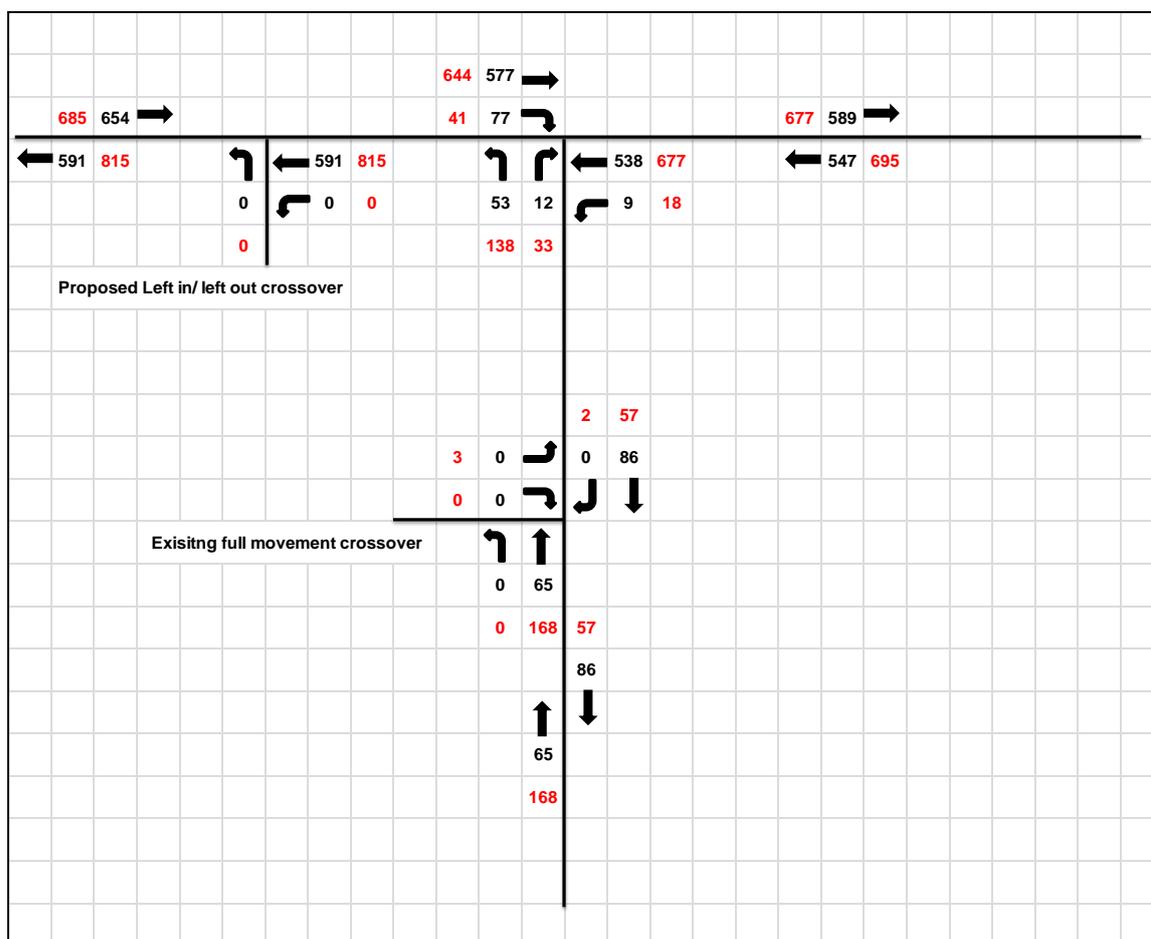


Figure 14: Total AM and PM peak hour traffic flows – Immediately post development scenario

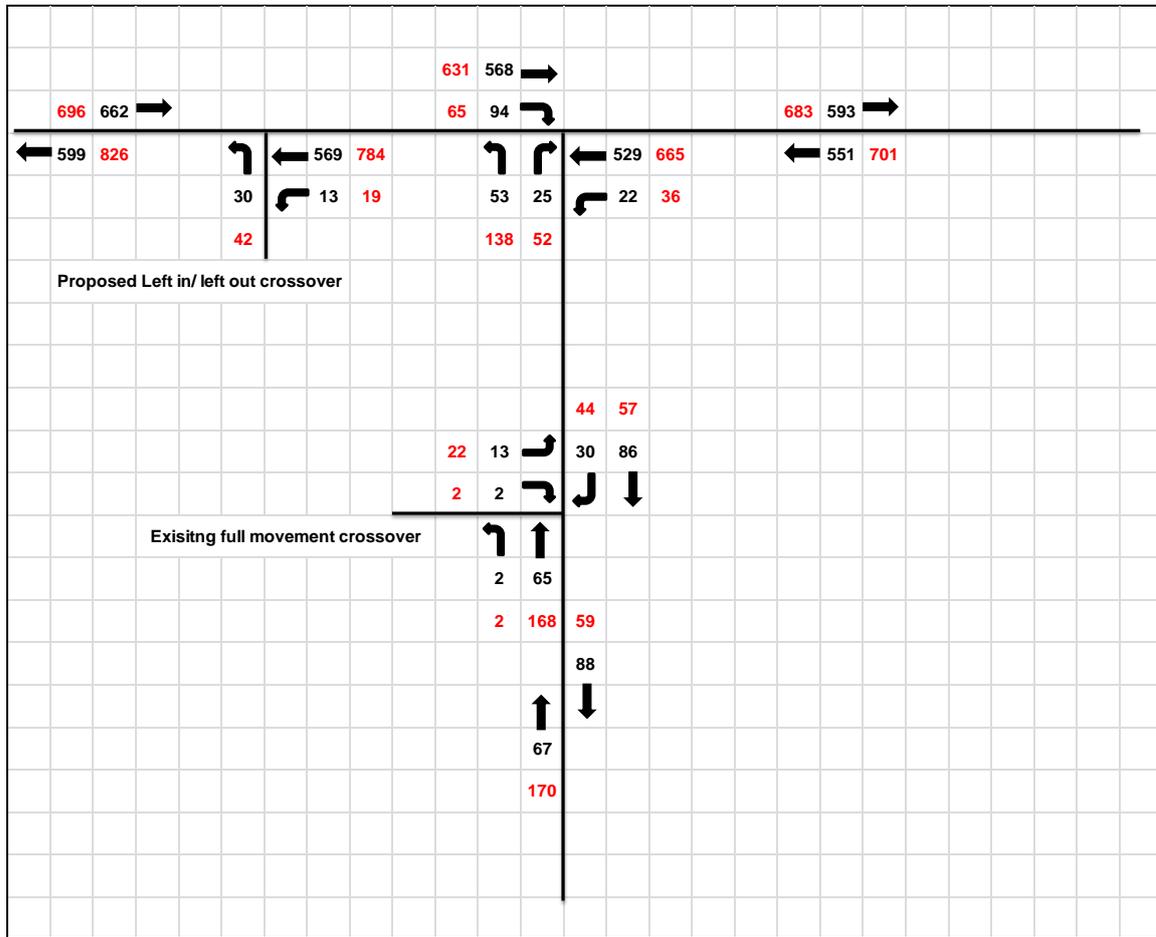
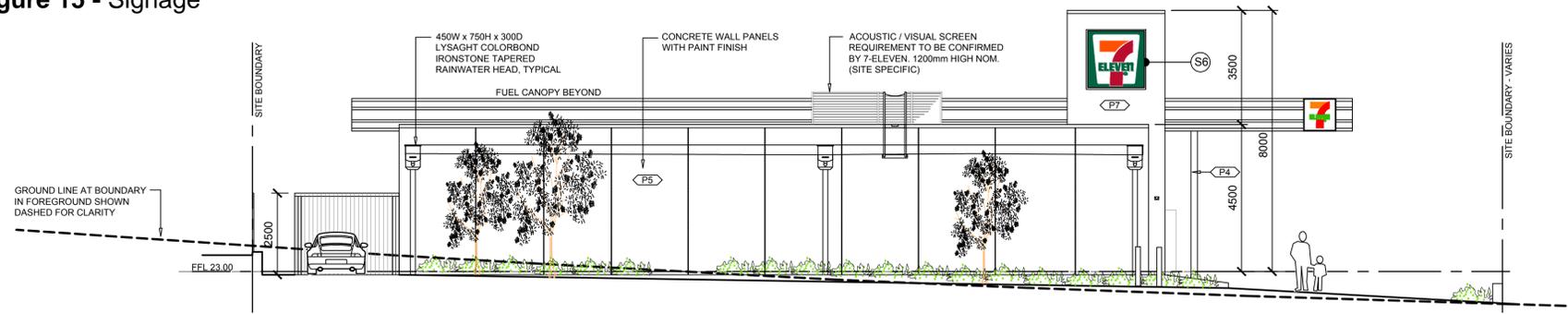
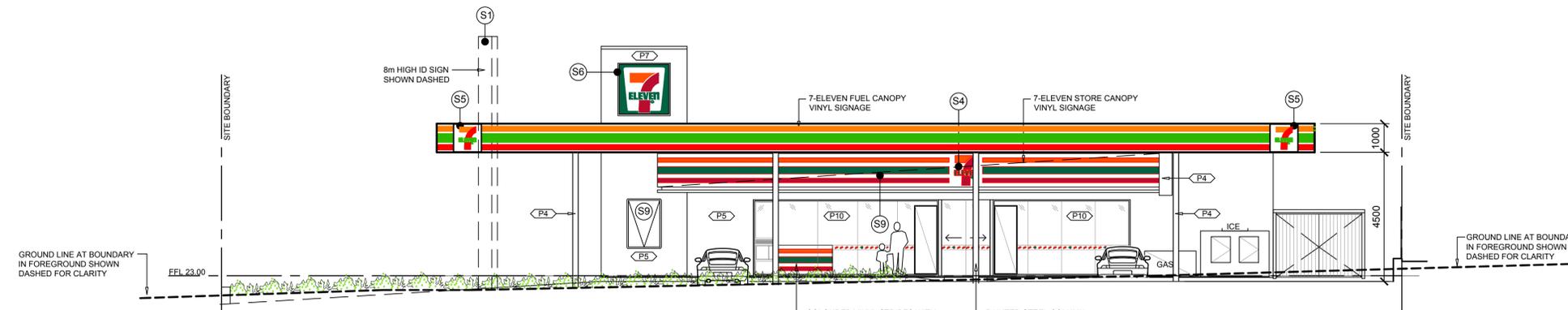


Figure 15 - Signage

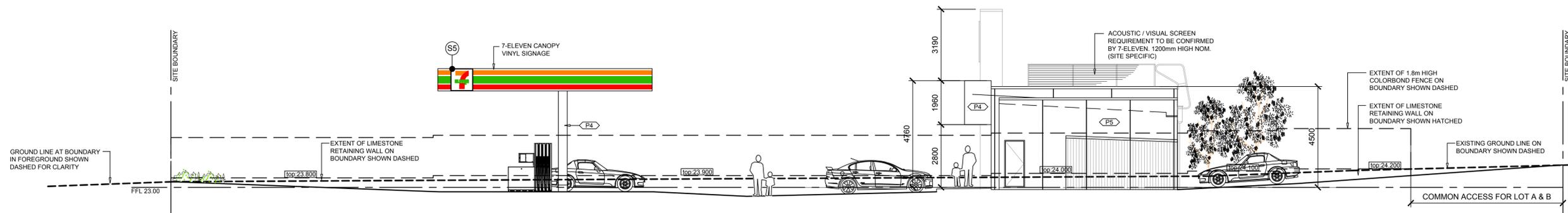


SOUTH EAST ELEVATION (convenience store)
Scale 1:100

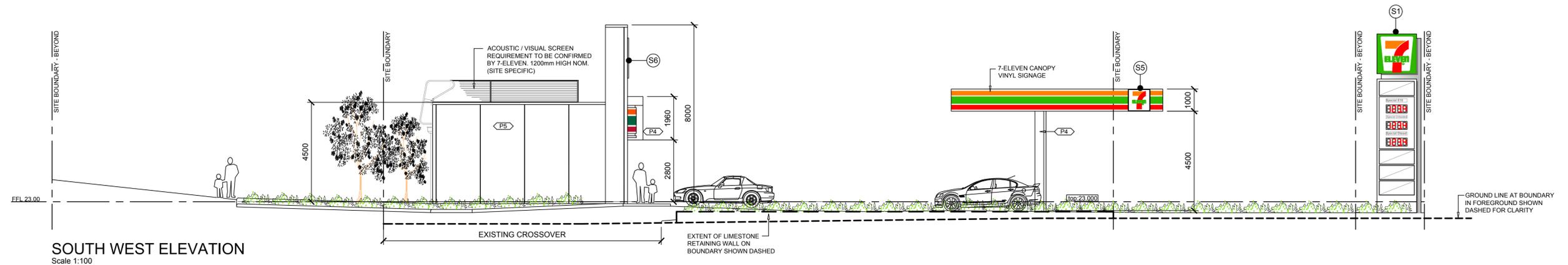
CODE	FINISH SPECIFICATION	LOCATION
P2	150x150 ROUGH SAWN POSTS COLOUR: CREAM	VERANDAH POSTS FASCIA & GUTTERS
P3	CORRUGATED IRON COLOUR: COLORBOND SLATE GREY	ROOF CLADDING
P4	DULUX WEATHERSHIELD X10 GLOSS ACRYLIC COLOUR: VIVID WHITE PW1H9	7-ELEVEN PYLON POLES, LIGHTING POLES, VENT PIPES, PETROL CANOPY COLUMNS & DOWNPIPES
P5	TEXTURED (ROUGH) FINISH GREY COLOUR	BUILDING - EXTERNAL CONCRETE WALL PANELS
P6	DULUX WEATHERSHIELD X10 GLOSS ACRYLIC COLOUR: BLACK	STEEL CANOPY COLUMNS AND BOLLARDS 300mm ABOVE GROUND LEVEL
P7	TIMBER LOOK CLADDING TO SIGNAGE TOWER	BUILDING TOWER CONCRETE WALL PANELS
P10	CLEAR GLAZING IN A POWDERCOATED ALUMINIUM FRAME WITH FRAMELESS MULLIONS	SHOPFRONT



NORTH WEST ELEVATION (fuel canopy)
Scale 1:100



NORTH EAST ELEVATION
Scale 1:100



SOUTH WEST ELEVATION
Scale 1:100

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Rev No.	Description	Date	By
B	RETAINING RL'S	19.12.16	RFH
A	DA ISSUE	13.12.16	RFH

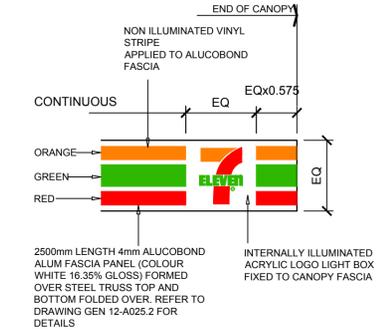
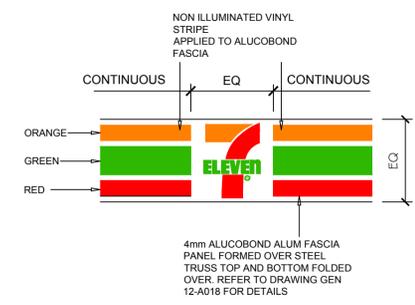
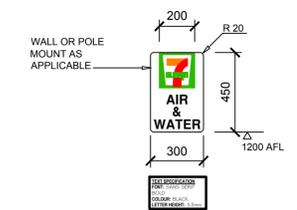
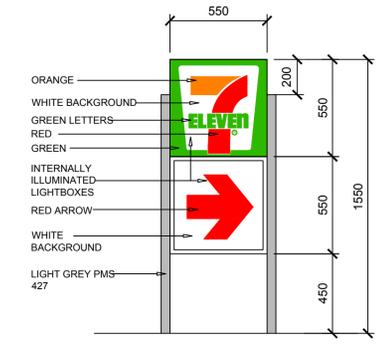
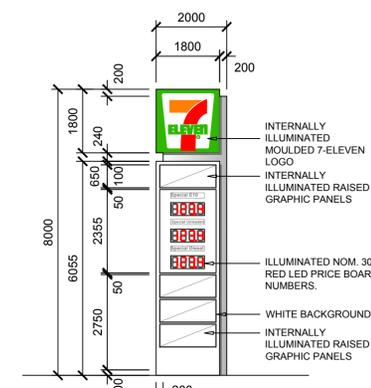
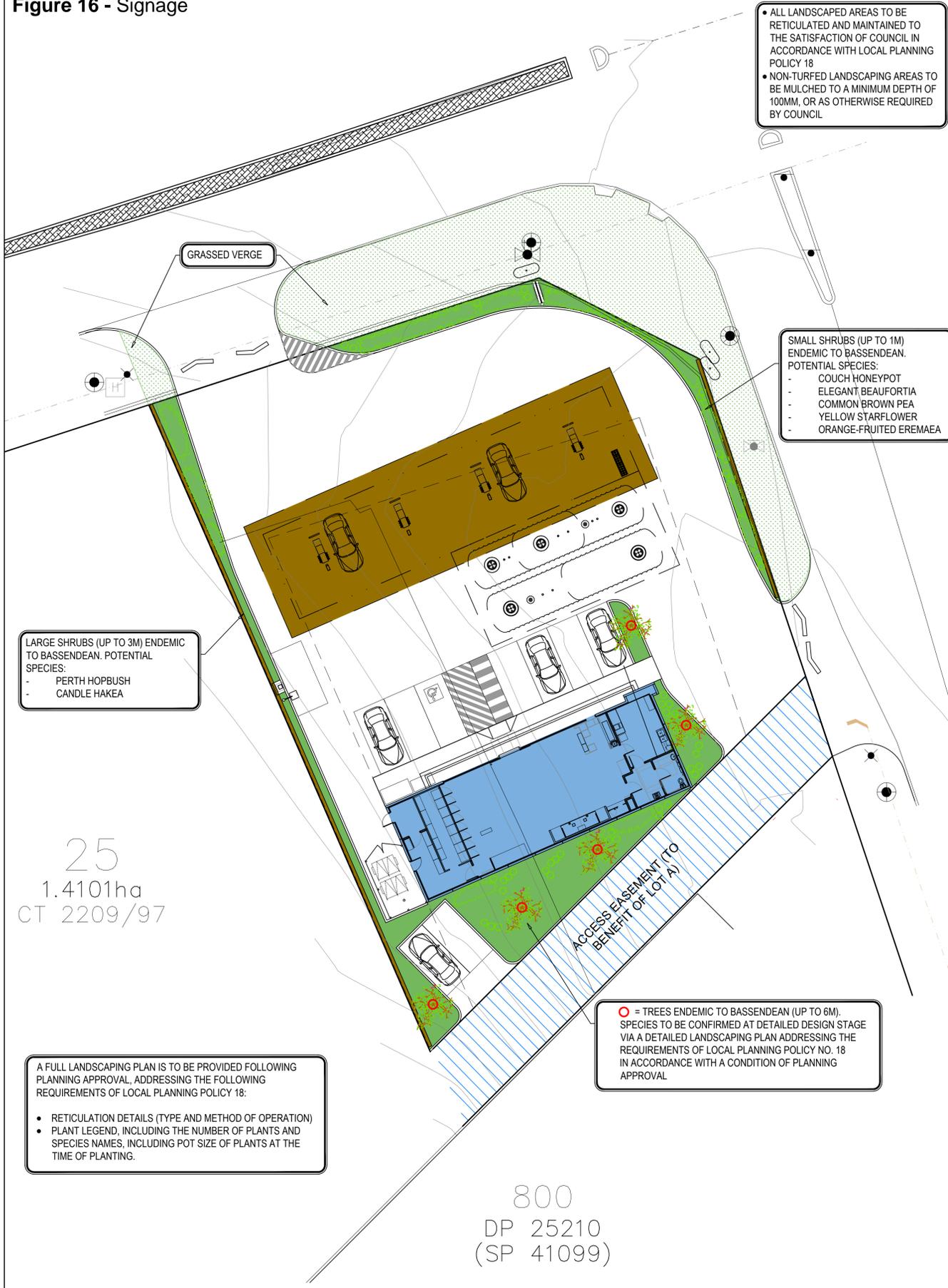
North	Project	Client
	PROPOSED 7ELEVEN CONVENIENCE STORE 300 COLLIER ROAD, BASSENDEAN WA	7 ELEVEN

Sheet	Drawn	Checked	Approved
ELEVATIONS			

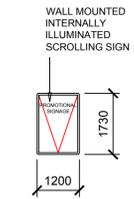
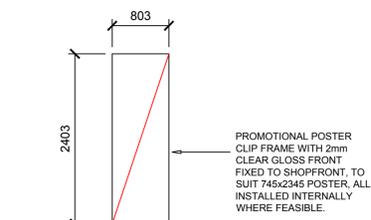
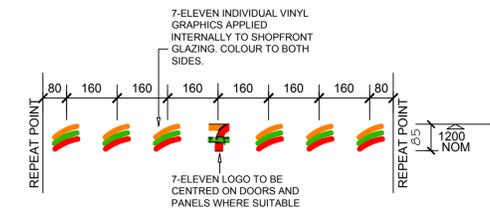
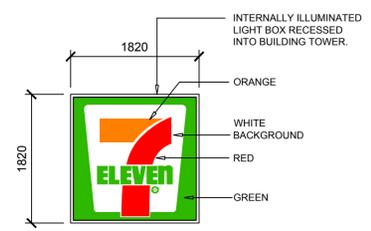
Drawn	Checked	Approved
PARA		
Date	DECEMBER 2016	
Scale	AS SHOWN	
Surv/Stat		
Approved		

33 Woodley Rd Glen Iris WA 6111 11 439 89794
FHM CONSULTING
 1616-TP3

Figure 16 - Signage



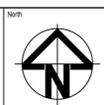
COLOUR LEGEND			
COLOUR	PANTONE	NON-ILLUM.	ILLUM.
ORANGE	PMS 165	7725-14	3630-13
GREEN	PMS 341	7725-56	3630-26
RED	PMS 485	7725-13	3630-43



LANDSCAPE PLAN
 SCALE 1:200

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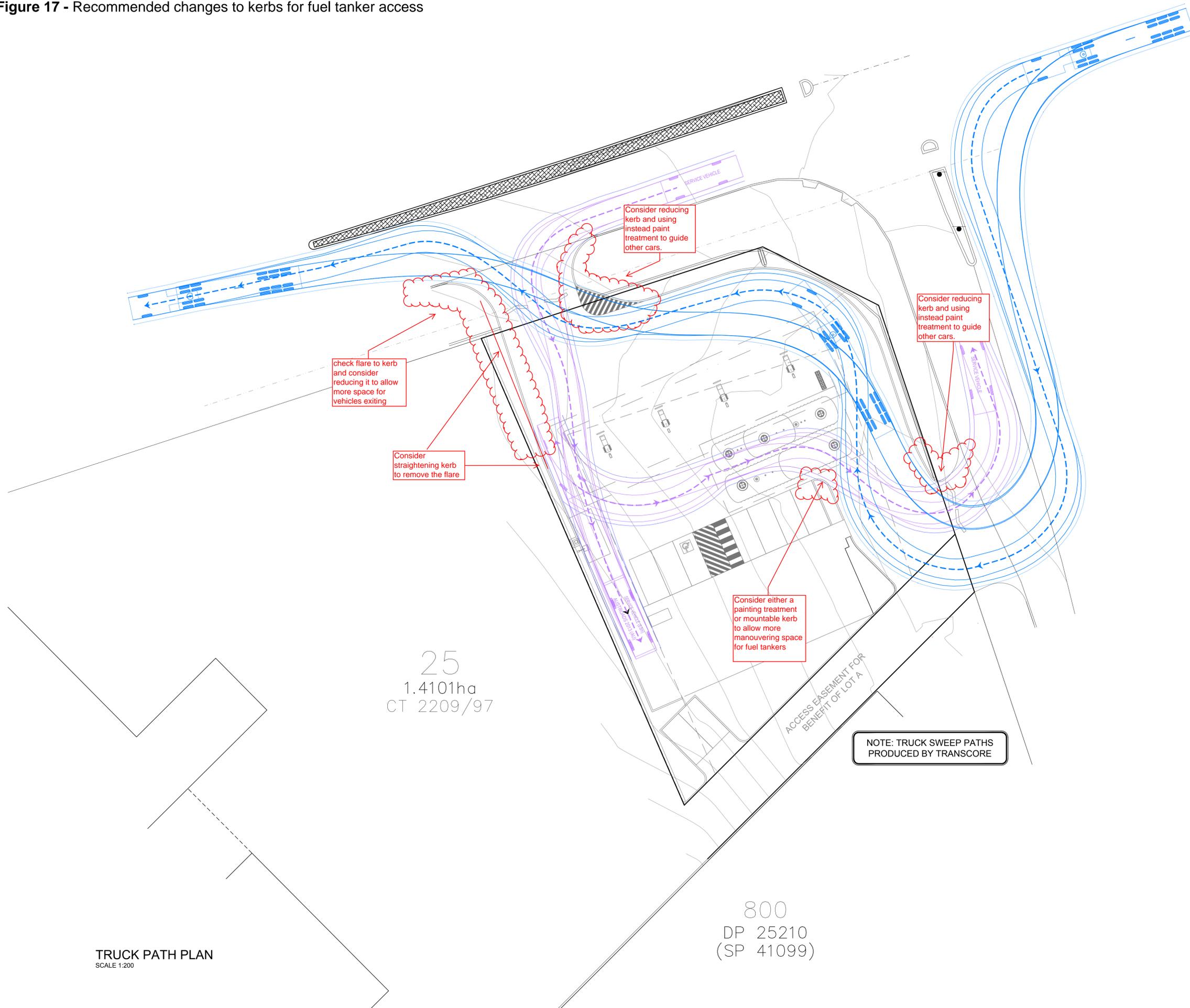
Rev No.	Description	Date	By
C	SITE AREA EASEMENT	13.02.16	RFH
B	VERGE GRASS PLANTING GUIDANCE	03.02.16	RFH
A	PLANTING GUIDANCE	19.12.16	RFH
	DA ISSUE	13.12.16	RFH



Project: **PROPOSED 7ELEVEN CONVENIENCE STORE**
 300 COLLIER ROAD, BASSENDEAN WA

Client: **7 ELEVEN**

Figure 17 - Recommended changes to kerbs for fuel tanker access



TRUCK PATH PLAN
SCALE 1:200

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Rev No.	Description	Date	By
C	LOT AREA - EASEMENT	13.02.17	RFH
B	COLLIER RD MEDIAN	03.02.17	RFH
A	DA ISSUE	13.12.16	RFH



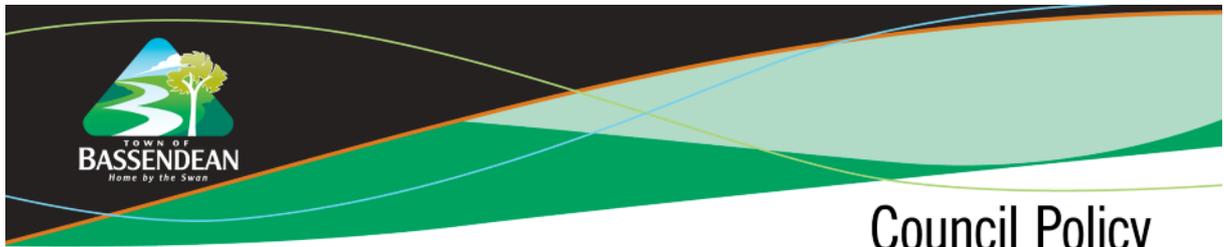
Project
**PROPOSED 7ELEVEN CONVENIENCE STORE
300 COLLIER ROAD, BASSENDEAN WA**

Client
7 ELEVEN

Sheet
TRUCK PATH

Clk	RFH
Dn	PARA
Date	DECEMBER 2016
Scale@A1	AS SHOWN
Surv/Stan	33 Woodley Rd Glen Iris WA 6111 439 89794
Approved	1616-TP5





Council Policy

LOCAL PLANNING SCHEME NO. 10

LOCAL PLANNING POLICY NO. 6 - INDUSTRIAL ZONES DEVELOPMENT DESIGN GUIDELINES

OBJECTIVE

To ensure a high standard of industrial development, in terms of appearance, landscaping, provision of parking and access.

APPLICATION

This policy applies to all the land zoned 'Light Industrial' and 'General Industrial' under Local Planning Scheme No. 10.

POLICY

All development within the 'Light Industrial' and 'General Industrial' zones shall comply with the following requirements:

Building Setbacks

The minimum setback requirements for all buildings within these zones shall be in accordance with those prescribed as follows:

Front	13-00m
Rear	Nil
Side	4.50m one side
Secondary Street	6.00m

Plot Ratio Limit and Definition

The maximum plot ratio for a site is 0.75.

Plot ratio is defined as the ratio of the gross total of the areas of all the floor to the land within the site boundaries. In calculating the gross total of the areas of all floors the areas shall be measured over any walls provided that lift shafts, stairs, toilets and amenities, external wall thicknesses, plant rooms and the gross floor area of any floor space used for the parking of wheeled vehicles, including access to and from that space within the building, shall not be included.

Site Cover

The maximum site coverage permitted to any development excluding loading bay awnings is 50%.

Vehicle parking

Car parking spaces shall be provided, constructed and maintained in accordance with the provisions of Part 5 of the Scheme, Local Planning Policy No. 8 (Parking Specifications) and the approved plan relating thereto.

Landscaping

All development within these zones shall be landscaped in accordance with the following requirements

- a) the minimum width of front boundary landscaping shall be 2 metres, except in the case of a corner lot, in which case the minimum shall be 1.0 metres on the secondary street as nominated by Council;
- b) the minimum width of side boundary landscaping (excluding side street boundaries) shall be 1 metre, to be provided from the front boundary to the setback line;
- c) landscaping is to be provided in accordance with Councils landscaping policy as amended from time to time, and shall be maintained by the owner of the lot thereafter.
- d) Shade trees in car parking areas shall be provided at a ratio of 1 per six car parking bays recommended in the Councils' landscaping policy.

Security Fencing

Where a security fence is proposed on a street lot boundary, it shall be of an open style either mesh fencing or palisade style fencing and setback 2-metres behind the street alignment.

Solid screen fences on industrial sites are only acceptable where located at or behind the 13-metre building line to the front setback area. In the case of corner lots solid screen fencing must be setback 6-metres from the secondary street.

Service Access

Provision shall be made for service access in the following manner:

- a) a paved accessway shall be provided for vehicles from the street to the rear of and to any other part of the building where provision is made in the external walls of the building for the entry of or the loading or unloading of vehicles;

- b) the access way shall be so constructed that all vehicles using it can enter from and return to a street in forward gear without reversing any part of the vehicle onto a street; and
- c) except as hereinafter mentioned, the access way shall not be less than 4.5 metres in width, unless the size of the lot makes the provision of such an access way impracticable or unreasonable the Council may permit an access way of a narrower width but in no case less than 3.0 metres in width.
- d) the provision of on-site delivery manoeuvring areas where large single axle commercial vehicles can enter and leave the site in forward gear. The manoeuvring area shall be exclusive of car parking bays and the interior of the building.

Storage and Refuse areas

The Council may require the provision of one or more areas for the storage of refuse in a development. This area shall be:

- a) screened from view from any public street, and enclosed by a wall of masonry or other approved building material, and being of not less than 1.8 metres in height; and
- b) accessible to service vehicles.

Irrespective of whether a storage area is required for a development, no land shall be used for open storage purposes unless it is screened from view of any public street by a fence or wall built to the specifications and satisfaction of the Council.

Building Materials

The facades of all buildings facing public streets shall be constructed of brick, glass, stone, masonry or concrete in respect of the ground floor level. The second floor level, or its equivalent, may be constructed of other material in accordance with the Building Code of Australia.

The use of zincalume sheeting for construction of Industrial buildings is prohibited, unless such buildings or parts of buildings are not visible from any public street, or Council approval is granted to vary this requirement.

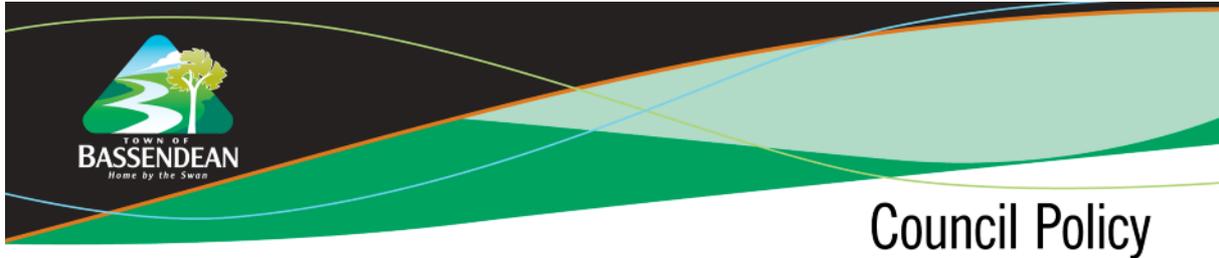
Floor Area

A factory or factory unit shall have a floor area of not less than 100m² and each the length and width shall not be less than 6 metres clear between the internal wall surfaces.

Factory Units

Multiple factory units on one lot shall not be permitted unless the following requirements are complied with:

- a) each factory unit shall have a visually screened service yard of not less than 16m² with a minimum depth of 2m adjoining and having connecting access to each unit; and
- b) the internal partition walls between factory units shall be constructed of brick, stone or concrete or other material approved by Council.



Local Planning Policy No 15

Percent for Art Policy

1.0 Preliminary

1.1 Citation

This Policy is adopted by the Town of Bassendean as a Planning Policy pursuant to Section 2.4 of Local Planning Scheme No .10.

1.2 Purpose

The Town of Bassendean considers there is a need to protect and enhance the utility, amenity and identity of the public domain of places such as centres, main streets, squares and parks within its municipality.

The purpose of this Policy is to assist in achieving the following objectives:

- a) improving legibility by introducing public art which assists in making streets, open spaces and buildings more identifiable,
- b) enhancing a sense of place by encouraging public art forms which provide an interpretation and expression of the local area's natural physical characteristics and social values,
- c) improving interpretation of cultural, environmental and built heritage,
- d) improving visual amenity by use of public art to screen unattractive views and improve the appearance of places, and
- e) improving the functionality of the public domain through the use of public art to provide appropriate street furniture functions

1.3 Guidelines

Interpretation and implementation of this Policy shall be in accordance with the guidelines for Percent for Art Policy which is provided in Appendix A to this document)

2.0 Application

2.1 Public Art to be Required

The Town of Bassendean shall require eligible proposals to provide public art in accordance with the described method for determining Public Art contributions described hereunder.

2.2 Proposals Eligible for Public Art Contributions

2.2.1 Projects Eligible

All development proposals for multiple dwellings, mixed use, commercial, civic, institutional, educational projects or public works with a value greater than \$1,000,000* shall be regarded as eligible proposals under this Policy.

* Value as used for determining Building Licence fees

2.2.2 Area of Application

This Policy applies throughout the Town.

The Policy should be read in conjunction with Planning Policy No 1 – Bassendean Town Centre Area Strategy and Guidelines which requires a public realm contribution of 2% of building construction costs for all development in the Town Centre which includes provision for Public Art.

2.2.3 Proponents

This Policy shall apply to all proponents, with the exception of those exempt from obtaining Local Authority planning approval under other legislation. Those proponents/projects so exempted should utilise this Policy and associated Guidelines as a guide for the implementation of their respective Percent for Art Policy obligations where applicable.

3.0 Method of Determining Public Art Contribution

3.1.1 Method of determining Public Art Contribution

The cost of any Public Art provided under this Policy shall be no less than one percent of the value of the eligible proposal.

3.1.2 Form of Public Art Contribution

Public Art required pursuant to this policy shall be provided in kind. Where requested by the proponent, the Council may alternatively accept a cash-in-lieu payment in accordance with the Town of Bassendean guidelines for Percent for Art Policy.

3.1.3 Location of Public Art Contribution

Public Art provided in-kind pursuant to this Policy shall be provided on site, or on crown land immediately adjacent to the site.

3.1.4 Separate Approval Generally Not Required for Public Art

Public Art provided under this Policy, in fulfillment of a condition of Planning Approval, shall not require a further Development Application.

TOWN OF BASSENDEAN GUIDELINES FOR PERCENT FOR ART POLICY

1.0 Operation and Intent

These Guidelines are adopted by the Town of Bassendean for the purpose of direction for the interpretation and implementation of the Town's Percent for Art Policy.

2.0 Implementation of Universal Percent for Art

2.1 Prescribed Areas

The Town of Bassendean has prepared a Public Art Master Plan which divides the Town into precincts, and shows the location of proposed public art works.

2.2 Cash In Lieu

Where the proponent elects, the public art contribution may alternatively be cash-in-lieu based on the rate described in the Town of Bassendean's Universal Percent for Art Policy. Such cash-in-lieu are to be:

- a) paid to the Town of Bassendean's Public Arts Fund (Percent for Public Art); and
- b) expended on a public art project within the prescribed area in the Public Art Master Plan within which proposal is situated.

Individual funds contributed within a prescribed area may be accrued for more comprehensive or detailed art projects and/or areas as outlined in the Town of Bassendean's Public Art Master Plan.

2.3 Eligible Costs

For the purpose of cash in lieu contributions, costs associated with the production of an art project may include:

- i) professional artist's budget, including artist fees, Request for Proposal, material, assistants' labour costs, insurance, permits, taxes, business and legal expenses, operating costs, and art consultant's fees if these are necessary and reasonable.
- ii) Fabrication and installation of artwork,
- iii) Site preparation,
- iv) Structures enabling the artist to display the artwork,
- v) Documentation of the artwork, and
- vi) Acknowledgment plaque identifying the artist, artwork and development.

2.4 Equity, Safety and Universal Access

Public art should be made accessible to all members of the community, irrespective of their age and abilities. While art in public spaces might be considered primarily a visual experience, it can provide a range of sensory experiences for people with disabilities - artwork can be tactile, aural and give off pleasant smells as well as being visual. Artwork need not be monumental, but can be at heights suitable for people in wheelchairs to touch, move through and explore. Artwork can be interactive play objects for family groups and children. Interpretive signage in an easy to read format, including Braille, will ensure that artworks are inclusive of all members of the community. Where feasible and appropriate to the site and community, the Authority will commission artworks that can be enjoyed as an interactive experience, irrespective of age, mobility or ability.

2.5 Exclusions to Public Art

Art projects ineligible for consideration include:

- i) Business logo.
- ii) Directional elements such as supergraphics, signage or colour coding.
- iii) 'Art objects' which are mass produced such as fountains, statuary or playground equipment.
- iv) most art reproductions.
- v) landscaping or generic hardscaping elements which would normally be associated with the project.
- vi) services or utilities necessary to operate or maintain artworks.

2.6 Design Documentation

The artist will be required to prepare detailed documentation of the artwork at various stages of the commission, design, fabrication and implementation processes. Depending upon the project, the documentation may include concept drawings, maquettes, structural and other engineering drawings, photographic images of works in progress, photographic images of completed and installed work and a maintenance schedule.

2.7 Approval of Artwork

The approval of the Council shall be required prior to the creation and installation of the Public Art. It is preferable that the Council delegate authority to grant approval to the Public Art to an appropriate Officer, or duly appointed panel.

2.8 Clearance Process

The public artwork must be completed and installed prior to the first occupation of the new development, and maintained thereafter by the owner(s)/occupier(s).

Alternatively, Council may accept a suitable agreement prepared at the applicant's expense binding the proponent to complete the works within a specified timeframe.

3.0 Maintenance

3.1 Maintenance and Resistance to Vandalism

Artworks that are low maintenance, robust, durable and resistant to Vandalism will be encouraged. Artists will be required to present the Town with a maintenance schedule at the completion of the commission.

3.2 Recording

The public artwork will be registered in the Town's Public Art Inventory once the artwork is completed.

3.3 Decommissioning

The proponent (or Town where the public art is situated on Crown Land) may decide to remove an artwork because it is in an advanced state of disrepair or damage, because the artwork is no longer considered suitable for the location or for other reasons. In such cases, the Town will prepare a documented archival record of the artwork prior to its removal.

The proponent (or Town where the public art is situated on Crown Land) must make a reasonable attempt to contact the artist at least 28 days ahead of any relocation, sale, alteration or removal of an artwork.

4.0 Creative Development Process

4.1 Creative Design Process

The proponent will commission artists and coordinate and manage the process by which they work alongside architects, landscape architects, planners and engineers. There will be a variety of approaches resulting in some easily identifiable artworks, and others that will be merged as an integral part of construction. While there is certainly a place for sculpture and civic landmark, there is also room for colour, movement, whimsy and theatre. This policy gives equal value to the purely aesthetic and to the functional.

4.2 Consultation with Stakeholders

Where appropriate, an invitation should be extended to community members to participate in the artwork process.

Some groups in the community are not comfortable with the expression of interest and tender processes, and will not enter into them without assistance. While artists from these groups will be encouraged to apply for all publicly advertised commissions, there may be opportunities for designating specific commissions for them. In such cases, the selection processes outlined above may be modified and more assistance given to the artists submitting Expressions of Interest or Requests for Proposals.

4.3 Collaboration

There is an expectation that commissioned artists will work in collaboration with other consultants engaged by the Proponent (most commonly, but not exclusively, landscape architects, urban planners and engineers) and that the conceptual and technical requirements of these professionals will be duly regarded by the artist when designing and installing the artwork.

There is an equal expectation that the artists' aesthetic judgement will be respected by other consultants engaged by the proponent. Changes to an artwork, even at concept stage, can only be made with the full knowledge and approval of the artist.

5.0 Artists Rights

5.1 Definition of Artist

Only professional artists will be eligible to carry out public art commissions. As the term 'artist' is self-referencing, for the purposes of this policy a professional visual artist can be defined as a person who fits into at least two of the following categories:

- A person who has a university degree or minimum 3 year full time TAFE Diploma in visual arts, or when the brief calls for it, other art forms such as multi media;
- A person who has a track record of exhibiting their artwork at reputable art galleries that sell the work of professional artists;
- A person who has had work purchased by major public collections, including (but not limited to) the Art Gallery of Western Australia, any of the university collections or Artbank;
- A person who earns more than 50% of their income from arts related activities, such as teaching, selling artwork or undertaking public art commissions.

Sometimes it will be appropriate to be more flexible and seek people other than professional artists to carry out artwork commissions. This may apply in instances when young, emerging and indigenous artists or students may be considered appropriate.

5.2 Artist Contract

The proponent will be required to forward copies of the artist's contract, maintenance schedule and artist contact details to the Town at the commencement of the project. In the case where the proponent is the Town, it shall satisfy itself that these requisites have been satisfied.

5.3 Moral Rights

Since 2000 moral rights legislation has protected artists. In brief, an artist's moral rights are infringed if:

- Their work is not attributed or credited;
- Their work is falsely attributed to someone else; or
- Their work is treated in a derogatory way by distorting, modifying or removing it without their knowledge or consent.

In practical terms this means that all artworks should have the artist's name on or attached it, that the Town cannot change an artwork in any way without seeking the artist's permission; likewise, cannot remove or re-locate the artwork without seeking the artist's permission. It may be that an artist has moved and the Town cannot find them, but evidence that a reasonable attempt to find the artist must be provided.

The Town will take special care to ensure that acts of restoration or preservation (of artworks) will be conducted in a sensitive manner with prior consultation with the artists. Wherever possible, preservation or restorative works should be carried out by professional conservators.

Special care will also be taken with the moral rights associated with works created by more than one artist, in that it is acknowledged that collaborators on artistic creations can take different views on issues such as relocation and restoration.

5.4 Acknowledgement of Artwork

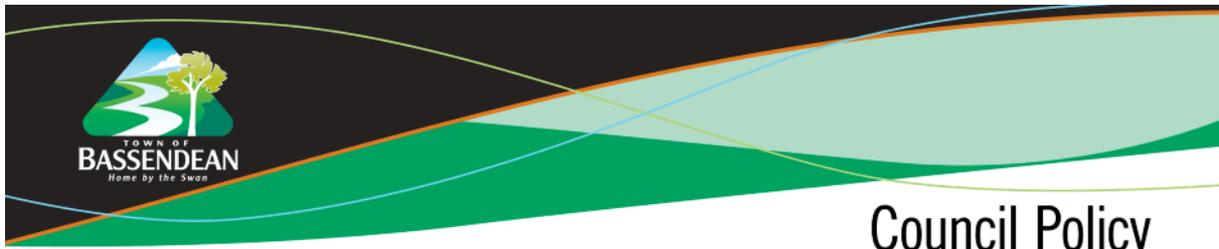
In line with moral rights legislation, the proponent will install a plaque or plate near each artwork, acknowledging the name of the artist, and the name of the person, agency or company who funded the artwork.

5.5 Copyright of Artwork

Once an artwork has been completed and accepted by the Town, copyright will be held jointly by the Town and the artist. In practical terms this means that the Town has the right to reproduce extracts from the design documentation and photographic images of the artwork for non-commercial purposes, such as annual reports, information brochures about the Authority and information brochures about the artwork. The artist will have the right to reproduce extracts from the design documentation or photographic images of the artwork in books or other publications associated with the artist or artwork.

5.6 Fees to Artists

A fee may be paid to artists invited to submit a Request for Proposal (RFP) and this may be credited to the value of the Public Art required under the Policy. The amount will be at the discretion of the proponent and in proportion to the overall artwork budget. The fee will be paid after the proposal had been submitted, deemed to comply with the requirements and the artist has attended their interview.



LOCAL PLANNING POLICY NO. 16 CONTROL OF ADVERTISEMENTS UNDER THE LOCAL PLANNING SCHEME NO. 10

1. PURPOSE OF POLICY

The principal purpose of this Policy is to provide assistance to the local government when determining an application for planning approval to erect, place or display an advertisement under the Town of Bassendean Local Planning Scheme no 10 (the "Scheme"). The Policy provides this assistance by setting out the standards which apply to different types of advertisement, the considerations which the local government should have regard to in determining an application for planning approval for an advertisement and the conditions which may be appropriately imposed on the approval of an application for planning approval for an advertisement.

This policy has been made in accordance with clause 2.2 of the Scheme. The Policy does not bind the local government in respect of any application for development approval but the local government is to have due regard to the provisions of the Policy and the objectives which the Policy is designed to achieve before making its determination.

2. CONSIDERATIONS IN DETERMINING APPLICATION FOR PLANNING APPROVAL

Considerations relevant to the determination of an application for development approval for an advertisement include:

- the category of advertisement;
- the standard specifications for the category of advertisement;
- the acceptable deviation from the standard specifications;
- the matters which the local government is directed to have regard to under the Scheme; and
- the manner of and the position in which the advertisement is to be displayed.

2.1 Categories of Advertisements

An advertisement is to be classified according to the following categories:

Animated sign - an advertisement which moves or is capable of moving, or contains moving parts or which changes its message, flashes on and off, chases, scintillates or has a moving, flashing or scintillating border or emblem, and includes a light display for the advertisement.

Development sign - an advertisement displayed on a lot which has been approved by the Western Australian Planning Commission for subdivision, advertising the lots for sale but upon which no work has taken place at the time of the application for planning approval of the device.

Fence sign - means an advertisement affixed to a fence on land, which advertisement will be visible from outside that land.

Hoarding - a detached or detachable structure, other than a pylon sign, that is erected for the sole purpose of displaying an advertisement and includes a wall panel or an illuminated panel but does not include a hoarding within the meaning of section 377 of the *Local Government (Miscellaneous Provisions) Act 1960*.

Horizontal sign - an advertisement attached to a building with its largest dimension horizontal.

Illuminated panel - a posted or painted advertisement externally illuminated by an artificial source of light.

Illuminated sign - an advertisement that is so arranged as to be capable of being lighted either from within or without the advertisement by artificial light provided, or mainly provided, for that purpose.

Institutional sign - an advertisement displayed on any land or building used as a surgery, clinic, hospital, rest home, home for the aged, or other place of a similar nature.

Other advertisement - an advertisement which is not described by any other category of advertisement referred to in clause 2.1 of this Policy.

Portable sign - means a portable free standing sign that only advertises a product or service available on the land on which it is erected, and includes a sandwich board sign consisting of two sign boards attached to each other at the top by hinges or other means, with a sign face not exceeding 1m² on each side.

Projection sign - an advertisement that is made by the projection of artificial light on a structure.

Property transaction sign - an advertisement indicating that the premises on which it is displayed are for sale or for lease or are to be auctioned.

Pylon sign - an advertisement supported by one or more piers and which is not attached to a building, and includes a detached sign framework supported on one or more piers to which sign infills may be added.

Roof sign - an advertisement displayed on the roof of a building.

Rural producer's sign - an advertisement displayed on land used for horticultural purposes and which advertises products produced or manufactured upon the land and includes the property owner's or occupier's name.

Semaphore sign - an advertisement supported at one of its ends only.

Sign infill - a panel which can be fitted into a pylon sign framework.

Tower sign - an advertisement displayed on a mast, tower, chimney stack or similar structure.

Verandah sign - an advertisement above, on or under a verandah, cantilever awning, cantilever verandah and balcony whether over a public thoroughfare or private land.

Vertical sign - an advertisement attached to a building with its largest dimension vertical.

Wall panel - a panel used for displaying a posted or painted advertisement.

Wall sign - an advertisement which is a sign painted on or directly affixed to the fabric of a wall.

2.2 Standard specifications

In addition to the specifications contained in Table 1, the specifications and requirements for each category of advertisement referred to in clause 2.1 follow.

2.2.1 Development signs

A development sign is to be removed from the site within 2 years of the grant of planning approval for the sign or when all of the lots, by number, in the subdivision being advertised have been sold, whichever is the sooner.

2.2.2 Hoardings

Hoardings should not –

- (a) be displayed in a Residential Zone be displayed any closer than its own height to a thoroughfare or a public place; and
- (b) have any part of it closer than 15 metres to any other advertisement displayed on the same lot.

2.2.3 Projection sign

An application for approval for a projection sign should not be approved if, upon the sign being projected onto a structure, it exceeds the specifications stated in columns 2, 3 and 4 of Table 1.

2.2.4 Property transaction sign

- (1) A property transaction sign advertising an auction shall, if approved -
 - (a) not be erected more than 28 days before the proposed date of the auction;
 - (b) be removed no later than 14 days after the auction, subject property has been sold, or at the direction of the local government whichever is the sooner; and

- (c) where such a sign is erected on land having a frontage to a road that is a main road within the meaning of the *Main Roads Act 1982*, consist of letters not less than 150 mm in height.
- (2) A property transaction sign advertising that flats and dwelling units in a building erected, or to be erected, are, or will be available for letting or for purchase shall, if approved -
 - (a) not be erected before the issue of a building licence for any such building; and
 - (b) not be erected or maintained for a period exceeding three months following completion of any such building, without the prior approval of the local government.
- (3) Any property transaction sign of any description shall be erected on the land to which it relates and not elsewhere.

2.2.5 Roof sign

A roof sign should not extend laterally beyond the external wall of the structure or building on which it is erected or displayed.

2.2.6 Semaphore sign

- (1) A semaphore sign should be fixed -
 - (a) at right angles to the wall or structure to which it is to be attached; and
 - (b) over or adjacent to the entrance to a building.
- (2) No more than one semaphore sign should be fixed over or adjacent to any one entrance to a building.

2.2.7 Verandah sign - verandah facia

A verandah sign fixed to the outer or facia of a verandah shall not project beyond the outer frame or surround of the facia.

2.2.8 Verandah sign - under verandah

A verandah sign under a verandah should be fixed at right-angles to the front wall of the building to which it is to be affixed except on a corner of a building at a thoroughfare intersection where the sign may be placed at an angle with the wall so as to be visible from both thoroughfares.

2.2.9 Vertical sign

A vertical sign where placed on a corner of a building at a thoroughfare intersection, may be placed at an angle with the wall so as to be visible from both thoroughfares.

2.2.10 Wall panel

A wall panel should comprise a framework surround with a lockable transparent cover behind which separate notices may be pinned affixed or painted.

2.3 Acceptable deviation

The local government may exercise its discretion to approve a deviation from the specific standards subject to the applicant demonstrating that the likely affect of the location, height, bulk, scale, orientation and appearance of the advertisement will not:

- (a) conflict with or detrimentally affect the amenity of the locality;
- (b) interfere with traffic safety; and
- (c) create visual pollution.

2.4 Proposed placement of advertisement

An advertisement should not be displayed where -

- (a) it would detract from the aesthetic environment of a park or other land used by the public for recreation;
- (b) in the case of an internally illuminated advertisement, its display would cause glare or dazzle or would otherwise distract the driver of any vehicle;

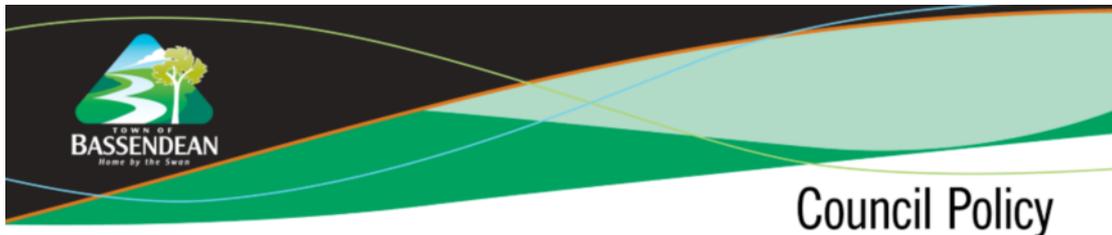
- (c) in the case of an externally illuminated advertisement, the light would not be directed solely onto the device and its structural surround and the light source be so shielded that glare would not occur or extend beyond the advertisement and cause the driver of any vehicle to be distracted;
- (d) it would be likely to interfere with, or cause risk or danger to traffic on a thoroughfare by virtue of the fact that it
 - (i) may be mistaken or confused with, or obstruct or reduce the effectiveness of any traffic control device;
 - (ii) would invite traffic to turn and would be sited so close to the turning point that there would not be reasonable time for a driver of a vehicle to signal and turn safely;
 - (iii) would invite traffic to move contrary to any traffic control device;
 - (iv) would invite traffic to turn where there is fast moving traffic and no turning lane;
 - (v) may obscure the vision of a person driving a vehicle.
- (e) in the case of an illuminated advertisement, it may be confused with or mistaken for the stop or tail light of a vehicle or vehicles;
- (f) it significantly obstructs or obscures the view of a river, the sea or any other natural feature of beauty; or
- (g) it would detrimentally affect the amenity of the area.
- (h) It would detrimentally affect the significance and aesthetics of a Heritage Area or a place on the Heritage List.

TABLE 1 - STANDARD SPECIFICATIONS

Advertisement	Max height of device M	Max length/width of device m	Max area.m ²	Min Headroom m	Max height above ground m	Projection max mm	Setbacks front m	Setbacks side M	Other Setbacks M	Minimum distance between ads	Location
Animated signs	1.5	1	3	2.4	6.0		2	1	6		
Development sign	4	5	20	1.0	5.0		15	10	15-		Development sites
Fence sign	1	20					2 x height	1			On fence
Hoarding			36	1.0			15				Non residential sites
Horizontal sign Height above thoroughfare				2.4		0.6		1			Fixed on wall Not to be fixed within 0.6m of end of wall
Less than 7.6	0.6										
7.6 to 9.0	0.7										
9.1 to 12	0.9										
More than 12(if there is no roof sign on the building)	4.5										
Illuminated sign	5	3	5	2.4	6.0	900	2	1	36		-
Institutional sign			3								
Other advertisement	6	8	30	-	6.0		6	3	6		As approved
Portable sign	1										A business may erect not more than one portable sign that does not project into the thoroughfare and is displayed during normal hours of business.
Projection sign	12	12	90	-	12.0						
Property transaction sign								-	-		-Transaction site
(a) Dwellings	3	4	5								
(b) multiple dwellings, shops, etc	4	5	20		5.0						
(c) large properties	6	8	30		6.0						
Pylon sign	6	2	4	2.4	6	900	1	2	6		Min 6m clearance of another sign

Advertisement	Max height of device M	Max length/width of device m	Max area.m ²	Min Headroom m	Max height above ground m	Projection max mm	Setbacks front m	Setbacks side M	Other Setbacks M	Minimum distance between ads	Location
Roof sign Height of main building above ground level 3.7 to 4.5 4.5 to 6.0 6.0 to 12 12 to 18 18m+	0.9 1.2 2.0 3.0 4.0			-					-		Not to extend beyond walls of buildings Roof signs will be subject to a structural engineers certificate Maximum height of roof sign 30m
Semaphore sign	0.6	0.9		2.4	3.6	900	-	1	-		-
Tower sign	20% of mast, tower or chimney	width of mast, tower, chimney, or structure where sign affixed		2.4							
Verandah sign -above facia -on facia -below facia	0.8 - 0.3	4 6 2.4	3 4	- - 2.7	5 - -	Nil Nil Nil	-	-	-		-
Vertical sign				0.3		600				3.6	Not to be fixed within 1.8m of end of wall, or project more than 1.0m above the wall to which it is affixed
Wall panel	1.5	1.5	3	-	3.0	100	-	-	-		-
Wall sign	3	8	12	1.0	5.0	100		-	-		-

Website: www.bassendean.gov.au Email: mail@bassendean.wa.gov.au Tel: (08) 9377 8000



Council Policy

LOCAL PLANNING POLICY NO. 18 LANDSCAPING WITH LOCAL PLANTS

Background

Landscaping is the term used to describe any vegetation and associated structures such as rockeries, ponds, sleepers and walls. It includes turf and grasses. Landscaping can enhance privacy, act as a natural cooling system for homes, soften the built form, create visual relief and generally improve the aesthetic appeal of new and existing developments. In addition to this, landscaping with local native vegetation can help to protect biodiversity and natural heritage values and contribute to a 'sense of place' for the area.

Landscaping can be a major component of urban renewal programs providing a boost for the local economy by stimulating business. Local plant themes can be incorporated into the landscaping of major roads, shopping centres, public transport routes, civic buildings and new developments.

The replacement of local native vegetation with exotic landscaping, coupled with the associated increase in water and fertiliser use, has an impact on water quality and the health of the Swan-Canning catchment.

1.1 Citation

This policy is adopted by the Town of Bassendean as a Planning Policy pursuant to Section 2.4 of Local Planning Scheme No. 10.

1.2 Purpose

The purpose of this policy is to assist the Town of Bassendean to promote the protection and enhancement of natural resources within the region by prescribing minimum standards for landscaping with local native plants.

1.3 Application of this policy

This policy applies to all applications that require planning consent under the Local Planning Scheme and require landscaping to be provided.

This policy has limited effect to applications based on Council's Energy Efficient Design Policy which encourages deciduous trees and plants to be provided in certain circumstances to aid summer shading.

1.4 Objectives

The objectives of the Town of Bassendean's 'Landscaping with Local Plants Policy' are to:

- provide development applicants with guidance as to the standard of landscaping expected by Council;
- build pride in the Town of Bassendean's natural environment and foster a 'sense of place' in the community through appropriate landscaping;
- Reduce threats to biodiversity by avoiding plant selection that may lead to future environmental weed problems;
- create visual stimulus and contrast between natural and built features;
- soften the impact of the built form;
- maintain and further promote the amenity and resultant quality of life provided for residents of the Town of Bassendean;
- promote better utilisation of water resources and the development of practices which conserve water; and
- minimise the extent of fertilisers leaching into drains and waterways, and in turn maintaining water quality within the Town.

1.5 Requirements

Landscape plans illustrating all landscaped areas must be prepared ideally by a professional landscape designer or qualified horticulturalist or landscape architect and submitted for Council's approval. Plans must focus on the use of local species and are to be prepared to a scale of not less than 1:200 and should show:

- street frontages, neighbouring buildings and fence lines;
- contours – both within the site and for the adjoining verge;
- reticulation details (type and method of operation);
- details of ground treatment for all common areas (for example; grass, paving, ground covers, mulch);
- plant legend, including the number of plants and species name including pot-size of plants at the time of planting; and
- accurate details of existing tree positions, with further detail for trees over 2m in height (species, trunk diameter, drip line and crown height).

Vegetation should be of sufficient size and spacing to meet the objectives of the policy within two years and landscaped areas must be developed in accordance with the approved plan and maintained thereafter for a period of twenty-four months

The following web site is one useful resource that shows local plants that are endemic to the area <http://www.apacewa.org.au/nursery>.

1.6 Relationship to LPS

This policy complements the Local Planning Scheme No. 10, the Residential Design Codes of Western Australia. This policy should be read in connection with:

1. Council's Street Tree Removal and Replacement Policy, Amenity Tree Evaluation Policy which controls trees within the verge area adjoining development sites;
2. Council's Verge treatment policy; and
3. Council's policy on the Retention of Trees on Development Sites.

Under the Local Planning Scheme No. 10, each application for planning approval is to be accompanied by:

1. The existing and proposed ground levels over the whole of the land the subject of the application and the location, height and type of all existing structures, and both the structures and vegetation proposed to be removed; and
2. The nature and extent of any open space and landscaping proposed for the site.

Under the Residential Design Codes each application for planning approval is required to be accompanied by an existing site analysis plan showing:

1. The position, type and size of any tree exceeding 3m in height; and
2. The street verge, including footpaths, street trees, crossovers, power poles and any service such as telephone, gas, water and sewerage in the verge.



Form 2 - Responsible Authority Report (Regulation 17)

Property Location:	Lot 54 (Nos. 72 – 74) Railway Parade, Bassendean
Development Description:	Proposed 22 Multiple Dwellings
Proposed Amendments:	To amend the approval so as to extend the period within which the development must be substantially commenced by a further two years. To delete conditions 1, 2, 3, 4, 15, 16, 17, 21, 24 and 26 from the approval.
DAP Name:	Metro Central JDAP
Applicant:	Mr Joe Douglas of Urban & Rural Perspectives
Owner:	Mark Francis Hammond & Sandra Lee Hammond
Value of Amendment:	Overall estimated cost of development remains unchanged at \$3.3 million dollars.
LG Reference:	2017-058
Responsible Authority:	Town of Bassendean
Authorising Officer:	Christian Buttle, Senior Planning Officer
Department of Planning File No:	DAP/15/00740
Report Date:	12 May 2017
Application Receipt Date:	4 April 2017
Application Process Days:	38 days
Attachment(s):	1. Original Determination Notice. 2. Revised drawings. 3. Applicant's supporting letter dated 31 March 2017.

Officer Recommendation:

That the Metro Central JDAP resolves to:

1. **Accept** that the DAP Application reference DAP/15/00740 as detailed on the DAP Form 2 date stamped received 4 April 2017 is appropriate for consideration in accordance with regulation 17 of the *Planning and Development (Development Assessment Panels) Regulations 2011*;
2. **Approve** the DAP Application reference DAP/15/00740 as detailed on the DAP Form 2 date stamped received 4 April 2017 and accompanying plans:

Dwg No.	Drawing Name	Rev No.	Dwg Date
A1.01	Proposed Site Development Plan	7	27.10.2015
A2.01	Proposed Site / Ground Floor Plan (Part A)	7	27.10.2015

A2.02	Proposed Site / Ground Floor Plan (Part B)	7	27.10.2015
A2.05	First Floor Plan (Part A)	7	27.10.2015
A2.06	First Floor Plan (Part B)	7	27.10.2015
A3.01	Elevations	7	27.10.2015
A3.02	Elevations	7	27.10.2015

in accordance with the provisions of the Town of Bassendean Local Planning Scheme No. 10, for the proposed minor amendment to the approved 22 Multiple Dwellings at Lot 54 (Nos. 72-74) Railway Parade, Bassendean, subject to the following conditions:

Amended Conditions

1. Deletion of Condition 1.
2. Deletion of Condition 2.
3. Deletion of Condition 3.
4. Deletion of Condition 4.
5. Modification to Condition 15 so as to now read:

A minimum of 8 bicycle parking spaces shall be provided for residents, and a minimum of 3 bicycle parking spaces shall be provided for visitors. The resident bicycle parking spaces shall be located in a secure weather protected compound, details of which shall be provided to the Town in advance of, or in conjunction with the application for a building permit, and **all spaces shall** be constructed in accordance with the provisions of AS 2890.3 (as amended).

6. Deletion of Condition 16.
7. Deletion of Condition 17.
8. Modification to Condition 26 so as to now read:

The bin storage areas are:

- (a) To be surrounded by a 1.8 metre high minimum wall with a self-closing gate;
- (b) To be provided with 75mm min thickness concrete floors grading to a 100mm industrial floor waste, connected to sewer, with a hose cock to enable both the bins and bin storage area to be washed out; and
- (c) To be provided with internal walls that are cement rendered (solid and impervious) to enable easy cleaning.

9. Modification to Condition 36 so as to now read:

This decision constitutes planning approval only and is valid for a period of **4 years from 23 June 2015**. If the subject development is not substantially

commenced within the 4 year period, the approval shall lapse and be of no further effect.

All other conditions and requirements detailed on the previous approval dated 23 June 2015 shall remain unless altered by this application.

Details: outline of development application

Insert Zoning	MRS:	Urban
	TPS:	Residential (R20/40)
Insert Use Class:		Multiple Dwellings ('P' (permitted) land use)
Insert Strategy Policy:		Not applicable
Insert Development Scheme:		Town of Bassendean Local Planning Scheme No. 10.
Insert Lot Size:		2561 sq.metres
Insert Existing Land Use:		N/A – Vacant Land

The applicant seeks approval to modify the development approval for 22 Multiple Dwellings which was originally issued by JDAP at its meeting held 23 June 2015 by:

- (a) Extending the period of validity within which the proposed development must be substantially commenced; and
- (b) Deleting a range of conditions to which the approval is currently subject.

Background:

Development approval for 22 Multiple Dwellings was granted by the Metro Central JDAP at its meeting held 23 June 2015.

Legislation & policy:

Legislation

- Planning and Development Act 2005;
- Planning Development (Local Planning Schemes) Regulations 2015; and
- Town of Bassendean Local Planning Scheme No. 10.

State Government Policies

The following state government policies are of relevance when considering the application for development approval:

- (a) Directions 2031 (Bassendean is an identified District Centre);
- (b) State Planning Policy 3.1 – Residential Design Codes of Western Australia; and
- (c) State Planning Policy 4.2 – Activity Centres for Perth and Peel.

Local Policies

The application for development approval was originally assessed against the Town's adopted Local Planning Policy No. 2 "Energy Efficient Design" and Local Planning Policy No. 3 Water Sensitive Design". These policies are referenced by Clause 5.3 of Local Planning Scheme No. 10 and were required to be suitably addressed in

order for the applicant to gain development entitlements at the higher R40 density code, as proposed.

Additionally, Local Planning Policy No. 18 – Landscaping with Local Plants is of relevance when considering the application for development approval.

Consultation:

Public Consultation

No public consultation was undertaken in conjunction with the assessment of this application for development approval.

The original application was referred to the following adjoining / nearby property owners for information purposes only and not for formal public comment:

- Rear (opposite side of right-of-way to north of development site) - No. 6 Ivanhoe Street, Bassendean (Bassendean Church of Christ);
- Left hand (western side) – Nos. 2 & 2B Broadway;
- Right hand (eastern side) – No. 70 Railway Parade.

The relationship of the development site to the other properties described above can be seen on the 2014 aerial photography extract which is provided below (development site is highlighted red):



The development site is also adjoined to the left hand (western) side by a BWS bottle shop which is held in the same ownership as the site which is the subject of current consideration.

Consultation with other Agencies or Consultants

Nil.

Planning assessment:

Deletion of Condition No.1

Condition 1 requires:

The design/extent of roof cover to balconies of units 12-19 and 22 being modified in order to facilitate the provision of direct solar access to the Living Room windows of the respective units, to the satisfaction of the Town, unless an alternative arrangement, such as the provision of solar hot water systems along with low water use landscaping for the development, can be provided to the satisfaction of the Town.

The drawings provided in support of the revised application for development approval satisfactorily address this matter by:

Setting the extent of roof cover for balconies of units 12-19 and 22 at 2.4 metres. This corresponds with the accepted extent of roof cover associated with units 1-11.

As such, this condition can be deleted as an acceptable (and matching) level of solar access to living room windows of units 12-19 and 22 will be achieved as is provided (and approved) for units 1-11.

Deletion of Condition No. 2

Condition 2 requires:

Upper floor unit 19 being set back from the left hand (western) side boundary generally in accordance with the Deemed-to-comply provisions of the R-Codes or a section of the wall being setback to achieve a light-well between the two bathrooms and this side of the building being detailed architecturally in a manner which is generally consistent with that of other dwellings within the development, while also allowing for casual surveillance in the manner described within the Officer report (non-major size fixed openings) to the satisfaction of the Town.

The drawings provided in support of the revised application for development approval satisfactorily address this matter by:

- Adding in a light well of 4.285 metres in length by 1.5 metres in width between bedrooms 1 and 2 (and alongside the bathrooms);
- Detailing this area in a manner which is consistent with the remainder of the development; and
- Incorporating additional windows into each of the bedrooms which provide for casual surveillance of the adjoining bottle shop car park.

As such, this condition can be deleted as a revised design has been provided which satisfactorily addresses the condition imposed on the original approval.

Deletion of Condition No. 3

Condition 3 requires:

“Outdoor living areas / Balconies for units 5, 19 and 20 being modified in order that a usable area of 10 sq. metres minimum with width and / or length

dimensions of 2.4 metres minimum being provided as measured in any direction.”

The drawings provided in support of the revised application incorporate suitably sized and dimensioned outdoor living areas / balconies of 10 sq.metre minimum with widths and / or lengths for units 5 and 19, but not for unit 20.

Unit 20 incorporates a main usable balcony area (measured from back wall of dwelling to inside face of balcony) of 3.8 metres x 2.45 metres (9.31 sq.metres). It incorporates two additional areas of 2.1m in length by 0.25m in width (area between back wall of dwelling and inside face of brick pier to each side of balcony) which is an additional 1.05 sq.metres, resulting in an overall area of 10.36 sq.metres.

While the brick piers in each corner of the balcony result in the balcony not meeting the Deemed-to-comply provisions the R-Codes (in terms of the entirety of the balcony having 2.4m minimum dimensions), the overall balcony area is slightly more (10.36 sq.m) than the 10 sq.m minimum required.

The brick piers in the far corners of the balcony, while resulting in small portions of the balcony having dimensions of less than 2.4m, do not impact upon the balcony to an extent that it is not “capable of use in conjunction with a habitable room the dwelling....” and therefore it is considered to be acceptable for approval.

Having regard to the comments provided above, this condition can be deleted.

Deletion of Condition No. 4

Condition 4 requires:

“The provision of a pedestrian path which provides wheelchair accessibility connecting the main pedestrian entrance to building ‘block 4’ with the public footpath.”

The revised drawings now incorporate a pedestrian path of 1.2 metres in width which connects the main pedestrian entrance to building block 4 with the public footpath. This footpath ‘wraps around’ visitor parking bays 1 and 2.

Having regard to the provision of this pedestrian path, condition 4 can now be deleted.

Deletion of Condition 15

Condition 15 requires:

“A minimum of 8 bicycle parking spaces shall be provided for residents, and a minimum of 3 bicycle parking spaces shall be provided for visitors. The resident bicycle parking spaces shall be located in a secure weather protected compound, details of which shall be provided to the Town in advance of, or in conjunction with the application for a building permit, and be constructed in accordance with the provisions of AS 2890.3 (as amended), while visitor bicycle parking spaces shall be relocated close to main pedestrian access points to the development to the satisfaction of the Town as advocated within AS2890.3.”

The revised drawings do show reference to a bike enclosure for residents, however detailed drawings have not been provided, and there is no indication of any weather protection having been provided to this structure (as required).

The revised drawings now also make reference to a '3 bike' parking rack close to the main entrance of the development, for visitors use.

Having regard to the comments referred to above, it is appropriate that this condition be retained in a modified format as identified below (with wording to be removed 'struck through' and new words to be added shown in bold):

*"A minimum of 8 bicycle parking spaces shall be provided for residents, and a minimum of 3 bicycle parking spaces shall be provided for visitors. The resident bicycle parking spaces shall be located in a secure weather protected compound, details of which shall be provided to the Town in advance of, or in conjunction with the application for a building permit, and **all spaces shall be constructed in accordance with the provisions of AS 2890.3 (as amended).**" ~~while visitor bicycle parking spaces shall be relocated close to main pedestrian access points to the development to the satisfaction of the Town as advocated within AS2890.3."~~*

Deletion of Condition 16

Condition 16 requires:

"The width of visitor car parking bay No. 3 shall be increased to 2.4m minimum."

The revised drawings increase the width of visitor car parking bay No. 3 from 2.335m to 2.4m and on this basis, condition 16 can be deleted.

Deletion of Condition 17

Condition 17 requires:

"The height of filling and associated retaining adjacent to the left hand (western) side property boundary being reduced to an extent that it does not exceed 500mm above existing ground levels."

The drawings which were originally approved incorporated a finished floor level (FFL) of RL 11.000 for unit 8 along the western boundary with the adjoining BWS bottle shop site. The proposed FFL of unit 8 has now been reduced by 343mm to 10.657, resulting in filling and retaining which does not exceed 500mm in height.

Having regard to the comments identified above, this condition can now be deleted.

Deletion of Condition 21

Condition 21 requires:

"Any fencing which is situated between a building and the Railway Parade or right-of-way frontages of the development site demonstrating compliance with the following requirements:

- (a) *The overall height of fencing not exceeding 1.8 metres above natural ground levels as viewed from outside of the development site; and*
- (b) *Infill panels above base level solid components which are shown on the approved drawings being visually permeable.”*

The detail shown on the drawings appears to demonstrate compliance with the requirements contained within condition. The requirements contained within the condition hold good, however, and it is appropriate that this condition remain in place without change.

Deletion of Condition 24

Condition 24 requires:

“Each dwelling shall be provided with an effective clothes dryer.”

Although the drawings submitted in conjunction with this application reference the word ‘dryer’ within most of the units (and reference an outdoor ‘drying line’ for other units), use of the word ‘dryer’ is not sufficient to ensure that the requirement to provide an effective clothes dryer for each dwelling will be met. The condition should be retained, without change.

Deletion of Condition 26

Condition 26 requires:

“The bin storage areas are:

- (a) *To be increased in size equivalent to that which would be occupied by two additional bins to cater for bulky rubbish storage while awaiting collection to the satisfaction of the Town;*
- (b) *To be surrounded by a 1.8 metre high minimum wall with a self-closing gate;*
- (c) *To be provided with 75mm min thickness concrete floors grading to a 100mm industrial floor waste, connected to sewer, with a hose cock to enable both the bins and bin storage area to be washed out; and*
- (d) *To be provided with internal walls that are cement rendered (solid and impervious) to enable easy cleaning.”*

The revised drawings which are the subject of determination satisfactorily increase the size of the bin storage areas as required by part (a) of original condition 26, so this component of the condition can be deleted.

The remainder of the condition holds good, however, and should be retained in a modified condition which reads as follows:

The bin storage areas are:

- (a) To be surrounded by a 1.8 metre high minimum wall with a self-closing gate;
- (b) To be provided with 75mm min thickness concrete floors grading to a 100mm industrial floor waste, connected to sewer, with a hose cock to enable both the bins and bin storage area to be washed out; and
- (c) To be provided with internal walls that are cement rendered (solid and impervious) to enable easy cleaning.

Amend Condition 36

Condition 36 states that:

“This decision constitutes planning approval only and is valid for a period of 2 years from the date of approval. If the subject development is not substantially commenced within the 2 year period, the approval shall lapse and be of no further effect.”

The applicant has requested that the period of validity of approval be extended for a further two years.

There are no changes within the planning framework which would impact the ability for the requested extension, and no objection is held in relation to this request. It is recommended that condition 36 be modified as identified below (with wording being removed being 'struck through' and new words shown in bold):

This decision constitutes planning approval only and is valid for a period of ~~2~~ **4** years from ~~the date of approval~~ **23 June 2015**. If the subject development is not substantially commenced within the ~~2~~ **4** year period, the approval shall lapse and be of no further effect.

Council Recommendation:

This matter will be presented to Council in order that it can formulate its own recommendation in relation to the proposal.

At the time of writing this report, the matter had yet to be considered by Council. Details of Council's recommendation will be conveyed to the JDAP at the meeting at which this proposal is considered.

Conclusion:

For the reasons identified within this report it is recommended that:

- Conditions 1, 2, 3, 4, 16, and 17 be deleted;
- Conditions 15, 26 and 36 be modified as shown in the officer recommendation for the reasons identified within the body of this report; and
- Conditions 21 and 24 remain unmodified for the reasons identified within the body of this report.



LG Ref: DA 2015-030
DoP Ref: DAP/15/00740
Enquiries: Development Assessment Panels
Telephone: (08) 6551 9919

Mr Carlo Famiano
Urban and Rural Perspectives
PO Box 2507
Malaga WA 6944

Dear Mr Famiano

**Metro Central JDAP – Town of Bassendean – DAP Application DA 2015-030
Lot 54 (Nos. 72-74) Railway Parade, Bassendean
14 Multiple Dwellings**

Thank you for your application and plans submitted to the Town of Bassendean on 26 February 2015 for the above development at the above mentioned site.

This application was considered by the Metro Central Joint Development Assessment Panel at its meeting held on 23 June 2015, where in accordance with the provisions of the Town of Bassendean Local Planning Scheme No.10, it was resolved to approve the application as per the attached notice of determination.

Should the applicant not be satisfied by this decision, a DAP Form 2 application may be made to amend or cancel this planning approval in accordance with Regulation 17 of the Development Assessment Panel Regulations 2011.

Also be advised that there is a right of review by the State Administrative Tribunal in accordance with Part 14 of the *Planning and Development Act 2005*. An application must be made within 28 days of the determination in accordance with the *State Administrative Tribunal Act 2004*.

Should you have any enquiries in respect to the conditions of approval please contact Mr Christian Buttle at the Town of Bassendean on (08) 9377 8022.

Yours sincerely

Zoe Hendry

DAP Secretariat

1/07/2015

Encl. DAP Determination Notice
Approved plans

Cc: Mr Christian Buttle
Town of Bassendean





Planning and Development Act 2005

Town of Bassendean Local Planning Scheme No.10

Metro Central Joint Development Assessment Panel

Determination on Development Assessment Panel Application for Planning Approval

Location: Lot 54 (Nos. 72-74) Railway Parade, Bassendean
Description of proposed Development: 14 Multiple Dwellings

In accordance with Regulation 8 of the *Development Assessment Panels Regulations 2011*, the above application for planning approval was **granted** on 23 June 2015, subject to the following:

Approve DAP Application reference DAP Dap/15/00740 and accompanying plans:

Dwg No.	Drawing Name	Rev No.	Dwg Date
A0.00	22 Bins Verge Pick Up Plan	3	18.05.2015
A1.01	Proposed Site Development Plan (with aerial underlay)	2	11.05.2015
A1.02	Proposed Site Development Plan	2	11.05.2015
A2.01	Proposed Site / Ground Floor Plan (Part A)	2	11.05.2015
A2.02	Proposed Site / Ground Floor Plan (Part B)	2	11.05.2015
A2.05	First Floor Plan	2	11.05.2015
A3.01	Elevations	2	11.05.2015
A3.02	Elevations	2	11.05.2015

in accordance with Clause 10.3 of the Town of Bassendean Local Planning Scheme No. 10, subject to the following conditions:

1. The design/extent of roof cover to balconies of units 12-19 and 22 being modified in order to facilitate the provision of direct solar access to the Living Room windows of the respective units, to the satisfaction of the Town, unless an alternative arrangement, such as the provision of solar hot water systems along with low water use landscaping for the development, can be provided to the satisfaction of the Town (see footnote).
2. Upper floor unit 19 being set back from the left hand (western) side boundary generally in accordance with the Deemed-to-comply provisions of the R-Codes **or a section of the wall being setback to achieve a light-well between the two bathrooms** and this side of the building being detailed architecturally in a manner which is generally consistent with that of other dwellings within the development, while also allowing for casual surveillance in the manner described within the Officer report (non-major size fixed openings) to the satisfaction of the Town.
3. Outdoor living areas / Balconies for units 5, 19 and 22 being modified in order that a usable area of 10 sq. metres minimum with width and / or length dimensions of 2.4 metres minimum being provided as measured in any direction.



4. The provision of a pedestrian path which provides wheelchair accessibility connecting the main pedestrian entrance to building 'block 4' with the public footpath.
5. A detailed and professionally prepared landscape plan being submitted prior to or with the application for a Building Permit for the Town's approval which provides full detail of the scope of works to be undertaken in both the private and public realms adjoining the development site, including, but not limited to:
 - (a) the location, type and size of proposed trees, shrubs and ground cover to be planted; and
 - (b) reticulation methods, including arrangements incorporated into the design to minimize water use.

Landscaping design and species selection shall pay particular attention to provisions contained within the Town of Bassendean Local Planning Policy No. 18 – Landscaping with Local Plants, and shall not include the use of artificial turf.

6. The site shall be landscaped in accordance with the approved landscaping plan and shall be maintained thereafter.
7. Submission of a plan detailing the location of all external lighting, to the satisfaction of the Town prior to or in conjunction with the application for a building permit. The lighting plan shall take particular account of the need to for lighting to be provided to pedestrian paths, car parking areas, bicycle parking locations, and the right-of-way within the vicinity of the pedestrian and vehicular entrance to the development and subsequent lighting installed must demonstrate that any light spill to adjoining properties is minimised to acceptable levels. Lighting in accordance with the approved plan is to be installed prior to occupation or strata titling of the building(s), whichever occurs first.
8. The following works shall be completed within the Railway Parade road reserve to facilitate the proposed development:
 - (a) Existing 1m wide concrete apron associated with redundant crossover forward of No. 74 Railway Parade shall be removed and replaced with barrier kerb and brick paving to match the remainder of the footpath;
 - (b) Existing Paved crossover forward of No. 74 Railway Parade shall be removed and replaced with paving to match the remainder of the footpath (both in material and paving pattern). This includes the removal of the white header course of paving (which defines the alignment of the existing crossover) and replacement to match the remainder of the footpath;
 - (c) The proposed crossover to Railway Parade shall have a 1m concrete apron adjacent to the kerb line. The crossover shall be centrally positioned in line with the access aisle between the visitor car parking spaces and shall be a maximum 6 metre width;



- (d) The proposed crossover from Railway Parade shall be constructed of heavy duty trafficable brick pavers, the material and colour of which shall match the adjoining footpath. The crossover shall have a cream coloured header course which delineates the crossover from the adjoining footpath; and
 - (e) Prior to the issue of a Building Permit, the applicant shall pay the Town a sum of \$682 to cover the removal and streetscape contribution associated with the loss of existing vegetation within the Railway Parade road reserve to facilitate bin storage.
- 9. Prior to the issue of a Building Permit for this development, a 1.0m strip of land shall be excised from the rear of the lot for the purposes of widening the adjoining right-of-way, or the owner shall enter into a legal agreement with the Town prepared by the Town's Solicitors at the owner's cost requiring excision of this land to be completed within twelve months of the issue of a Building Permit, or prior to the completion of the development, whichever occurs earlier.
- 10. The strip of land to the rear of the site which is excised for right-of-way purposes shall be paved, drained and kerbed to the specifications of the Town prior to occupation of the dwellings.
- 11. The sealing and kerbing of all car parking areas and access ways to the Town's specifications.
- 12. The on-site car parking spaces and access ways being constructed and maintained thereafter to the Town's satisfaction.
- 13. Each dwelling being provided with one car parking space. Such arrangement shall be reflected on any subsequent strata plan for the property.
- 14. Visitor parking spaces being clearly marked for "Visitors Only" and used as such.
- 15. A minimum of 8 bicycle parking spaces shall be provided for residents, and a minimum of 3 bicycle parking spaces shall be provided for visitors. The resident bicycle parking spaces shall be located in a secure weather protected compound, details of which shall be provided to the Town in advance of, or in conjunction with the application for a building permit, and be constructed in accordance with the provisions of AS 2890.3 (as amended), while visitor bicycle parking spaces shall be relocated close to main pedestrian access points to the development to the satisfaction of the Town as advocated within AS2890.3.
- 16. The width of visitor car parking bay No. 3 shall be increased to 2.4m minimum.
- 17. The height of filling and associated retaining adjacent to the left hand (western) side property boundary being reduced to an extent that it does not exceed 500mm above existing ground levels.
- 18. All storm water being contained and disposed of on site. Details of the method of storm water containment and disposal being included with the drawings submitted for a Building Permit.



19. The street number being prominently displayed at the front of the development.
20. The provision of side and rear fences, behind the street setback line, of 1.8 metres in height, unless higher fencing is shown on the approved drawings. Where the ground levels vary on either side of the fence, the required height shall be measured above the higher ground level. Fencing along the common boundary with the adjoining commercial premises at Lot 51 (No. 76) Railway Parade shall be constructed of brick unless otherwise approved by the Town.
21. Any fencing which is situated between a building and the Railway Parade or right-of-way frontages of the development site demonstrating compliance with the following requirements:
 - (a) The overall height of fencing not exceeding 1.8 metres above natural ground levels as viewed from outside of the development site; and
 - (b) Infill panels above base level solid components which are shown on the approved drawings being visually permeable.
22. External fixtures, including but not restricted to air-conditioning units, satellite dishes and non-standard television aerials, but excluding solar collectors, are to be located such that they are not visible from the street. Prior to the issue of a building permit, details being submitted of all proposed ventilation systems, including the location of plant equipment, vents and air conditioning units for the Town's approval. All equipment must be adequately screened to the satisfaction of the Town.
23. External clothes drying is prohibited on any of the balconies unless screened from view of the street or other public place.
24. Each dwelling shall be provided with an **effective clothes drying facility**.
25. A Waste Management Plan (WMP) is to be submitted for the Town's approval prior to or in conjunction with the application for a Building Permit. The WMP shall address matters including, but not necessarily limited to, the following:
 - (a) Measures to be implemented for the purpose of minimising the delivery of waste to landfill during occupation, including: the onsite separation of materials for recycling and the expectations of owners and /or tenants;
 - (b) Site Plan showing the location and size of the on-site rubbish disposal area(s), including the number of general rubbish and recycling bins to be provided for the development, including sharing arrangements where the number of bins is less than the number of dwellings;
 - (c) An estimation of the volume of waste to be generated by the proposed development and the capacity of this volume of waste to be accommodated by on site bin storage capacity;
 - (d) Details of intended method of collection;
 - (e) Details of where the bins would be located when waiting collection;
 - (f) Details of advice to be provided to owners and occupiers regarding the WMP; and
 - (g) Details of how the WMP will continue to be applied in perpetuity across the life of the development, including the WMP being incorporated into the strata by-laws for the proposed development.



26. The bin storage areas are:
- (a) To be increased in size **equivalent to that which would be occupied by two additional bins** to cater for bulky rubbish storage while awaiting collection, to the satisfaction of the Town;
 - (b) To be surrounded by a 1.8 metre high minimum wall with a self-closing gate;
 - (c) To be provided with 75mm min thickness concrete floors grading to a 100mm industrial floor waste, connected to sewer, with a hose cock to enable both the bins and bin storage area to be washed out; and
 - (d) To be provided with internal walls that are cement rendered (solid and impervious) to enable easy cleaning.
27. Bins shall be stored only in an approved, designated location, and shall not be stored within any of the approved car parking bays or associated access aisles.
28. The surface finish of boundary walls on the common boundaries with adjoining properties to be the same finish as the external wall finish for the remainder of the dwelling, unless otherwise approved by the Town.
29. Prior to the issue of a building permit the applicant shall lodge a Construction Management Plan to the satisfaction of the Town of Bassendean that provides details of the following:
- (a) Estimated timeline and phasing of construction;
 - (b) Dust control measures;
 - (c) Noise control measures;
 - (d) Access points for heavy vehicles during demolition and construction; and
 - (e) 24 hours contact details of staff available to deal with either an emergency situation or to respond to complaints.
30. The incorporation of public art into the proposed development or a cash-in-lieu payment of one percent of the construction cost of the proposed development in accordance with the Town's adopted Local Planning Policy No. 15 "Percent for Art Policy". Detailed arrangements and agreement with respect to art to be provided on site or alternatively payment of the required fee shall be made prior to or in conjunction with the application for a Building Permit.
31. Prior to the issue of a building permit, a development bond for the sum of \$11,000 being lodged with Council to ensure the satisfactory completion of all works associated with landscaping, car parking, access ways, screen walls, and other associated works.
32. Prior to the issue of a building permit, an acoustic report shall be submitted to the Town for approval which shall:
- (a) be prepared by an acoustic consultant with relevant qualifications and experience equivalent to those required for admission as a Member of the Australian Acoustical Society (to the satisfaction of the Town's Health Services);



- (b) include the presence of tonal components, amplitude or frequency modulations or impulses to ensure noise emissions received at the proposed noise sensitive premises are in compliance with the requirements of the Environmental Protection Act 1986.
 - (c) to satisfaction of the Town, address all matters that are required to demonstrate that acceptable noise criteria will be achieved including:
 - the identification of all noise sources to be addressed from adjacent road and rail infrastructure as well as private properties at Lot 51 (No. 76) Railway Parade and Lot 4 (No. 6) Ivanhoe Street, including, but not limited to: noise emissions from refrigeration motors, air-conditioning units, vehicular movements (including customers and delivery vehicles) and rubbish disposal and collection;
 - determination of noise source levels and character;
 - acoustic data to be in octave bands where noise sources are internal;
 - the establishment of Assigned Levels for noise sensitive premises in the vicinity in accordance with the *Environmental Protection (Noise) Regulations 1997*; and
 - incorporate the following data:
 - (i) date, time and results of measurements and or modelling used to represent the noise associated with live bands;
 - (ii) assigned Levels determined for adjacent areas/noise sensitive premises in the vicinity; and
 - (iii) recommendations for construction and noise control.
33. Measures recommended within the acoustic report shall be implemented to the satisfaction of the Town, and any costs associated with such implementation shall be the responsibility of the owner/applicant.
34. The building hereby approved shall not be occupied until all of the conditions of planning approval have been complied with to the satisfaction of the Town, unless the applicant has entered into an agreement with Council to comply with those conditions within a specified period.
35. This decision constitutes planning approval only and is valid for a period of 2 years from the date of approval. If the subject development is not substantially commenced within the 2 year period, the approval shall lapse and be of no further effect.

Advice Notes:

1. Council's Local Planning Policy No. 2 (LPP2), read in conjunction with Clause 5.3 of the Town's Local Planning Scheme No. 10, requires that each dwelling achieve a minimum 70 point score against the checklist contained within LPP2 to facilitate the density of development which has been proposed. Options available to the applicant to facilitate an increased points score for units 12-19 and 22 include:
 - (a) Cutting back the roof cover to balconies by approximately 1.0m in order to facilitate direct winter sun penetration to living areas of these units (increasing the points score for each dwelling from 57.5 to the minimum required 70 point score); or alternatively



- (b) Providing a solar hot water system for each of these dwellings (increasing the points score for each dwelling from 57.5 to 67.5) along with provision of a detailed landscaping plan which demonstrates low water use for the development as a whole (which would increase points score for each dwelling by a further 5 points to 72.5 points per dwelling).

The applicant is requested to incorporate solar hot water systems into the proposed development.

2. The applicant is advised that in relation to the requirement for a 1% Public Art contribution to be made that the Town can consider on site art works subject to Council approval and demonstration of equivalent value and public access.
3. Please liaise with the Town's Operational Services Directorate in relation to obtaining detailed specifications for works associated with widening of the right-of-way to the rear of the site, prior to undertaking any works on site.
4. The applicant is advised that the central median island within the Railway Parade road reserve allows for only left in / left out vehicle movements from the visitor parking bays on the Railway Parade frontage of the development site.
5. The issue of a Building Permit is required prior to the commencement of any works on site.
6. Dial Before You Dig:
Underground assets may exist in the area that is subject to your application. In the interests of health and safety and in order to protect damage to third party assets please telephone 1100 before excavating or erecting structures. If alterations are required to the configuration, size, form or design of the development upon contacting the Dial Before You Dig service, an amendment to the development consent (or a new development application) may be necessary. Individuals owe asset owners a duty of care that must be observed when working in the vicinity of plant or assets. It is the individual's responsibility to anticipate and request the nominal location of plant or assets on the relevant property via Dial Before You Dig "1100" number in advance of any construction activities.
7. Telecommunications Act 1997 (Commonwealth):
Telstra (and its authorised contractors) are the only companies that are permitted to conduct works on Telstra's network and assets. Any person interfering with a facility or installation owned by Telstra is committing an offence under the Criminal Code Act 1995 (Cth) and is liable for prosecution. Furthermore, damage to Telstra's infrastructure may result in interruption to the provision of essential services and significant costs. If you are aware of any works or proposed works which may affect or impact on Telstra's assets in any way, please contact Telstra's Network Integrity Team on 1800810443.
8. If the planning approval lapses, no development shall be carried out without further approval having first been sought and obtained.



9. If an applicant is aggrieved by this determination there is a right of review under Part 14 of the *Planning and Development Act 2005*. An application for review must be lodged within 28 days of the determination.

Where an approval has so lapsed, no development shall be carried out without further approval having first been sought and obtained, unless the applicant has applied and obtained Development Assessment Panel approval to extend the approval term under regulation 17(1)(a) of the *Development Assessment Panel Regulations 2011*.



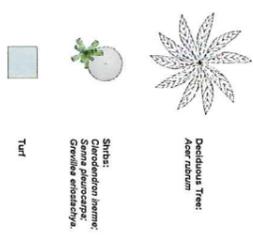
DEVELOPMENT ASSESSMENT PANELS
APPROVED
23 JUNE 2015

TOWN OF BASSENDEAN
 13 MAY 2015
 RECEIVED

UNIT	ORIENTATION (CONCRETE SLABS FACING NORTH)	NORTH FACING CURTAIN WALLS AND WINDOWS 50% OF NORTH WALL	BED WINDOWS AND UNDER FACING (BED SOUTH FACING)	EAST AND WEST FACING WALLS BLANK	60% ROOMS CROSS VENTILATED	SOLAR HOT WATER UNIT PERIOD	LANDSCAPE DESIGN COMPLIANCE	TOTAL
1	0	25	15	20	10	0	0	70
2	0	25	15	20	10	0	0	70
3	0	25	15	20	10	0	0	70
4	0	25	15	20	10	0	0	70
5	0	25	15	20	10	0	0	70
6	0	25	15	20	10	0	0	70
7	0	25	15	20	10	0	0	70
8	0	25	15	20	10	0	0	70
9	0	25	15	20	10	0	0	70
10	0	25	15	20	10	0	0	70
11	0	25	15	20	10	0	0	70
12	0	25	15	20	10	0	0	70
13	0	25	15	20	10	0	0	70
14	0	25	15	20	10	0	0	70
15	0	25	15	20	10	0	0	70
16	0	25	15	20	10	0	0	70
17	0	25	15	20	10	0	0	70
18	0	25	15	20	10	0	0	70
19	0	25	15	20	10	0	0	70
20	0	25	15	20	10	0	0	70
21	0	25	15	20	10	0	0	70
22	0	25	15	20	10	0	0	70

PLANT RATIO AREA

Plant Name	Area
UNIT 1	12.44 m ²
UNIT 2	12.44 m ²
UNIT 3	12.44 m ²
UNIT 4	12.44 m ²
UNIT 5	12.44 m ²
UNIT 6	12.44 m ²
UNIT 7	12.44 m ²
UNIT 8	12.44 m ²
UNIT 9	12.44 m ²
UNIT 10	12.44 m ²
UNIT 11	12.44 m ²
UNIT 12	12.44 m ²
UNIT 13	12.44 m ²
UNIT 14	12.44 m ²
UNIT 15	12.44 m ²
UNIT 16	12.44 m ²
UNIT 17	12.44 m ²
UNIT 18	12.44 m ²
UNIT 19	12.44 m ²
UNIT 20	12.44 m ²
UNIT 21	12.44 m ²
UNIT 22	12.44 m ²
TOTAL	273.68 m ²



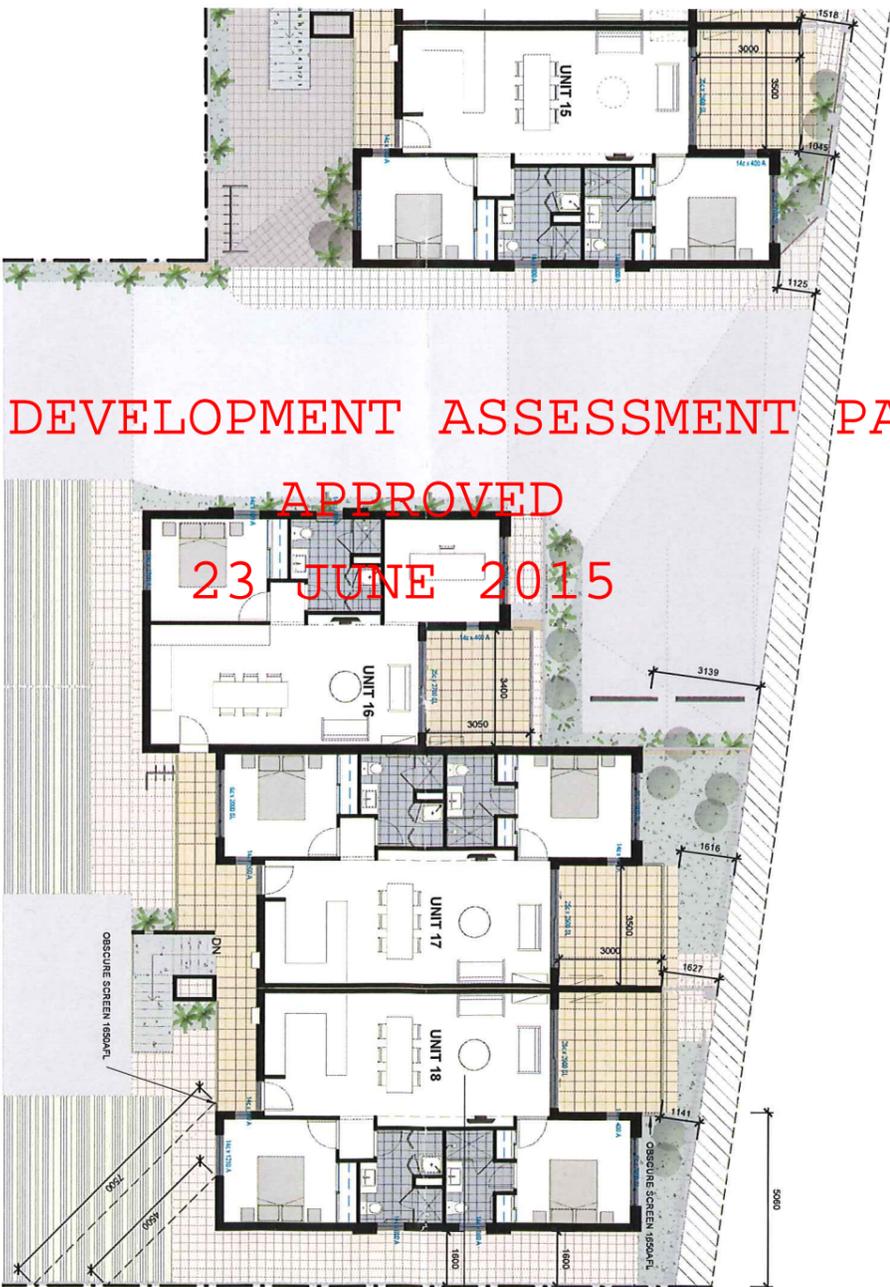
PROPOSED MULTIPLE DWELLING DEVELOPMENT
 LOTS 54 (No. 72) RAILWAY PARADE, BASSENDEAN,
 (Town of Bassendean)

No.	Description	Date	PROPOSED SITE / GROUND FLOOR PLAN (PART B)
1	DA PLANS	02.02.2015	
2	REVISED DA	11.05.2015	

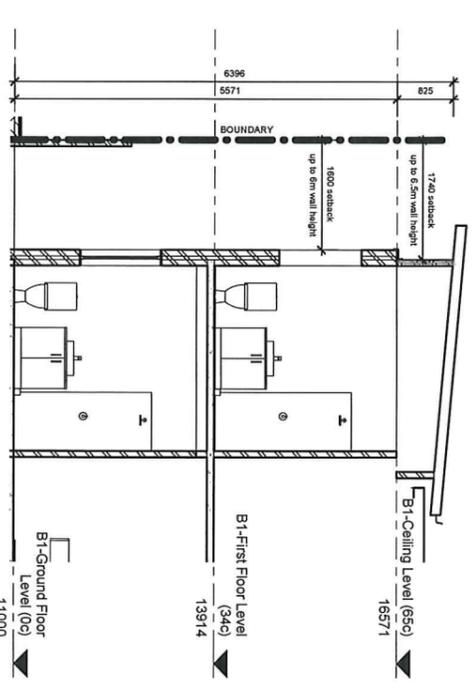
Drawn by	Checked by	Date	Scale
Urban & Land Development	Urban & Land Development	11.05.2015	A2.02
Urban & Land Development	Urban & Land Development		1:100



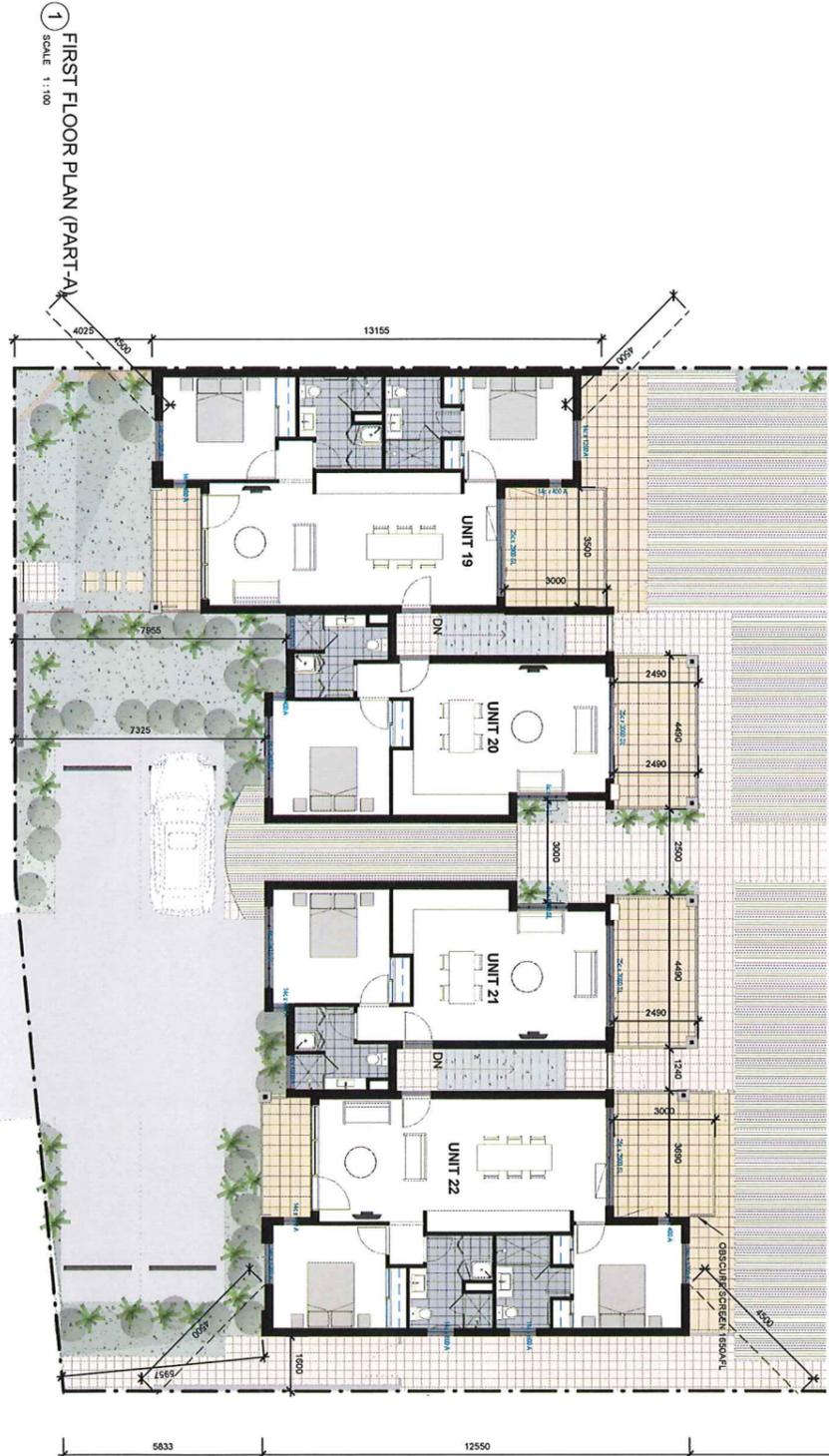
2 FIRST FLOOR PLAN (PART-B)
SCALE 1:100



DEVELOPMENT ASSESSMENT PANELS
APPROVED
23 JUNE 2015



3 Section 1
SCALE 1:50



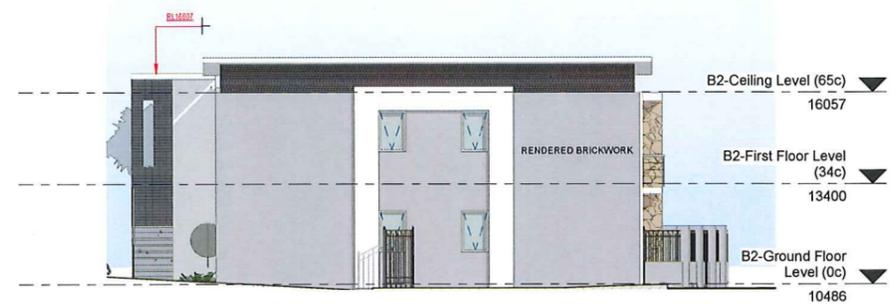
1 FIRST FLOOR PLAN (PART-A)
SCALE 1:100

TOWN OF BASSENDEAN
13 MAY 2015
RECEIVED



No.	Description	Date	Author	Checked By	Scale
1	DA PLANS	02/02/2015	LAL	DK	A2.05
2	REVISED DA	11/05/2015	LAL	DK	A2.05

TOWN OF BASSENDEAN
 13 MAY 2015
 RECEIVED



① UNIT 4 EAST ELEVATION
 SCALE 1:100



⑤ SOUTH ELEVATION
 SCALE 1:100

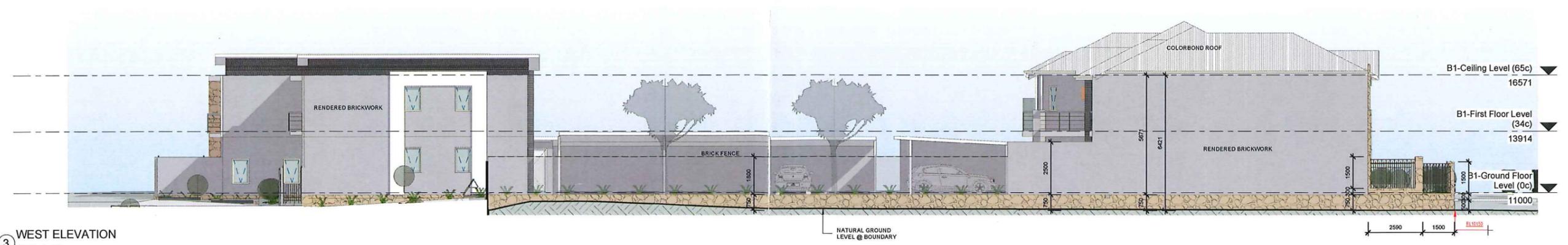
DEVELOPMENT ASSESSMENT PANELS

APPROVED

23 JUNE 2015

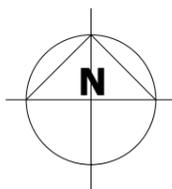
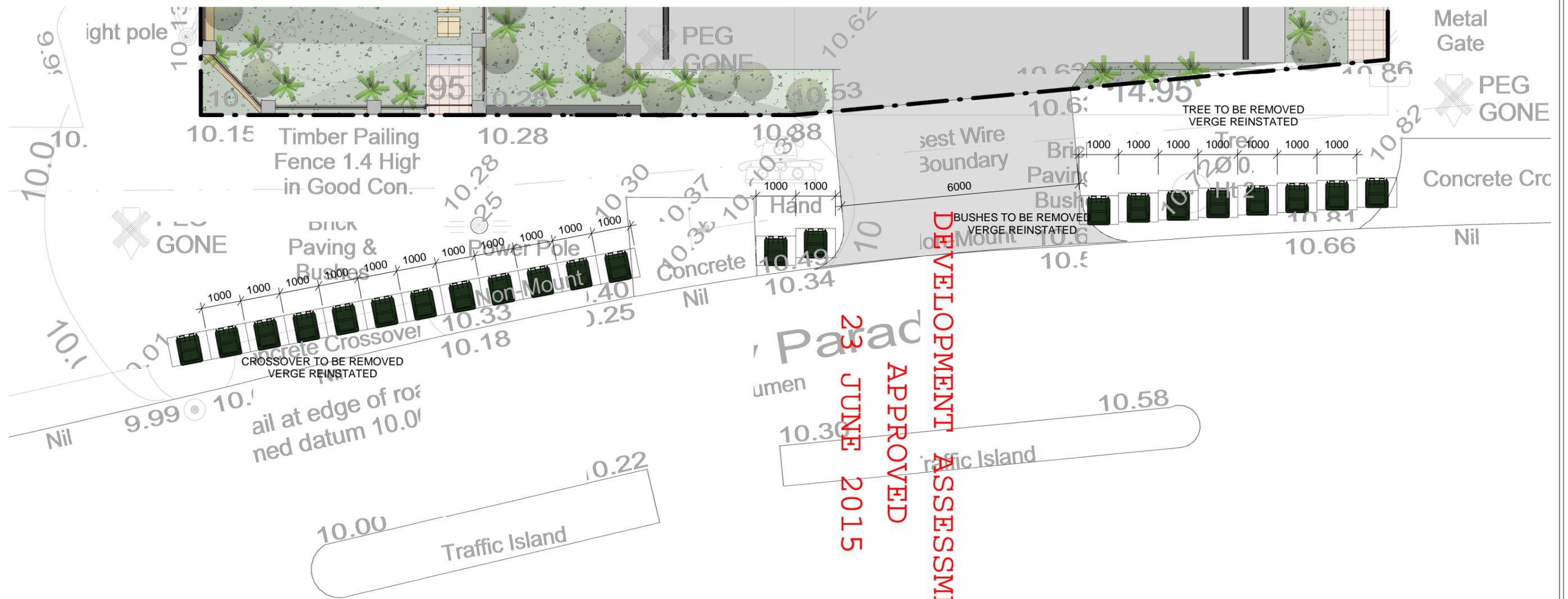


② NORTH ELEVATION FRONT BLOCK
 SCALE 1:100



③ WEST ELEVATION
 SCALE 1:100

No.	Description	Date	ELEVATIONS
1	DA PLANS	02/02/2015	
2	REVISED DA	11/05/2015	
	Project Number	1416	Uniting Australia
	Drawn by	11/05/2015	AK.01
	Checked by		2
	Scale	A3	1:100

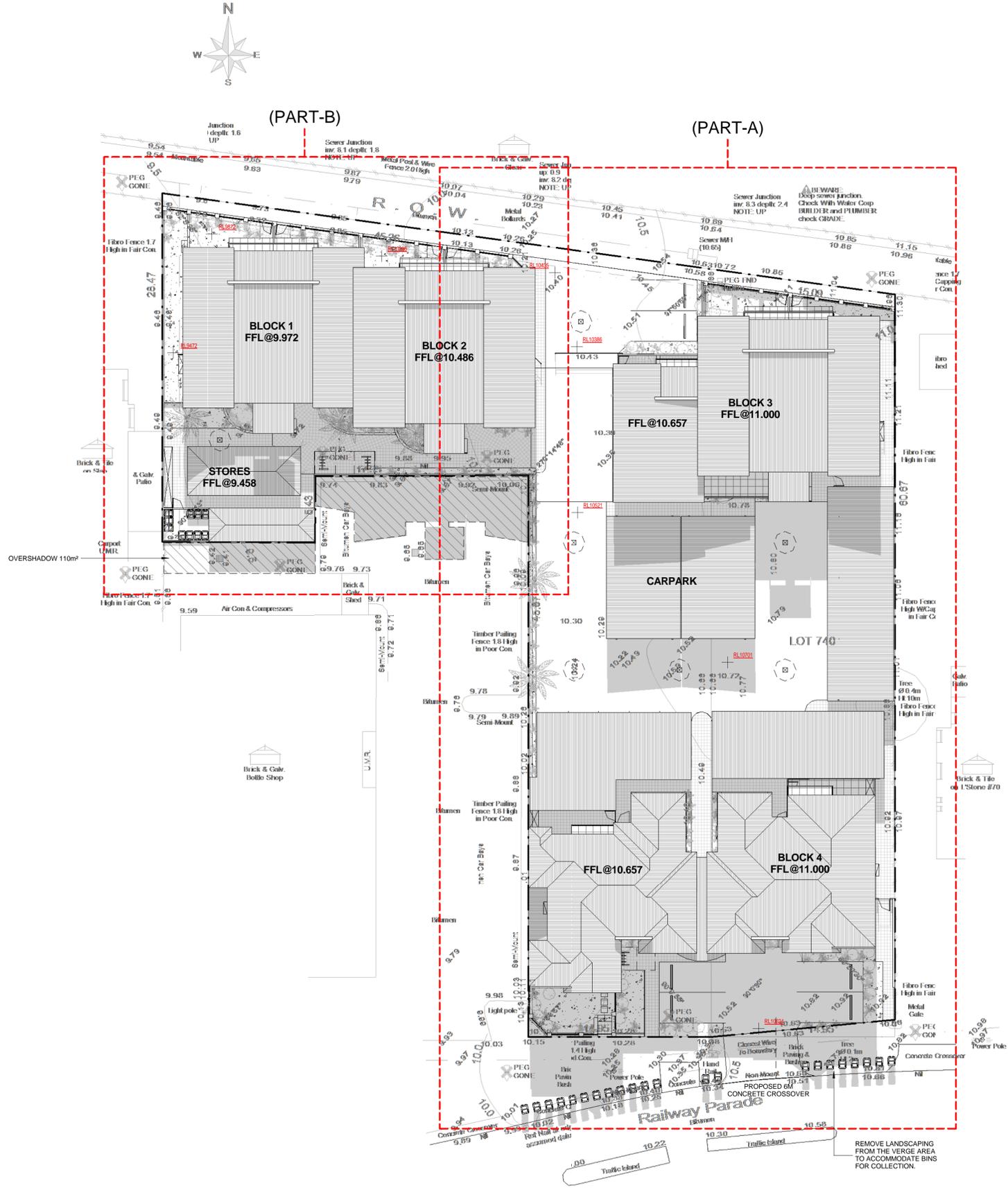


No.	DESCRIPTION	DATE
3	VERGE PICK UP PLAN	18.05.2015

22 BINS VERGE PICKUP PLAN

Project number	1416	Drawing number	Revision/Issue
Issue Date	18.05.2015	A0.00	3
Drawn by	NK	Scale @ A3	1 : 100
Checked by	CF		

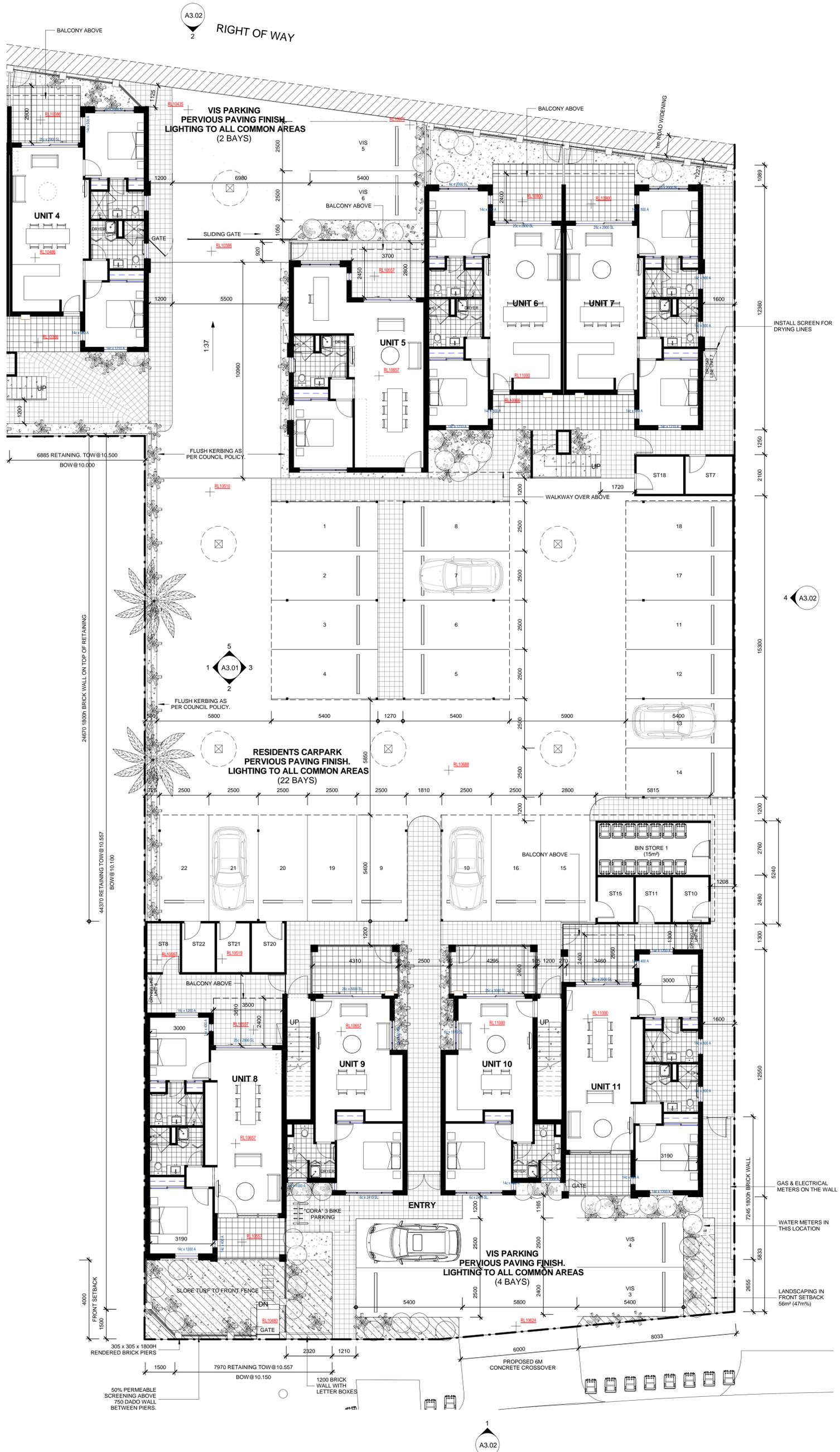
PROPOSED SITE COVER 1386m² (54%)
 PROPOSED OPEN SPACE 1175m² (46%)



27/10/2015 11:22:03 AM

PROPOSED MULTIPLE DWELLING DEVELOPMENT
 LOTS 54 (No.72) RAILWAY PARADE, BASSENDEAN,
 (Town of Bassendean)

No.	Description	Date	PROPOSED SITE DEVELOPMENT PLAN		
1	DA PLANS	02.02.2015	Project number	1416	Revision
2	REVISED DA	11.05.2015	Issue Date	27.10.2015	7
4	REVISED DA	06.07.2015	Drawn by	NK	A1.01
5	REVISED DA	11.09.2015	Checked by	CF	Scale @ A1
7	REVISE BINS & BIKES STORES	27.10.2015	Scale @ A1		1:200



PLOT RATIO AREA	
Name	Area
UNIT 1	74.44 m ²
UNIT 2	74.44 m ²
UNIT 3	74.44 m ²
UNIT 4	74.44 m ²
UNIT 5	88.19 m ²
UNIT 6	74.81 m ²
UNIT 7	74.06 m ²
UNIT 8	74.76 m ²
UNIT 9	51.73 m ²
UNIT 10	51.88 m ²
UNIT 11	74.64 m ²
UNIT 12	74.44 m ²
UNIT 13	74.44 m ²
UNIT 14	73.74 m ²
UNIT 15	74.44 m ²
UNIT 16	66.86 m ²
UNIT 17	74.44 m ²
UNIT 18	74.44 m ²
UNIT 19	70.20 m ²
UNIT 20	51.97 m ²
UNIT 21	51.97 m ²
UNIT 22	74.26 m ²
Grand total: 22	1528.00 m ²

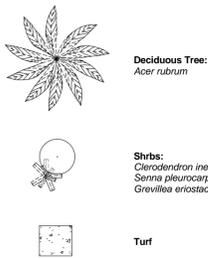


**PROPOSED MULTIPLE DWELLING DEVELOPMENT
 LOTS 54 (No.72) RAILWAY PARADE, BASSENDEAN,
 (Town of Bassendean)**

No.	Description	Date	PROPOSED SITE / GROUND FLOOR PLAN (PART A)	
1	DA PLANS	02.02.2015	Project number	1416
2	REVISED DA	11.05.2015	Drawing number	A2.01
4	REVISED DA	06.07.2015	Issue Date	27.10.2015
5	REVISED DA	11.09.2015	Drawn by	NK
7	REVISE BINS & BIKES STORES	27.10.2015	Checked by	CF
			Scale @ A1	1:100



UNIT	ORIENTATION (LONGEST AXIS FACING NORTH)	NORTH FACING COURTYARD AND LIVING AREA WINDOWS 50% OF NORTH WALL	BED WINDOWS MINIMISED IN AREA AND NORTH FACING (1BED SOUTH FACING)	EAST AND WEST FACING WALLS BLANK	60% ROOMS CROSS VENTILATED	SOLAR HOT WATER UNIT OR SOLLER PERGOLA	LANDSCAPE DESIGN COPLIANCE	TOTAL
1	0	25	15	20	10	0	0	70
2	0	25	15	20	10	0	0	70
3	0	25	15	20	10	0	0	70
4	0	25	15	20	10	0	0	70
5	0	25	15	20	10	0	0	70
6	0	25	15	20	10	0	0	70
7	0	25	15	20	10	0	0	70
8	0	25	15	20	10	0	0	70
9	0	25	15	20	10	0	0	70
10	0	25	15	20	10	0	0	70
11	0	25	15	20	10	0	0	70
12	0	25	15	20	10	0	0	70
13	0	25	15	20	10	0	0	70
14	0	25	15	20	10	0	0	70
15	0	25	15	20	10	0	0	70
16	0	25	15	20	10	0	0	70
17	0	25	15	20	10	0	0	70
18	0	25	15	20	10	0	0	70
19	0	25	15	20	10	0	0	70
20	0	25	15	20	10	0	0	70
21	0	25	15	20	10	0	0	70
22	0	25	15	20	10	0	0	70



PLOT RATIO AREA	
Name	Area
UNIT 1	74.44 m²
UNIT 2	74.44 m²
UNIT 3	74.44 m²
UNIT 4	74.44 m²
UNIT 5	68.19 m²
UNIT 6	74.81 m²
UNIT 7	74.06 m²
UNIT 8	74.76 m²
UNIT 9	51.73 m²
UNIT 10	51.88 m²
UNIT 11	74.84 m²
UNIT 12	74.44 m²
UNIT 13	74.44 m²
UNIT 14	73.74 m²
UNIT 15	74.44 m²
UNIT 16	62.86 m²
UNIT 17	74.44 m²
UNIT 18	74.44 m²
UNIT 19	70.20 m²
UNIT 20	51.97 m²
UNIT 21	51.97 m²
UNIT 22	74.26 m²
Grand total: 22	1620.00 m²



20160015 11/24/16 AW

PROPOSED MULTIPLE DWELLING DEVELOPMENT
LOTS 54 (No.72) RAILWAY PARADE, BASSENDEAN,
(Town of Bassendean)

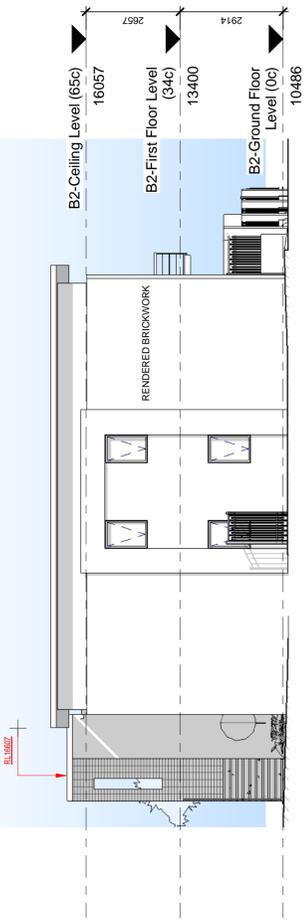
No.	Description	Date	FIRST FLOOR PLAN (PART A)		
2	REVISED DA	11.05.2015	Project number	1416	Revision
4	REVISED DA	06.07.2015	Drawing number	A2.05	7
5	REVISED DA	11.09.2015	Issue Date	27.10.2015	
6	WINDOW SIZES OF UNIT 19 ADDED	20.10.2015	Drawn by	NK	
7	REVISE BINS & BIKES STORES	27.10.2015	Checked by	CF	Scale @ A1



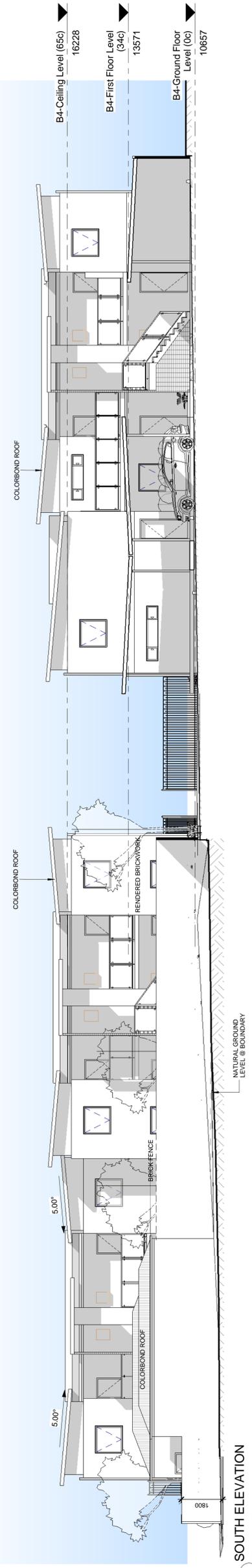
No.	Description	Date	FIRST FLOOR PLAN (PART B)		
4	REVISED DA	06.07.2015	Project number	1416	Drawing number
5	REVISED DA	11.09.2015	Issue Date	27.10.2015	Revision
7	REVISE BINS & BIKES STORES	27.10.2015	Drawn by	NK	A2.06
			Checked by	CF	Scale @ A1
					1:100

No.	Description	Date	ELEVATIONS	
1	DA PLANS	02.02.2015		
2	REVISED DA	11.05.2015		
4	REVISED DA	06.07.2015		
5	REVISED DA	11.09.2015		
7	REVISE BINS & BIKES STORES	27.10.2015		

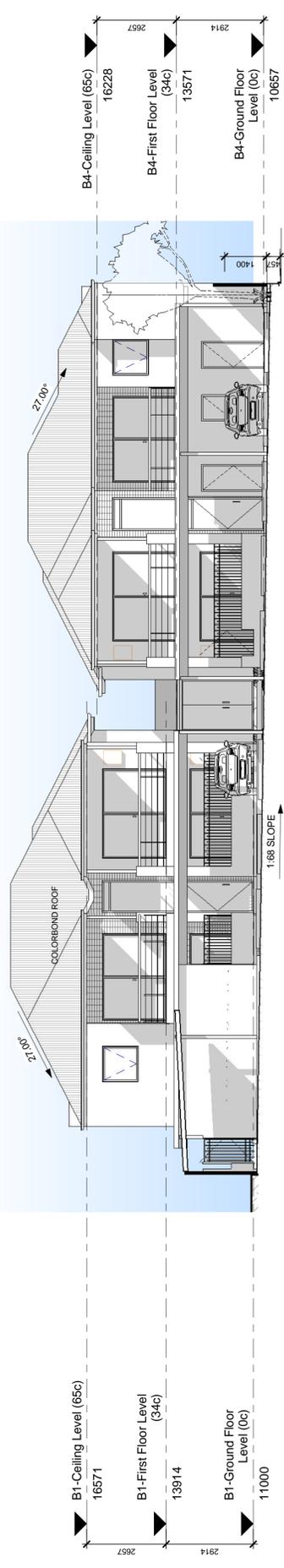
Project number	1416	Drawing number	A3.01	Revision	7
Issue Date	27.10.2015	Drawn by	NK	Scale	@ A1
Checked by	CF				



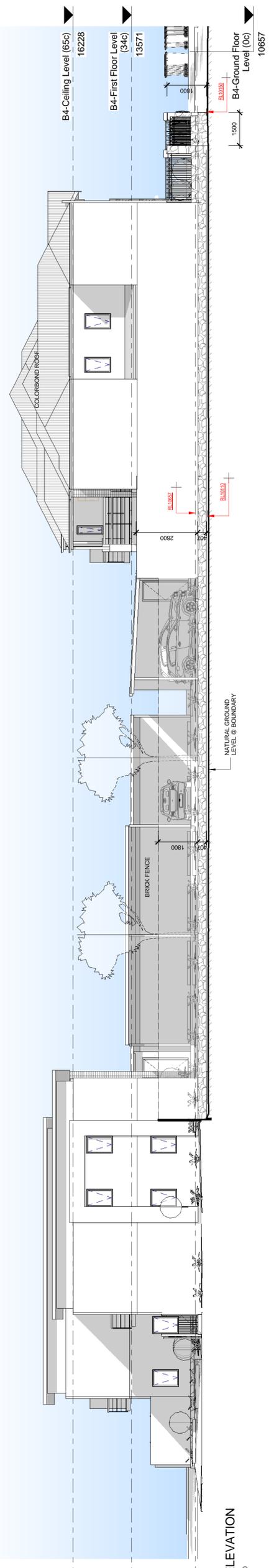
1 SCALE 1:100



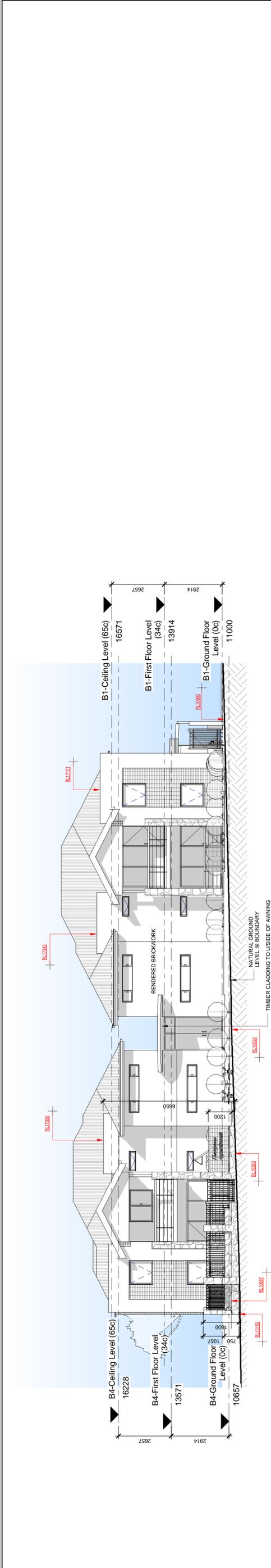
5 SCALE 1:100



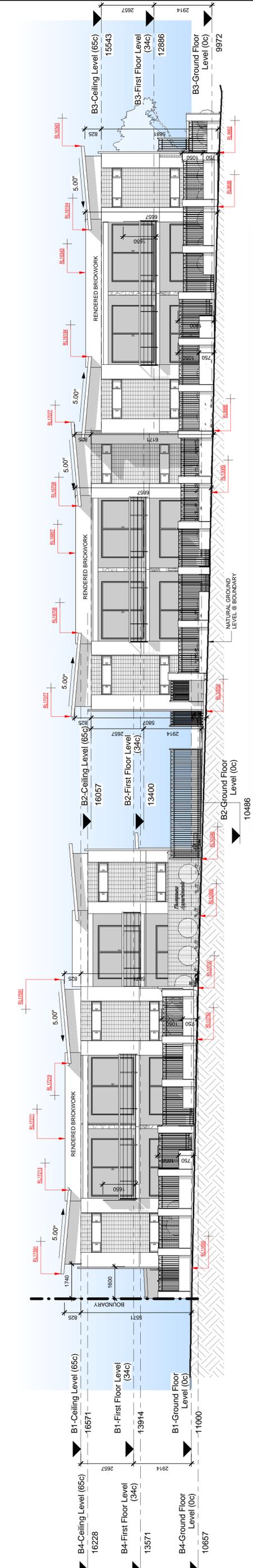
2 SCALE 1:100



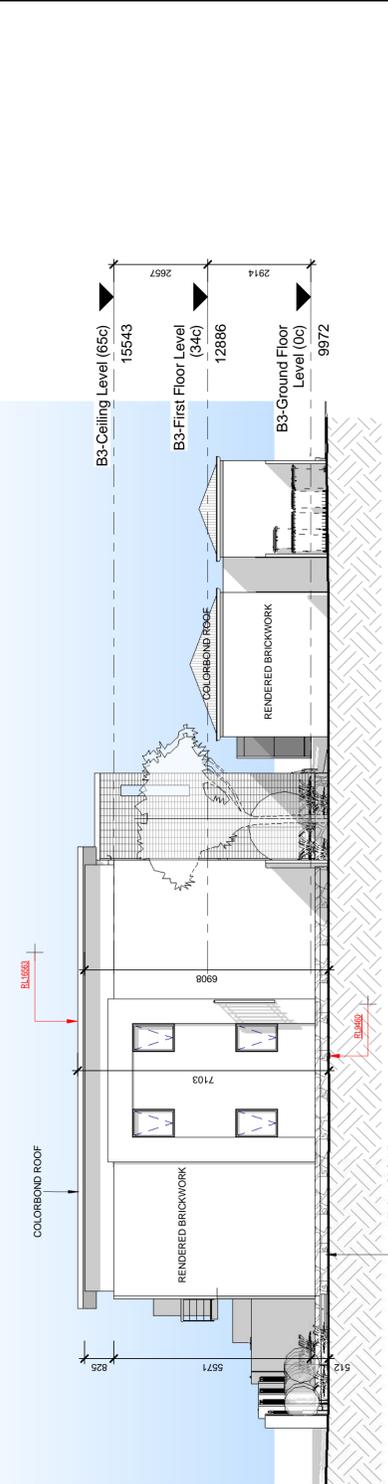
3 SCALE 1:100



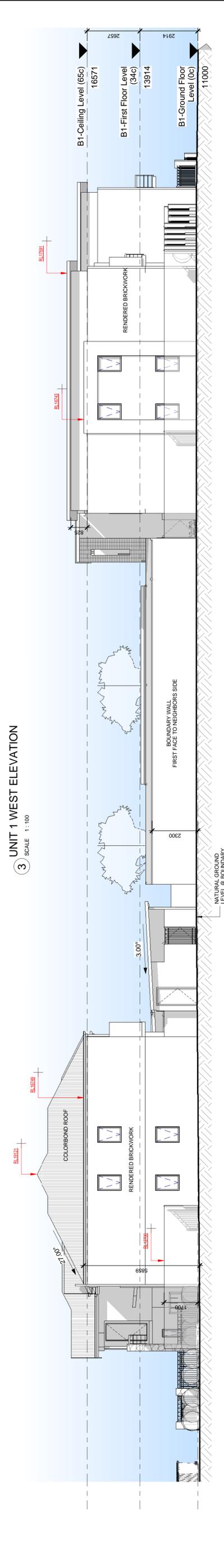
1 RAILWAY PARADE ELEVATION
 SCALE 1:100



2 ROW ELEVATION
 SCALE 1:100



3 UNIT 1 WEST ELEVATION
 SCALE 1:100



4 EAST ELEVATION
 SCALE 1:100

PROPOSED MULTIPLE DWELLING DEVELOPMENT
 LOTS 54 (No.72) RAILWAY PARADE, BASSENDEAN,
 (Town of Bassendean)

No.	Description	Date	ELEVATIONS	
1	DA PLANS	02.02.2015		
2	REVISED DA	11.05.2015		
4	REVISED DA	06.07.2015	Project number	1416
5	REVISED DA	11.09.2015	Issue Date	27.10.2015
7	REVISE BINS & BIKES STORES	27.10.2015	Drawn by	Author
			Checked by	Checker
			Scale	A1
			Drawing number	A3.02
			Revision	7

31 March 2017

Chief Executive Officer
Town of Bassendean
PO Box 87
BASSENDEAN WA 6934

Document #: IPA:11884817
Date: 31.03.2017
Officer: MARY BIDSTRUP
File: A4380



Attention: Mr Brian Reed – Manager, Development Services

Dear Brian

**APPLICATION TO AMEND DEVELOPMENT APPROVAL
PROPOSED MULTIPLE DWELLING DEVELOPMENT (22 APARTMENTS)
LOT 54 (NOS.72 & 74) RAILWAY PARADE, BASSENDEAN
TOWN OF BASSENDEAN (YOUR REF: DA2015-030 & DAP/15/00740)**

Urban & Rural Perspectives, on behalf of the current landowners, hereby submit an application seeking the Metro Central Joint Development Assessment Panel's approval to:

- a) amend the current development approval for the abovementioned property so as to extend the period within which the proposed multiple dwelling development must be substantially commenced; and
- b) delete a number of conditions to which the approval is currently subject.

Please find enclosed the following information to assist the Town and Joint Development Assessment Panel's consideration and processing of the application:

- A completed and signed 'Application for Development Approval' form;
- A completed and signed 'Application for Amendment of a Development Assessment Panel Determination' form (DAP Form 2);
- A copy of the Certificate of Title for Lot 54;
- Remittance of \$450.00 being the application fees payable to the Town and Development Assessment Panel (i.e. \$295.00 and \$155.00 respectively);
- Three (3) hard copies of scaled plans prepared in support of the application; and
- Two (2) compact discs containing a copy of the application documentation and plans in electronic format.

We request that a receipt in respect of the abovementioned application fees be forwarded to this office at **PO Box 2507 MALAGA WA 6944** at the Town's earliest convenience.

When assessing the application we ask that the Town and Metro Central Joint Development Assessment Panel (JDAP) have due regard for the following key points:

1. On 1 July 2015 the Metro Central JDAP granted conditional planning approval for the development of twenty two (22) new multiple dwellings on Lot 54.
2. A number of conditions imposed on the planning approval required the preparation and submission of amended plans for consideration and approval by the Town prior to preparation and lodgment of a building permit application. Amended plans were subsequently prepared by this office and submitted to the Town on 23 July 2015.



3. Processing of the amended plans by the Town took a considerable amount of time. The Town's acceptance of the amended plans was provided via email on 26 November 2015 however formal correspondence confirming its approval was not provided to this office until 8 January 2016.
4. Given the time taken to secure the Town's formal written approval to the amended plans, the two (2) year planning approval period has been substantially diminished. The associated loss of time has had a significant impact on the future development of the land in terms of the preparation of working drawings, appointment of specialist consultants, arranging building contracts, pre-sales and construction financing.
5. In light of the above, the landowners would like to secure the JDAP's formal approval to amend the current planning approval for Lot 54 by extending the period within which the proposed multiple dwelling development must be substantially commenced. An additional two (2) year approval term is hereby requested.
6. In addition to our request to extend the period of the current planning approval for Lot 54, approval is sought to delete the following conditions from the original approval when issuing a new approval on the grounds these conditions have now been addressed to the Town's satisfaction through the preparation and submission of amended plans (see Attachment 1):
 - Conditions 1 to 4 inclusive;
 - Condition 15;
 - Condition 16;
 - Condition 17;
 - Condition 21;
 - Condition 24; and
 - Condition 26.

In light of the above information we respectfully request the Town and Metro Central JDAP's favorable consideration and the JDAP's approval of this application at its earliest possible convenience.

Should you have any queries or require any additional information please do not hesitate to contact the undersigned of this office on 9248 8777 or joe@urp.com.au.

Yours faithfully,



Joe Douglas
Managing Director / Principal Town Planner
Urban & Rural Perspectives



Encl. Completed & signed 'Application for Development Approval' form & 'DAP Form 2'
Application fees of \$450.00 (Cheque)
Three (3) hard copies of the amended site development plans
Two (2) compact discs containing electronic copies of the application documentation & plans

cc: Mark & Sandra Hammond - Landowners